

Indiana Department of Transportation

County Franklin

Route SR 252

Des. No. 1700194

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:	State Road (SR) 252/Franklin County
Designation Number:	1700194
Project Description/Termini:	Slide Correction Project along SR 252, approximately 0.80 mile east of US 52.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

_____	_____	_____	_____
ESM Signature	Date	ES Signature	Date
_____		_____	
FHWA Signature		Date	

Release for Public Involvement

_____	_____	_____	_____
ESM Initials	Date	ES Initials	Date

Certification of Public Involvement

_____	_____
Office of Public Involvement	Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env.
Reviewer Signature: _____ Date: _____

Name and Organization of CE/EA
Preparer: Chad Costa/Lochmueller Group

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. The level of public involvement should be commensurate with the proposed action.

Does the project have a historic bridge processed under the Historic Bridges PA*? Yes No
If No, then: Opportunity for a Public Hearing Required? X

*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks: Notice of Entry
Notice of entry letters were mailed to potentially affected property owners within the project area on February 7, 2019 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the notice of entry letter is included in Appendix G, pages G1 to G3.
Public Involvement:
The project will meet the minimum requirements described in the current Indiana Department of Transportation (INDOT) Public Involvement Manual which requires the project sponsor to offer the public an opportunity to submit comment and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Public Controversy on Environmental Grounds Yes No
Will the project involve substantial controversy concerning community and/or natural resource impacts? X

Remarks: At this time, there is no substantial public controversy concerning impacts to the community or to natural resource.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: Seymour
Local Name of the Facility: SR 252

Funding Source (mark all that apply): Federal X State X Local Other*

*If other is selected, please identify the funding source:

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PURPOSE AND NEED:

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

Need:

The need for this project stems from the deteriorated condition of the pavement and roadway embankment along SR 252 creating safety concerns for the traveling public. This condition is a result of the land slide occurring along the south side of SR 252 where the failure plane of the slide is occurring at the soil/rock interface of the overburden soil and weathered shale layer. Scarp lines (lines in embankment resulting from erosion) are visible in the eastbound and westbound lane and is where the pavement failure is occurring. To keep the road open to traffic the District has completed pavement overlays to correct the slumped pavement, but such maintenance activities provide only temporary relief.

Purpose:

The purpose of the project is to restore the section of SR 252 damaged by the landslide activity, stabilizing the failed slope, which will reduce the potential for future slide activity, and result in improved traffic mobility and safety for the traveling public.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Franklin

Municipality: Brookville

Limits of Proposed Work: Along SR 252, beginning approximately 0.66 mile east of US 52 and ending approximately 0.86 mile east of US 52 for a total length of 0.2 mile (1,084 feet).

Total Work Length: 0.20 Mile(s)

Total Work Area: 1.80 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

If yes, when did the FHWA grant a conditional approval for this project?

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: _____	

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

The Federal Highway Administration (FHWA) and the INDOT Seymour District propose to proceed with a federal-aid slide correction project along SR 252 in Franklin County, Indiana.

Location:

The project is located along SR 252, approximately 0.8 mile east of US 52. Specifically, the project is located in Brookville Township within Section 28, Township 9 North, Range 2 West as depicted on the Brookville and Whitcomb U.S. Geological Survey 1:24,000 quadrangles (Appendix B, page B2).

Existing Conditions:

Within the project area, SR 252 is functionally classified as a major collector and consists of two 11-foot wide asphalt travel lanes (one in each direction) with a 1 to 4-foot wide usable shoulder along the westbound lane and no shoulder along the eastbound lane. The posted speed limit on SR 252 is 55 miles per hour (mph). Drainage along westbound SR 252 is conveyed via a roadside ditch draining from east to west. The ditch outlets into three small structures within the project area (Appendix B, pages B19 and B20). These structures all drain into an unnamed tributary (UNT) to East Fork Whitewater River which

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runs parallel to SR 252 south of the project limits. Adjacent land use within the project area includes residential and forested land (Appendix B, page B3).

Preferred Alternative:

The preferred alternative will construct a soldier pile wall with ground anchors along SR 252 to reinforce the landslide area along the south side of the roadway. The piles will be installed on varying 7 feet and 8 feet centers and will be embedded to a minimum of 10 feet into the weathered shale and an additional 2 feet into bedrock. Concrete lagging panels will be installed between the soldier piles to retain the earth. Drilling to a depth of 30 feet will occur to install shafts for the new retaining wall. Installation of this new retaining wall will require the removal of the existing stone retaining wall along the south side of the roadway. New guardrail, approximately 1,088 feet in length will be installed along the eastbound shoulder.

The proposed roadway typical section will consist of two 11-foot travel lanes, a 4-foot paved shoulder with guardrail along the eastbound lane, and a 2-foot paved shoulder along the westbound lane. A concrete gutter will accommodate drainage along the westbound lane. To accommodate construction of the wall and provide maintenance access a 12-foot wide aggregate haul road will be constructed along the south side of the wall (Appendix B, page B19). Beyond the haul road, Class II riprap will be placed along the sideslope for erosion control.

To help minimize impacts to the existing westbound roadside ditch, adjacent steep wooded hillside, and utilities, the proposed roadway will be shifted south 2 feet to 4 feet. This shift will allow the required 2-foot paved shoulders on the north side to be constructed and the 4-foot paved shoulder on the south side to accommodate guardrail. Also, to help improve safety of the roadway, the horizontal curve superelevation and associated transition rates are being improved. These improvements will be completed with a combination of widening, mill and overlay, and variable depth intermediate hot mix asphalt (HMA) pavement.

The roadway reconstruction will involve full-depth replacement of the roadway along the eastbound lanes. Beginning about midway into the eastbound lane to the edge of the shoulder along the westbound lane, the roadway surface will be milled to a depth of 1.5 inches and an HMA overlay will be applied the entire length of the project.

Two culverts conveying stormwater and roadside drainage under SR 252 will be replaced with culverts that are hydraulically sufficient in size. Excavation will occur to replace the existing CMP culverts, remove the existing stone retaining wall, and reconstruct the roadway and will not exceed 10 feet. No new permanent lighting will be installed and the use of temporary lighting during construction is not anticipated. Please refer to Appendix B for maps depicting the project area (Appendix B, pages B1 to B4), photographs of the project area (Appendix B, pages B5 to B13), and the Preliminary Design Plans (Appendix B, pages B14 to B23).

The termini of the project provide the logical beginning and end point necessary to complete the slide correction. The project is independent of any other action and able to be constructed without relying on the completion of any other project.

The proposed maintenance of traffic (MOT) plan includes the complete closure of SR 252 within the project area. An official detour utilizing will be established. Please refer to the *Maintenance of Traffic* section of this document for full details.

The preferred alternative meets the purpose and need of the project by correcting the existing deficiencies in the roadway pavement and stabilizing the roadway sideslope which will prevent any further landslides.

OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Soil Nail Wall: This alternative would involve the construction of a soil nail wall to stabilize the land slide. Due to the slope and extent of the slide, the soil nailing would have to extend further down the slope along the eastbound lane of SR 252 than the preferred alternative. This would require additional tree clearing and have resulted in more stream impacts. Additionally,

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there is a chance that the soil nail wall would fail over time due to the substrate present along SR 252. Due to the added environmental impacts and the potential for future failure, this alternative was discarded from further consideration.

No Build Alternative: This alternative involved not improving the roadway along SR 252 where a landslide is causing for the road to fracture. Not addressing slide and roadway deterioration in the roadway would likely result in the continued sliding of the roadway side slope and the eventual failure of the road. While this alternative eliminates costs and any environmental impacts, it would not have met the objectives of the purpose and need of the project because it would result in the failure of the roadway. Therefore, this alternative was discarded from further consideration.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe)

ROADWAY CHARACTER:

Functional Classification: Major collector
 Current ADT: 1,195 VPD (2020) Design Year ADT: 1,195 VPD (2040)
 Design Hour Volume (DHV): 105 Truck Percentage (%) 3.1
 Designed Speed (mph): 55 Legal Speed (mph): 55

	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	Travel lanes		Travel lanes	
Pavement Width:	24	ft.	28	ft.
Shoulder Width:	1-4	ft.	2-4	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

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DESIGN CRITERIA FOR BRIDGES:

 Structure/NBI Number(s): N/A Sufficiency Rating: N/A
 (Rating, Source of Information)

	Existing		Proposed	
Bridge Type:	N/A		N/A	
Number of Spans:	N/A		N/A	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	N/A	ft.	N/A	ft.
Outside to Outside Width:	N/A	ft.	N/A	ft.
Shoulder Width:	N/A	ft.	N/A	ft.
Length of Channel Work:			192	ft.

Describe bridges and structures; provide specific location information for small structures.

Remarks: No bridges are present within the project area. Two pipe culverts within the project area will be replaced. Neither structure is large enough to be included in the INDOT BIAS bridge and structure inspection database.

The first structure is a 2-foot and 6-inch diameter, 35-foot long CMP located 572 feet east of the western terminus (at Station 77+62) and conveys UNT 3 to East Fork Whitewater River (Appendix B, page B21). This CMP will be replaced by Structure No. 11, a 48-foot long pipe culvert with a 3-foot and 6-inch diameter.

The second structure is a 1-foot and 6-inch diameter, 48-foot long CMP located 101 feet west of the eastern terminus (at Station 81+73) and conveys UNT 2 to East Fork Whitewater River (Appendix B, page B21). This CMP will be replaced by Structure No. 12, a 53-foot long pipe culvert with a 2-foot and 6-inch diameter.

The proposed project will impact a total of 91 linear feet of UNT 2 to East Fork Whitewater River and 101 linear feet of UNT 3 to East Fork Whitewater River (Appendix B, page B3, B21 and B22).

Will the structure be rehabilitated or replaced as part of the project? Yes No N/A

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks: The MOT will require the closure of SR 252 within the project area. Because of the road closure, a detour will be established (Appendix B, page B18). The marked detour route will include SR 52, SR 1, I-74, Ohio SR 128, and Ohio SR 126. The length of the detour will be approximately 56 miles. Access to all drives will be maintained during construction. The detour is expected to last 6 months. Since the detour route will use roads in the State of Ohio, coordination with the Ohio Department of Transportation (ODOT) will occur prior to letting. This is included as a firm commitment in *Section J: Environmental Commitments* of this document.

The closure and detour will pose a temporary inconvenience to traveling motorists, including school buses and emergency services; however, all inconveniences will cease upon project completion. Delays would occur during construction but will cease upon project completion.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ *225,000 (2019) Right-of-Way: \$ 675,000 (2020) Construction: \$ 6,368,379 (2022)

*PE phase was programmed in the FY 2018-2021 STIP

Anticipated Start Date of Construction: Spring 2022

Date project incorporated into STIP July 2, 2019

Is the project in an MPO Area? Yes No

If yes,

Name of MPO N/A

Location of Project in TIP N/A

Date of incorporation by reference into the STIP N/A

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.37	0.02
Commercial	0	0
Agricultural	0	0
Forest	1.73	0
Wetlands	0	0
Other: Maintained Roadside	0	0
Other:	0	0
TOTAL	2.10	0.02

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks: Within the project area, the existing ROW is located along the edge of pavement along SR 252. The width varies from 23 to 30 feet wide along the centerline of SR 252.

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The project requires approximately 2.1 acres of permanent ROW and approximately 0.02 acre of temporary ROW. The permanent ROW to be acquired will consist of 1.73 acre of forested land and 0.37 acre of residential land. The temporary ROW to be acquired will consist of residential land for driveway reconstruction. The typical ROW width from the SR 252 centerline to the north will be 35 feet with a maximum width of 45 feet. The typical ROW width from the centerline south will be 65 feet (Appendix B, pages B3, B19 and B20).

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Streams, Rivers, Watercourses & Jurisdictional Ditches	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Federal Wild and Scenic Rivers	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Natural, Scenic or Recreational Rivers	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Nationwide Rivers Inventory (NRI) listed	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Outstanding Rivers List for Indiana	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Navigable Waterways	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: Based on a desktop review, a site visit on June 24, 2019 by Lochmueller Group, the aerial map of the project area (Appendix B, page B3), the USGS topographic map (Appendix B, page B2), and the water resource map of the Red Flag Investigation (RFI) report (Appendix E, page E8) there are 10 streams, rivers, watercourses, and jurisdictional ditches located within the 0.5 mile search radius. There are four streams present within or adjacent to the project area.

A *Waters of the U.S. Determination* was approved by INDOT Ecology and Waterway Permitting Office (EWPO) on August 29, 2019 (Appendix F, pages F1 to F23). It was determined that there are four likely jurisdictional streams, UNT 1, UNT 2, UNT 3, and UNT 4 to East Fork Whitewater River, within the project area. None of these streams are listed as a Federal Wild and Scenic River, a State Natural, Scenic, and Recreational River or as an IDNR Outstanding River. The U.S. Army Corps of Engineers (USACE) makes all final determinations regarding jurisdiction.

UNT 1 to East Fork Whitewater River flows west within the survey area, south of SR 252. The ordinary high water mark (OHWM) was 11.1 feet wide by 1.3 feet deep. UNT 1 would likely be considered jurisdictional due to the hydrologic connectivity to Whitewater River, a traditionally navigable water (TNW), via East Fork Whitewater River. This stream is outside of the construction limits of the project and will not be impacted.

UNT 2 to East Fork Whitewater River flows south within the survey area, through a small structure under SR 252. The OHWM was 6.2 feet wide by 0.4 feet deep. UNT 2 would likely be considered jurisdictional due to the hydrologic connectivity to Whitewater River, a TNW, via East Fork Whitewater River. A total of 91 linear feet (0.01 acre) of UNT 2 will be impacted by replacing the small structure under SR 252, grading, and riprap placement.

UNT 3 to East Fork Whitewater River flows southeast within the survey area, through a small structure under SR 252. The OHWM was 4.2 feet wide by 0.7 feet deep. UNT 3 would likely be considered jurisdictional due to the hydrologic connectivity to Whitewater River, a TNW, via East Fork Whitewater River. A total of 101

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linear feet (0.01 acre below OHWM) of UNT 3 will be impacted by the replacement of the small structure under SR 252, grading activities, and riprap placement.

UNT 4 to East Fork Whitewater River flows southwest within the survey area, north of SR 252. The ordinary high water mark (OHWM) was 4.6 feet wide by 0.8 feet deep. UNT 4 would likely be considered jurisdictional due to the hydrologic connectivity to Whitewater River, a TNW, via East Fork Whitewater River. This stream is outside of the construction limits of the project and will not be impacted.

Due to total impacts of 192 linear feet (0.02 acre below OHWM) to UNT 2 and UNT 3 to East Fork Whitewater River, a USACE Section 404 Regional General Permit (RGP) and an IDEM Section 401 Water Quality Certification (WQC) will likely be required. Because impacts are below the 300 linear feet and/or 0.1 acre below OHWM threshold to require mitigation, mitigation for impacts to streams will not likely be required.

Early coordination information was sent to the U.S. Fish and Wildlife Service (USFWS), Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife (DFW), and the US Army Corps of Engineers (USACE) on July 18, 2019 (Appendix C, pages C1 to C4). The USACE did not respond to the early coordination letter.

The USFWS responded on July 30, 2019 with recommendations to avoid or minimize impacts to streams in the area (Appendix C, pages C14 to C15). Their recommendations include minimizing the use of riprap, implementing time of year restrictions for work in the waterway, and restricting the type of work that takes place below the low-water level of the streams. All applicable USFWS recommendations are included in the *Environmental Commitments* section of this document.

The IDNR DFW responded on August 16, 2019 with recommendations to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources to the greatest extent possible (Appendix C, pages C45 to C47). These recommendations include stream crossing type, maintaining the natural features of the stream as much as possible, minimizing the use of riprap, only using certain materials for erosion control, not placing riprap in the thalweg channel, limiting the use of cofferdams, not dewatering directly into the stream, implementing time of year restrictions on work in the streams, limiting excavation in the low-flow area of the stream, and minimizing the movement of resuspended sediment from the project area. All applicable IDNR DFW recommendations are included in the *Environmental Commitments* section of this document.

An automated letter was generated from the Indiana Department of Environmental Management (IDEM) website on July 18, 2019 (Appendix C, pages C5 to C9). Applicable recommendations from the Proposed Roadway Letter include coordinating with appropriate agencies with regards to stream impacts and limiting stream disturbance. All applicable IDEM recommendations are included as commitments in *Section J: Environmental Commitments* of this document

Other Surface Waters

Reservoirs

Lakes

Farm Ponds

Detention Basins

Storm Water Management Facilities

Other: _____

Presence

Impacts

Yes

No

Remarks:

Based on a desktop review, a site visit on June 24, 2019 by Lochmueller Group, the aerial map of the project area (Appendix B, page B3), the USGS topographic map (Appendix B, page B2), and the water resource map in the RFI (Appendix E, page E8), there are 6 lakes located within the 0.5 mile search radius. No other surface waters are present within the project area, therefore, no impacts are expected.

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A *Waters of the U.S. Determination Report* was approved by INDOT EWPO on August 29, 2019 (Appendix F, pages F1 to F23). It was determined that no other surface water resources exist within the project area. The USACE makes all final determinations regarding jurisdiction.

Early coordination letters were sent to the USFWS, the IDNR DFW, and the USACE on July 18, 2019 (Appendix C, pages C1 to C4). The USACE did not respond to the early coordination letter. The USFWS responded on July 30, 2019 and had no recommendations relating to other surface waters (Appendix C, pages C14 to C15). The IDNR responded on August 16, 2019 and had no recommendations relating to other surface waters (Appendix C, pages C45 to C47).

An automated letter was generated from the IDEM website on July 18, 2019 (Appendix C, pages C5 to C9). There were no recommendations applicable to other surface water resources within the letter.

	Presence	Impacts	
	<input type="checkbox"/>	Yes	No
Wetlands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0.0 acre(s) Total wetland area impacted: 0.0 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
N/A	N/A	N/A	N/A	N/A

	Documentation	ES Approval Dates
Wetlands (Mark all that apply)		
Wetland Determination	<input checked="" type="checkbox"/>	<u>August 29, 2019</u>
Wetland Delineation	<input type="checkbox"/>	<input type="text"/>
USACE Isolated Waters Determination	<input type="checkbox"/>	<input type="text"/>
Mitigation Plan	<input type="checkbox"/>	<input type="text"/>

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

Substantial adverse impacts to adjacent homes, business or other improved properties;	<input type="checkbox"/>
Substantially increased project costs;	<input type="checkbox"/>
Unique engineering, traffic, maintenance, or safety problems;	<input type="checkbox"/>
Substantial adverse social, economic, or environmental impacts, or	<input type="checkbox"/>
The project not meeting the identified needs.	<input type="checkbox"/>

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks: Based on a review of the National Wetlands Inventory (NWI) on-line mapper (fws.gov/wetlands/data/Mapper.html) (Appendix F, page F18), a site visit on June 24, 2019 by Lochmueller Group, the USGS topographic map (Appendix B, page B2), and the water resource map of the RFI report (Appendix E, page E8), there are eight wetlands located within the 0.5 mile search radius. No wetlands are present within or adjacent to the project area, therefore, no impacts are expected.

A *Waters of the U.S. Determination Report* was approved by INDOT EWPO on August 29, 2019 (Appendix F, pages F1 to F23). It was determined that there are no wetlands within or near the project area. The USACE makes all final determinations regarding jurisdiction.

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Early coordination letters were sent to the USFWS, the IDNR DFW, and the USACE on July 18, 2019 (Appendix C, pages C1 to C4). The USACE did not respond to the early coordination letter.

The USFWS responded on July 30, 2019 and had no recommendations relating to wetlands (Appendix C, pages C14 to C15).

The IDNR responded on August 16, 2019 with recommendations to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources to the greatest extent possible (Appendix C, pages C45 to C47). Their responses contained recommendations pertaining to wetland impacts; however, these recommendations do not apply to this project.

An automated letter was generated from the IDEM website on July 18, 2019 (Appendix C, pages C5 to C9). Any recommendations pertaining to wetland resources, are not applicable to this project.

	<u>Presence</u>	<u>Impacts</u>		
Terrestrial Habitat Unique or High Quality Habitat	<input checked="" type="checkbox"/>	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>	Use the remarks box to identify each

type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks: Based on a desktop review, a site visit on June 24, 2019 by Lochmueller Group, and the aerial map of the project area (Appendix B, page B3) there is upland forest and maintained roadside within the project area. Dominant vegetation within the project area includes Shumard's oak (*Quercus shumardii*), tall fescue (*Schedonorus arundinaceus*), and Kentucky bluegrass (*Poa pratensis*). The project will require approximately 1.2 acre of ground disturbance. The project will disturb approximately one acre of forest within 100 feet of the roadway and 0.2 acre of maintained roadside. Excavation will occur to replace the existing CMP culverts, remove the existing stone retaining wall, and reconstruct the roadway and will not exceed 10 feet. Drilling to a depth of 30 feet will also occur to install shafts for the new retaining wall. The avoidance of terrestrial habitat is not feasible as the project limits are required for the correction of the slide, replacement of the culverts, and reconstruction of the roadway which meets the purpose and need for the project, as detailed in the *Purpose and Need* section of this document.

Early coordination letters were sent to the USFWS and the IDNR DFW on July 18, 2019 (Appendix C, pages C1 to C4).

The USFWS responded on July 30, 2019 with recommendations to avoid or minimize impacts to any potential wildlife crossings (Appendix C, pages C14 to C15). All applicable USFWS recommendations are included in the *Environmental Commitments* section of this document.

The IDNR DFW responded on August 16, 2019 with recommendations to avoid or minimize impacts to potential bat habitat and any potential wildlife crossings within the project area (Appendix C, pages C45 to C47). All applicable IDNR DFW recommendations are included in the *Environmental Commitments* section of this document.

An automated letter was generated from the IDEM website on July 19, 2019 (Appendix C, pages C5 to C9). Applicable recommendations from the Proposed Roadway Letter include limiting disturbance of riparian vegetation, implementing erosion and sediment control measures, and coordinating with the appropriate permitting agency. All applicable IDEM recommendations are included as commitments in *Section J: Environmental Commitments* of this document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

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Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?

Yes

No

Are karst features located within or adjacent to the footprint of the proposed project?

If yes, will the project impact any of these karst features?

Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks:

Based on a desktop review, the proposed project is located outside the designated karst region of Indiana, as outlined in the October 13, 1993 Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, page B2) and the water resources map of the RFI report (Appendix E, page E8), there are no karst features identified within the project area. In the early coordination response, the Indiana Geological Survey (IGS) did not indicate that karst features may exist in the project area (Appendix C, pages C10 to C12). The IGS did identify high potential for liquefaction and slope instability in the area. Additionally, they identified low potential for bedrock and sand and gravel resources. This information was communicated with the designer on May 20, 2020. No impacts are expected.

Threatened or Endangered Species

Within the known range of any federal species

Any critical habitat identified within project area

Federal species found in project area (based upon informal consultation)

State species found in project area (based upon consultation with IDNR)

Presence

X

Impacts

Yes	No
X	

Is Section 7 formal consultation required for this action?

Yes

No

Remarks:

Based on a desktop review and the RFI report, completed by Lochmueller Group on June 17, 2019, the IDNR Franklin County Endangered, Threatened, and Rare Species List has been checked and is included in Appendix E, page E11. The highlighted species on the list reflect the federal and state identified ETR species located within the county. According to the IDNR DFW early coordination response dated August 16, 2019 (Appendix C, pages C39 to C41), the Natural Heritage Program's Database has been checked. To date, no plant or animal species listed as state or federally threatened, endangered or rare have been reported to occur in the project vicinity.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C45 to C47). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were found within or adjacent to the project area other than the Indiana bat and northern long-eared bat.

The project qualifies for the Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB), dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on July 26, 2019, and based on the responses provided, the project was found to "Not Likely to Adversely Affect" the Indiana bat and/or the NLEB. INDOT reviewed and verified the effect finding on July 29, 2019, and requested USFWS's review of the finding (Appendix C, pages C22 to C37). No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Avoidance and Mitigation Measures (AMMs) are included as firm commitments in the Environmental Commitments section of this document.

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This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at this site becomes available, or if the project plans are changed, USFWS will be contacted for consultation.

SECTION B – OTHER RESOURCES

Drinking Water Resources

- Wellhead Protection Area
- Public Water System(s)
- Residential Well(s)
- Source Water Protection Area(s)
- Sole Source Aquifer (SSA)

Presence

Impacts

Yes	No

If a SSA is present, answer the following:

- Is the Project in the St. Joseph Aquifer System?
- Is the FHWA/EPA SSA MOU Applicable?
- Initial Groundwater Assessment Required?
- Detailed Groundwater Assessment Required?

Yes

No

Remarks:

The project is located in Franklin County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the *FHWA/United States Environmental Protection Agency Sole Source Aquifer Memorandum of Understanding* is not applicable to this project. No impacts are expected.

The IDEM Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on July 18, 2019 by Lochmueller Group. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

The IDNR Water Well Web Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on July 19, 2019 by Lochmueller Group. No wells are located near the project area. Therefore, no impacts are anticipated.

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by Lochmueller Group on July 19, 2019 and the RFI report (Appendix E, page E3); this project is not located within an Urban Area Boundary location. No impacts are expected.

Based on a desktop review, a site visit on June 24, 2019 by Lochmueller Group, the aerial map of the project area (Appendix B, page B3), and the preliminary design plans, no public water systems were identified. Therefore, no impacts are expected.

Flood Plains

- Longitudinal Encroachment
- Transverse Encroachment
- Project located within a regulated floodplain
- Homes located in floodplain within 1000' up/downstream from project

Presence

Impacts

Yes	No

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

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Remarks: The IDNR Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on July 17, 2019 by Lochmueller Group. This project is not located in a regulatory floodplain from approved IDNR floodplain maps (Appendix F, page F19). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CRF 771, and/or 44 CFR. No impacts are expected.

Farmland	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006* N/A
**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks: Based on a desktop review, a site visit on June 24, 2019 by Lochmueller Group, the aerial map of the project area (Appendix B, page B3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on July 18, 2019, to Natural Resources Conservation Service (NRCS). The NRCS responded on December 30, 2019 and stated that the project will not cause a conversion of prime farmland (Appendix C, page C48).

SECTION C – CULTURAL RESOURCES

	<u>Category</u>	<u>Type</u>	<u>INDOT Approval Dates</u>	<u>N/A</u>
Minor Projects PA Clearance	A	9	July 5, 2019	<input type="checkbox"/>
	B	9		

**Eligible and/or Listed
Resource Present**

Results of Research

Archaeology	<input type="checkbox"/>
NRHP Buildings/Site(s)	<input type="checkbox"/>
NRHP District(s)	<input type="checkbox"/>
NRHP Bridge(s)	<input type="checkbox"/>

Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

**Documentation
Prepared**

Documentation (mark all that apply)

	<u>ES/FHWA Approval Date(s)</u>	<u>SHPO Approval Date(s)</u>
Historic Properties Short Report	<input type="checkbox"/>	<input type="checkbox"/>
Historic Property Report	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Records Check/ Review	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ia Survey Report	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ic Survey Report	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase II Investigation Report	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase III Data Recovery	<input type="checkbox"/>	<input type="checkbox"/>
APE, Eligibility and Effect Determination	<input type="checkbox"/>	<input type="checkbox"/>
800.11 Documentation	<input type="checkbox"/>	<input type="checkbox"/>

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Memorandum of Agreement (MOA)

MOA Signature Dates (List all signatories)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks:

On July 5, 2019, the INDOT Cultural Resources Office (CRO) determined that this project falls within the guidelines of Category A, Type 9 and Category B, Type 9 under the Minor Projects Programmatic Agreement (MPPA) (Appendix D, pages D1 to D7). Category A-9 covers erosion control measures and category B-9 covers installation, replacement, repair, lining or extension of culverts and other drainage structures.

There is a pipe that begins at a stone head wall north of SR 252 and protrudes from a stone retaining wall south of SR 252. According to research done by Weintraut & Associates, the stone retaining wall south of SR 252 is of mid to late twentieth century construction. The stone head wall on the north side of SR 252 appears to be of late nineteenth or early twentieth century construction. The culvert that conveys drainage through these head walls lacks integrity and/or a context that suggests it might have engineering or historical significance. Therefore, this aspect of the project qualifies under MPPA Category B-9.

Since all project work will occur within previously disturbed soils or on a slope that is too steep to contain archaeological deposits, no archaeological investigation was performed. No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

- Publicly owned park
- Publicly owned recreation area
- Other (school, state/national forest, bikeway, etc.)

Presence

Use

Yes	No

Evaluations Prepared

- Programmatic Section 4(f)*
- "De minimis" Impact*
- Individual Section 4(f)

FHWA Approval date

Wildlife & Waterfowl Refuges

- National Wildlife Refuge
- National Natural Landmark
- State Wildlife Area
- State Nature Preserve

Presence

Use

Yes	No

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**Evaluations
Prepared**

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

**FHWA
Approval date**

Historic Properties

Sites eligible and/or listed on the NRHP

Presence

Use

Yes No

<input style="width: 40px; height: 15px;" type="text"/>		<input style="width: 40px; height: 15px;" type="text"/>
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**Evaluations
Prepared**

Programmatic Section 4(f)*
"De minimis" Impact*
Individual Section 4(f)

**FHWA
Approval date**

**FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.*

Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities, unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreational areas, wildlife/waterfowl refuges, and NRHP eligible or listed historical properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, a site visit on June 24, 2019 by Lochmueller Group, the aerial map of the project area (Appendix B, page B3), and the RFI report (Appendix E, page E7), there are no Section 4(f) resources within or adjacent to the project area. Therefore, no impacts are expected.

Section 6(f) Involvement

Presence

Use

Section 6(f) Property

Yes No

<input style="width: 30px; height: 15px;" type="text"/>		<input style="width: 30px; height: 15px;" type="text"/>
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Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF) which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of Section 6(f) property list on the INDOT ES website (<https://www.in.gov/indot/2523.htm>) revealed a total of six properties in Franklin County (Appendix J, page J1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to Section 6(f) resources as a result of this project.

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SECTION E – Air Quality

Air Quality

Conformity Status of the Project

Is the project in an air quality non-attainment or maintenance area?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

If YES, then:

Is the project in the most current MPO TIP?

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

Is the project exempt from conformity?

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

If the project is NOT exempt from conformity, then:

Is the project in the Transportation Plan (TP)?

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

Is a hot spot analysis required (CO/PM)?

<input type="checkbox"/>	<input type="checkbox"/>
--------------------------	--------------------------

Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Remarks:

The FY 2020-2024 STIP is listed based on the lead Des. No. in the contract. The lead Des. No. for this contract is 1700195. The FY 2020-2024 STIP includes Des. No. 1700194 by reference with the contract number R-40431 (Appendix H, page H1).

This project is located within Franklin County, which is currently in attainment for all criteria pollutants according to the IDEM website (<https://www.in.gov/idem/airquality/2339.htm>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION F – NOISE

Noise

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

No	Yes/ Date
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ES Review of Noise Analysis		
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Remarks:

This is a Type III project. In accordance with 23 CFR 772 and the current *INDOT Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

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SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

- Will the proposed action comply with the local/regional development patterns for the area?
- Will the proposed action result in substantial impacts to community cohesion?
- Will the proposed action result in substantial impacts to local tax base or property values?
- Will construction activities impact community events (festivals, fairs, etc.)?
- Does the community have an approved transition plan?
- If No, are steps being made to advance the community's transition plan?
- Does the project comply with the transition plan? (explain in the remarks box)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Remarks:

The project will ultimately be beneficial to local businesses and properties due to improvements of deteriorating roadway condition and it will not substantially change access to properties within the area. Overall, the negative impacts to property owners within the project area will be minimal and will consist primarily of short-term construction impacts. No relocations are expected. Property owners will be provided access throughout the duration of the project to reduce impacts as much as possible. The project is not anticipated to result in substantial impacts to community cohesion, because it will not change access to properties within the area. The project is not expected to impact the surrounding community or cause economic impacts to the surrounding area. Therefore, this project will have minimal or no negative impacts to the community or local economy.

According to the Fairs and Festivals website (www.indianafestivals.org), accessed on July 22, 2019 by Lochmueller Group, there are 13 fairs and festivals scheduled within 10 miles of the project. The project will close SR 252 and utilize a detour during construction, which will last approximately 6 months (Appendix B, page B20). Although a detour will be required and traffic delays may occur, this project should not adversely affect access to these festivals.

The MOT may pose delays and temporary inconveniences to traveling motorists (including school buses and emergency services); however, all inconveniences will cease upon project completion. The MOT for the project is not anticipated to impact access to community events. The project sponsor will be responsible for contacting school districts and emergency services at least two weeks prior to any construction activities that would limit access, this is included as a commitment in *Section J: Environmental Commitments*.

The Americans with Disabilities Act (ADA) Transition Plan for Franklin County, Indiana was approved and implemented on October 9, 2018. The project will comply with the published ADA Transition Plan and will not create any additional barriers to access. No existing pedestrian facilities exist within the project area. Therefore, no pedestrian facilities will be impacted by the project.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Indirect impacts are effects which are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions regardless of what agency or person undertakes such other actions.

The purpose of the project is to correct existing roadway condition deficiencies resulting from landslide activity. As such, the project will not add capacity to the existing roadway network or provide additional access to any currently undeveloped area. Therefore, the project is not expected to increase development in the area or result in substantial indirect or cumulative impacts.

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Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Based on a desktop review, a site visit on June 24, 2019 by Lochmueller Group, the aerial map of the project area (Appendix B, page B3), and the RFI report (Appendix E, page E7) there are five public facilities located within 0.5 mile of the project area. No public facilities are within or adjacent to the project area. Access to all properties will be maintained during construction. Therefore, no impacts are expected.

Early coordination information was sent to Franklin County Community School Corporation, Franklin County Board of Commissioners, Franklin County Council, Franklin County Highway Department, and Franklin County Surveyor's Office on July 18, 2019. No response was received from these agencies.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Under FHWA Order 6640.23A, FHWA and INDOT, as recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT *Categorical Exclusion Preparation Manual*, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional ROW. This project will require 2.10 acres of new permanent ROW and approximately 0.02 acre of temporary ROW. Therefore, an EJ analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exist and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city, or town and is called the community of comparison (COC). In this project, the COC is Brookville Township. The community that overlaps the project limits is called the affected community (AC). In this project, the AC is Census Tract 9697.

An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the American Community Survey five-year estimates data (2014-2018) was obtained from the U.S. Census Bureau website (<https://data.census.gov/>) on May 17, 2020 by Lochmueller Group. The data collected for minority and low-income populations within the AC are summarized in the table below.

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	COC	AC 1
	Brookville Township, Franklin County, Indiana	Census Tract 9697
LOW-INCOME POPULATION		
Total Population for Whom Poverty Status is Determined	5,617	2,504
Total Population Below Poverty Level	456	347
Percent Low-Income	8.1%	13.9%
125 Percent of COC	10.1%	
AC Percent Low-Income Greater Than 125 Percent of COC?		Yes
AC Percent Low-Income Greater Than 50 Percent?		No
Population of EJ Concern?		Yes
MINORITY POPULATION		
Total Population	5,698	2,504
Minority Population	59	34
Percent Minority	1.04%	1.36%
125 Percent of COC	1.29%	
AC Percent Minority Greater Than 125 Percent of COC?		Yes
AC Percent Minority Greater Than 50 Percent?		No
Population of EJ Concern?		Yes

The AC, Census Tract 9697 has a percent low-income of 13.9% which is below 50% but is above the 125% COC threshold. Therefore, the AC is a low-income population of EJ concern.

The AC, Census Tract 9697 has a percent minority of 1.36% which is below 50% but is above the 125% COC threshold. Therefore, the AC is a minority population of EJ concern.

Overall, the negative impacts to property owners within the project area will be minimal and consist primarily of short-term construction impacts and the loss of a strip of ROW. No relocations are anticipated. The ROW to be acquired will not substantially diminish the existing use of the affected property owners. The MOT during construction will close SR 252 and a detour route will be established. Property owners will be provided access throughout the duration of the project to reduce impacts as much as possible. No permanent impacts to community cohesion are anticipated. Long-term impacts from the project to any EJ community in this area will be beneficial due to the improved safety of travel along SR 252 at this location. It is expected the project will not have a disproportionately high and adverse environmental or health impact to low-income or minority populations of EJ concern when compared to non EJ populations. The EJ analysis along with maps and data were submitted to INDOT Environmental Services on May 18, 2020 and they concurred that the project will not have a disproportionately high and adverse impact to EJ populations (Appendix I, pages I1 to I10).

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Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
 Is a Business Information Survey (BIS) required?
 Is a Conceptual Stage Relocation Study (CSRS) required?
 Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, businesses, or farms will take place as a result of this project.

Utility coordination has begun and will continue through project development to ensure impacts to utilities in the area are minimal.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation X
 Phase I Environmental Site Assessment (Phase I ESA)
 Phase II Environmental Site Assessment (Phase II ESA)
 Design/Specifications for Remediation required?

Documentation

	No	Yes/ Date
ES Review of Investigations		August 16, 2019

Include a summary of findings for each investigation.

Remarks: Based on a review of GIS and available public records, a RFI was completed on June 17, 2019 by Lochmueller Group and approved on August 16, 2019 by INDOT Site Assessment and Management (Appendix E, pages E1 to E11). One underground storage tank (UST) site is located within the 0.5 mile radius of the project area. No sites are within the project area. The UST site is 0.35 mile from the project area. No impacts are expected because of distance. Further investigation for hazardous material concerns is not required at this time.

SECTION I – PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Individual Permit (IP)
 Nationwide Permit (NWP)
 Regional General Permit (RGP) X
 Pre-Construction Notification (PCN)
 Other
 Wetland Mitigation required
 Stream Mitigation required

IDEM

Section 401 WQC X
 Isolated Wetlands determination
 Rule 5 X
 Other

This is page 21 of 24 Project name: SR 252 Slide Correction Project Date: July 14, 2020

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IDNR Wetland Mitigation required Stream Mitigation required Construction in a Floodway Navigable Waterway Permit Lake Preservation Permit Other Mitigation Required US Coast Guard Section 9 Bridge Permit Others (Please discuss in the remarks box below)	<table border="1" style="width: 50px; height: 100px; border-collapse: collapse;"> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> <tr><td style="width: 50px; height: 20px;"></td></tr> </table>										

Remarks: Due to total impacts of 192 linear feet (0.02 acre below OHWM) to UNT 2 and UNT 3 to East Fork Whitewater River, a USACE Section 404 RGP and an IDEM Section 401 WQC will likely be required. Because impacts are below the 300 linear feet and/or 0.1 acre below OWHM threshold to require mitigation, mitigation for impacts to streams will not likely be required.

The project may disturb up to 1.2 acres of land. Therefore, the project is expected to exceed the minimal guidelines of soil disturbance and an IDEM Rule 5 Notice of Intent will be required.

Applicable recommendations provided by the permitting agencies are included in the Environmental Commitments section of this document. If any permit is found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

SECTION J- ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s), and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks: **Firm:**

1. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
2. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT ES and INDOT Seymour District Environmental Services Division will be contacted immediately. (INDOT ESD)
3. Delineate all environmental resources, UNT 1 to UNT 4 to Whitewater River, on the plans. Beyond construction limits, mark these resources as "Do Not Disturb". If additional impacts are required beyond those considered in the environmental document, additional documentation or permits may be needed. (INDOT ESD)
4. USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after June 24, 2021, an inspection of the structures by a qualified individual, must be performed. Inspection of the structures should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. (INDOT Seymour District)
5. Coordination will occur with the Ohio Department of Transportation regarding the usage of Ohio SR 128 and Ohio SR 126 as part of the detour for this project. (INDOT Seymour District)
6. **General AMM 1:** Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
7. **Lighting AMM 1:** Direct temporary lighting away from suitable habitat during the active season. (USFWS)

Indiana Department of Transportation

County Franklin

Route SR 252

Des. No. 1700194

8. **Tree Removal AMM 1:** Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
9. **Tree Removal AMM 2:** Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed (no tree clearing from April 1 to September 30). (USFWS)
10. **Tree Removal AMM 3:** Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
11. **Tree Removal AMM 4:** Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year. (USFWS)

For Further Consideration:

1. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS)
2. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottomed culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community. (USFWS)
3. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If rip rap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
4. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)
5. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS)
6. If box or pipe culverts are used, the bottoms should be buried to a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the bankful width); maintain the natural stream substrate within the structure; have a minimum openness ratio (height x width/length) of 0.25; and have stream depth and water velocities during low-flow conditions that are approximate to those in the natural stream channel. (IDNR DFW)
7. The new, replacement, or rehabbed structure should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. (IDNR DFW)
8. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Where riprap must be used, we recommend placing only enough riprap to provide stream bank toe protection, such as from the toe of the bank up to the OHWM. The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to the area and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. (IDNR DFW)
9. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting from April 1 through September 30. (IDNR DFW)
10. Do not construct any temporary runarounds or causeways. (IDNR DFW)

This is page 23 of 24 Project name: SR 252 Slide Correction Project

Date: July 14, 2020

Indiana Department of Transportation

County Franklin

Route SR 252

Des. No. 1700194

11. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure. (IDNR DFW)
12. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR DFW)

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks: Early coordination with the regulatory agencies was completed on July 18, 2019 (Appendix C, pages C1 to C4). If no response was received, it was assumed the agency did not feel the project will result in substantial impacts. The following agencies/individuals were contacted during the coordination phase.

	Agency	Date of Response(s)
1.	USACE, Louisville District	No Response
2.	USFWS, Bloomington Field Office	July 30, 2019; July 29, 2019; May 17, 2020
3.	USDA, NRCS	December 30, 2019
4.	National Park Service, Midwest Regional Office	No Response
5.	U.S. Department of Housing and Urban Development	No Response
7.	IDNR, Division of Fish and Wildlife	August 16, 2019
8.	Indiana Geological Survey	July 18, 2019
9.	Indiana Department of Environmental Management	July 19, 2019
10.	INDOT, Office of Public Involvement	July 22, 2019
11.	INDOT, Environmental Services	August 8, 2019
12.	Franklin County Board of Commissioners	No Response
13.	Franklin County Council	No Response
14.	Franklin County Highway Department	No Response
15.	Franklin County Surveyor's Office	No Response
16.	Franklin County Emergency Management Agency	No Response
17.	Franklin County Community School Corporation	No Response
18.	Franklin County Sheriff's Department	No Response
19.	Franklin County EMS	No Response
20.	Brookville Township Trustee	No Response
21.	Brookville Town Board	No Response
22.	Brookville Police Department	No Response
23.	Brookville Fire Department	No Response

Appendix A: INDOT Supporting Documentation

Threshold Chart.....A1

Appendix B: Graphics

General Location Map.....B1
 USGS Brookville & Whitcomb, Indiana Quadrangle Topographic Map.....B2
 Project Aerial (2017).....B3
 Photo Location Map.....B4
 Site Photographs.....B5-B13
 Preliminary Plan Sheets.....B14-B23

Appendix C: Early Coordination

Sample Early Coordination Letter (July 18, 2019).....C1-C4
 Indiana Department of Environmental Management
 Electronic Response (July 18, 2019).....C5-C9
 Indiana Geological Survey
 Electronic Response (July 18, 2019).....C10-C12
 Indiana Department of Transportation (INDOT), Office of Public Involvement
 Response Email (July 22, 2019).....C13
 United States Fish and Wildlife Service
 Response Email (July 30, 2019).....C14-C15
 IPaC Official Species List (May 15, 2020).....C16-C21
 IPaC Concurrence Verification Letter (July 29, 2019).....C22-C37
 Bridge/Structure Assessment Forms (June 24, 2019).....C38-C43
 INDOT, Environmental Services
 Response Letter (August 8, 2019).....C44
 Indiana Department of Natural Resources (IDNR), Division of Fish and Wildlife
 Response Letter (August 16, 2019).....C45-C47
 Natural Resources Conservation Service
 Response Letter (December 30, 2019).....C48

Appendix D: Section 106 of the National Historic Preservation Act (NHPA)

MPPA Project Assessment FormD1-D5

Appendix E: Red Flag Investigation

Red Flag Investigation.....E1-E11

Appendix F: Water Resources

Waters of the U.S. Determination Report.....F1-F8
 Water Resources Map.....F9-F11
 Soil Survey.....F12-F16
 USGS StreamStats Map.....F17
 NWI Wetlands Map.....F18
 FEMA Floodplain Map.....F19
 Preliminary Jurisdictional Determination.....F20-F23

Appendix G: Public Involvement

Notice of Survey.....G1
 INDOT Notice of Survey Attachment.....G2

Appendix H: Air Quality

Relevant pages from the INDOT 2020-2024 STIPH1

Appendix I: Environmental Justice (EJ) Analysis

EJ Analysis.....I1-I9
 INDOT Concurrence Email (May 18, 2020).....I10

Appendix J: Additional Information

Land and Water Conservation Fund Grants: Franklin County, Indiana J1

Categorical Exclusion

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	“No Effect”, “Not likely to Adversely Affect” (Without AMMs ⁴ or with AMMs required for all projects ⁵)	“Not likely to Adversely Affect” (With any other AMMs)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	“No Effect”, “Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level <ul style="list-style-type: none"> • District Env. Supervisor • Env. Services Division • FHWA 	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes Yes	Yes Yes Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

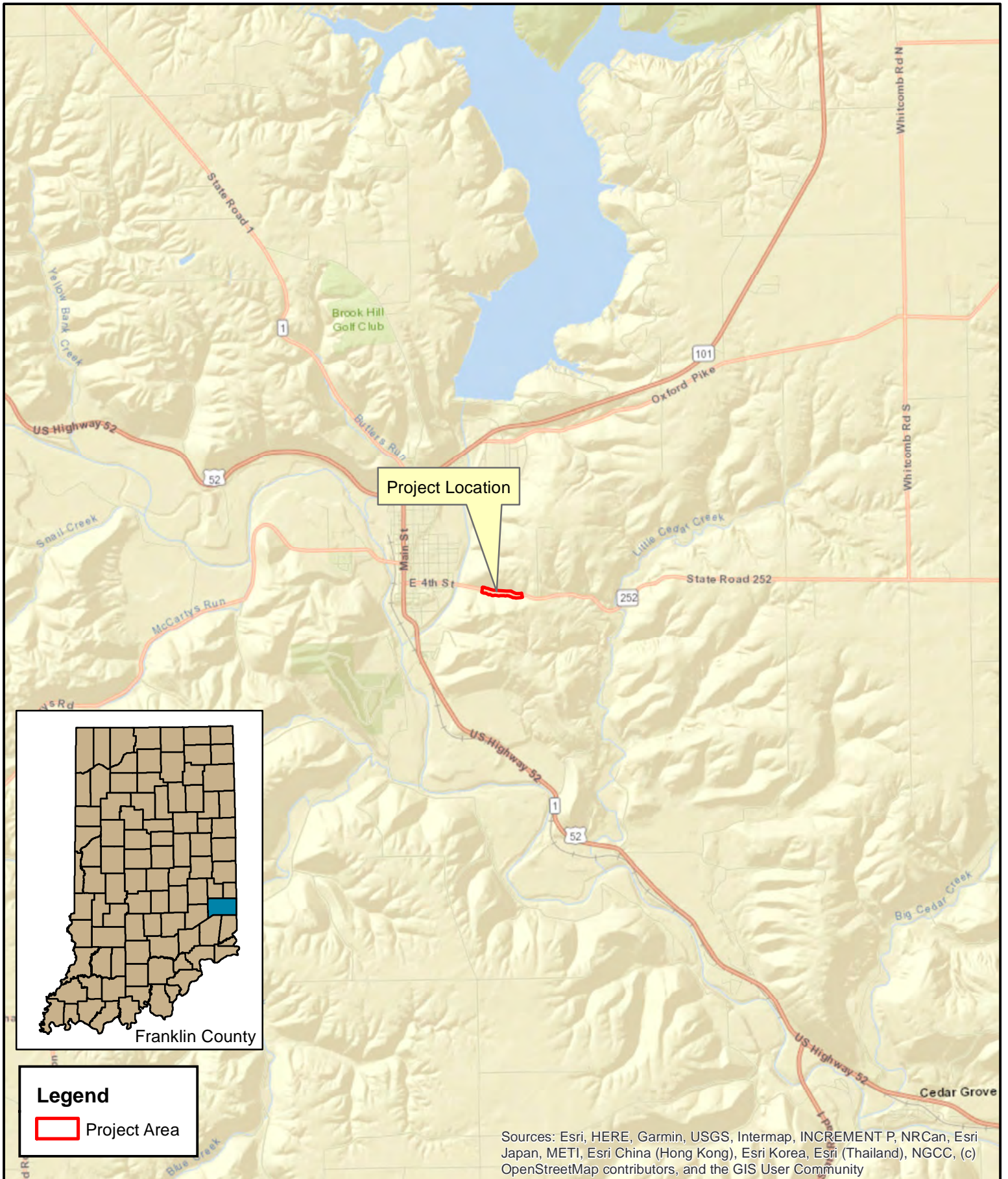
⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User's Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as “required for all projects”.

⁶Potential for causing a disproportionately high and adverse impact.

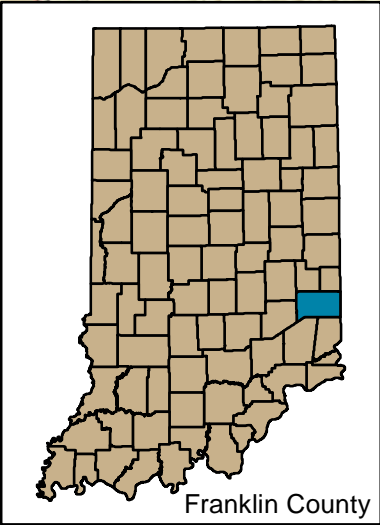
⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

Categorical Exclusion
Appendix B
Graphics



Project Location



Legend

Project Area

Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, (c) OpenStreetMap contributors, and the GIS User Community

LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

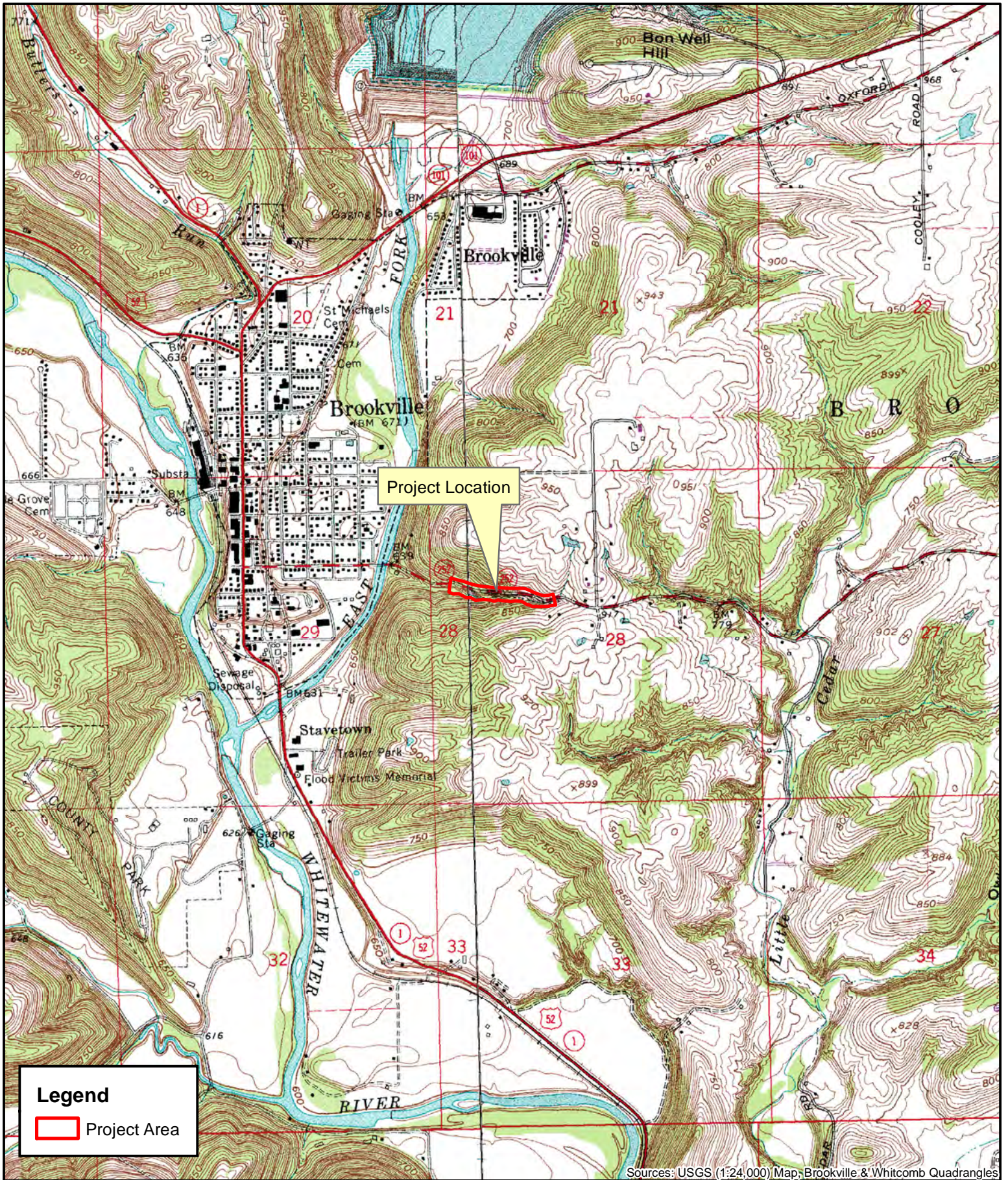
General Location Map
 Des. No. 1700194



County: Franklin
 Township: Brookville
 State: Indiana

SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 7/10/2019, C. Kunkel

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Legend

Project Area

Sources: USGS (1:24,000) Map: Brookville & Whitcomb Quadrangles

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3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

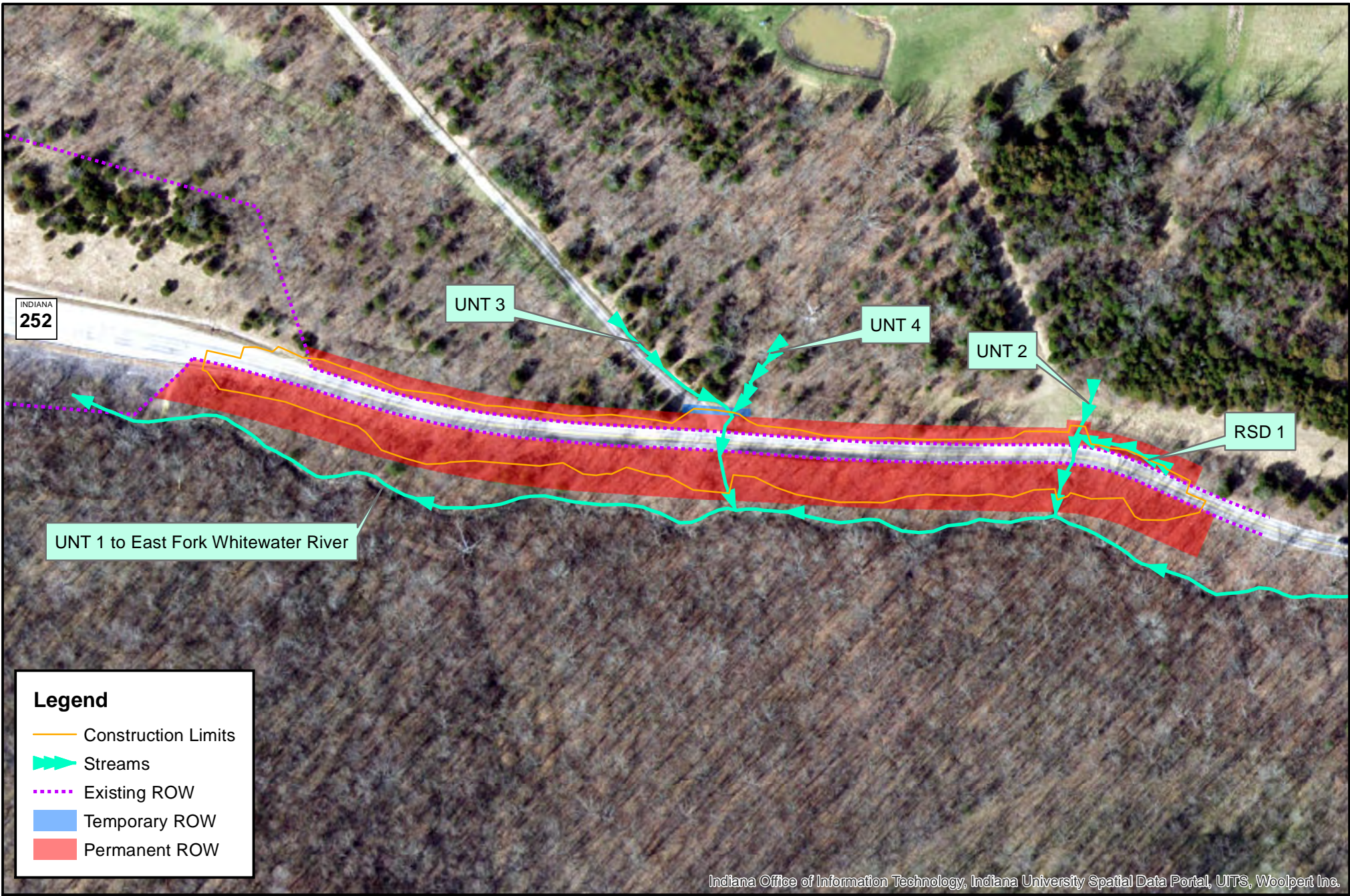
USGS Quadrangle Map
 Brookville & Whitcomb Quadrangles
 Des. No. 1700194

0 0.25 0.5
 Miles

County: Franklin
 Township: Brookville
 State: Indiana

SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 7/10/2019, C. Kunkel

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Legend

- Construction Limits
- ▶▶▶ Streams
- ⋯ Existing ROW
- Temporary ROW
- Permanent ROW

Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.

LOCHMUELLER GROUP
 3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

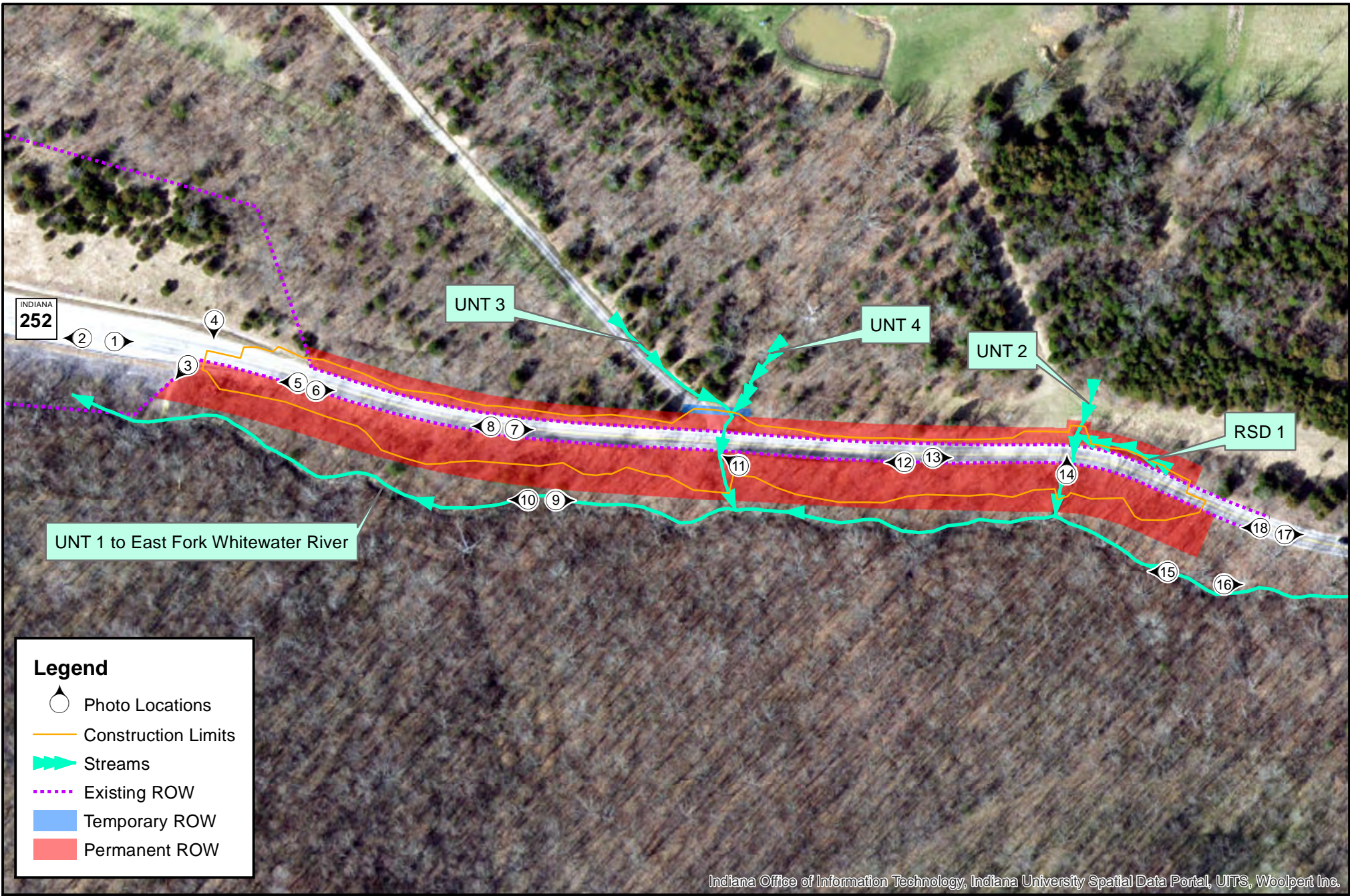
Aerial Map (2017)
 Des. No. 1700194

0 250 500
 Feet

County: Franklin
 Township: Brookville
 State: Indiana

SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 5/22/2020, C. Kunkel

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Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.

LOCHMUELLER GROUP
 3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

Photo Location Map
 Des. No. 1700194

0 250 500 Feet

N

County: Franklin
 Township: Brookville
 State: Indiana

SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 5/22/2020, C. Kunkel

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1. Looking east along SR 252



2. Looking west along SR 252



3. Looking southwest at culvert from SR 252



4. Looking south at culvert under SR 252



5. Looking west along SR 252



6. Looking east along SR 252



7. Looking east along SR 252



8. Looking west along SR 252



9. Looking east upstream along UNT 1 to East Fork Whitewater River



10. Looking west downstream along UNT 1 to East Fork Whitewater River



11. Looking northwest at culvert conveying UNT 3 to East Fork Whitewater River



12. Looking west along SR 252



13. Looking east along SR 252



14. Looking north at culvert conveying UNT 2 to East Fork Whitewater River



15. Looking west downstream UNT 1 to East Fork Whitewater River



16. Looking east upstream UNT 1 to East Fork Whitewater River



17. Looking east along SR 252



18. Looking west along SR 252



19. Looking west along SR 252



20. Looking east along SR 252

PROJECT	DESIGNATION
1700194	1700194
CONTRACT	BRIDGE FILE
R-40431	N/A

INDIANA DEPARTMENT OF TRANSPORTATION

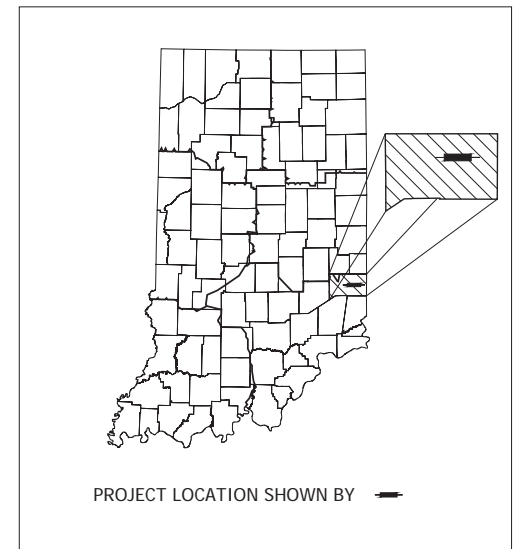


ROAD PLANS

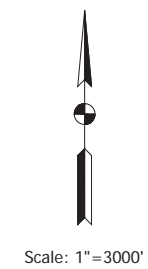
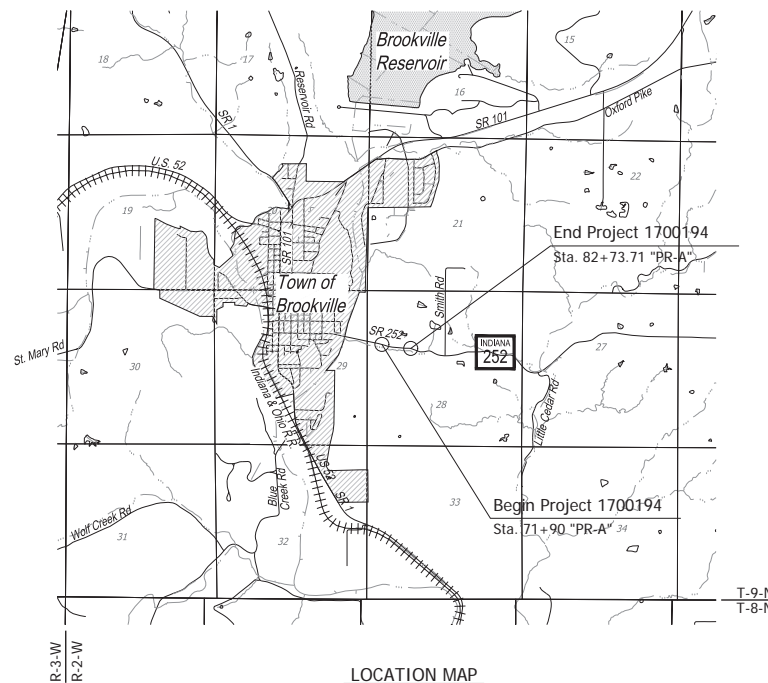
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PROJECT NO. 1700194 P.E.
 PROJECT NO. 1700194 R/W
 PROJECT NO. 1700194 CONST.

TRAFFIC DATA	
A.A.D.T. (2020)	1,195 V.P.D.
A.A.D.T. (2040)	1,195 V.P.D.
D.H.V (2040)	105 V.P.H.
DIRECTIONAL DISTRIBUTION	48 %
TRUCKS	3.10% A.A.D.T. 2.83% D.H.V.
DESIGN DATA	
DESIGN SPEED	55 M.P.H.
PROJECT DESIGN CRITERIA	3R (Non-Freeway)
FUNCTIONAL CLASSIFICATION	Major Collector
RURAL/URBAN	Rural
TERRAIN	Rolling
ACCESS CONTROL	None



Slide Correction Project is Located Along S.R. 252, Approximately 0.8 Miles East of U.S 52.



BRIDGE LENGTH:	N/A	MI.
ROADWAY LENGTH:	0.203	MI.
TOTAL LENGTH:	0.203	MI.
MAX. GRADE:	9.397	%
HYDROLOGIC UNIT CODE:	05080003070180	
	East Fork Whitewater R-Wolf Creek	

BEGINNING	LATITUDE: 39° 25' 05" N	LONGITUDE: 85° 00' 01" W
ENDING	LATITUDE: 39° 25' 03" N	LONGITUDE: 85° 00' 02" W

PRELIMINARY PLANS
5/14/2020

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS

Plans Prepared By:

LOCHMUELLER GROUP
6200 Vogel Road
Evansville, Indiana 47715
Phone: 812.479.6200
Toll Free: 800.423.7411

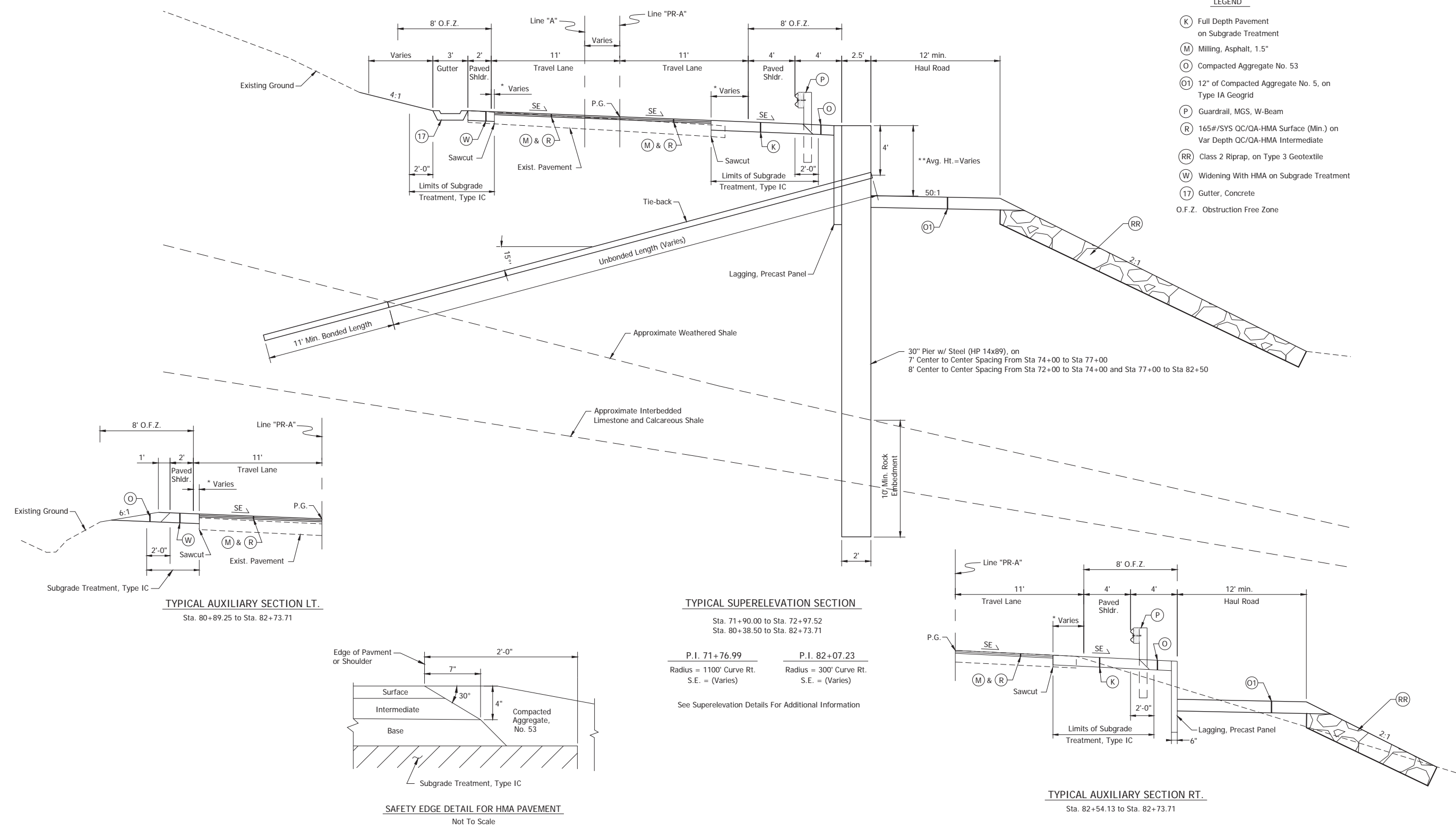
PLANS PREPARED BY:	LOCHMUELLER GROUP, INC.	(812) 479-6200
	6200 Vogel Road, Evansville, IN 47715	PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

	BRIDGE FILE
	DESIGNATION
	1700194
	SHEETS
SURVEY BOOK	1 of 15
ELECTRONIC	
CONTRACT	PROJECT
R-40431	1700194

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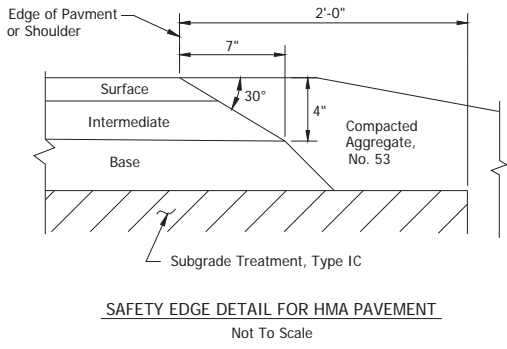
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- (M) Milling, Asphalt, 1.5"
- (O) Compacted Aggregate No. 53
- (O1) 12" of Compacted Aggregate No. 5, on Type IA Geogrid
- (P) Guardrail, MGS, W-Beam
- (R) 165#/SYS QC/QA-HMA Surface (Min.) on Var Depth QC/QA-HMA Intermediate
- (RR) Class 2 Riprap, on Type 3 Geotextile
- (W) Widening With HMA on Subgrade Treatment
- (17) Gutter, Concrete
- O.F.Z. Obstruction Free Zone



TYPICAL AUXILIARY SECTION LT.
Sta. 80+89.25 to Sta. 82+73.71

TYPICAL SUPERELEVATION SECTION
Sta. 71+90.00 to Sta. 72+97.52
Sta. 80+38.50 to Sta. 82+73.71

TYPICAL AUXILIARY SECTION RT.
Sta. 82+54.13 to Sta. 82+73.71

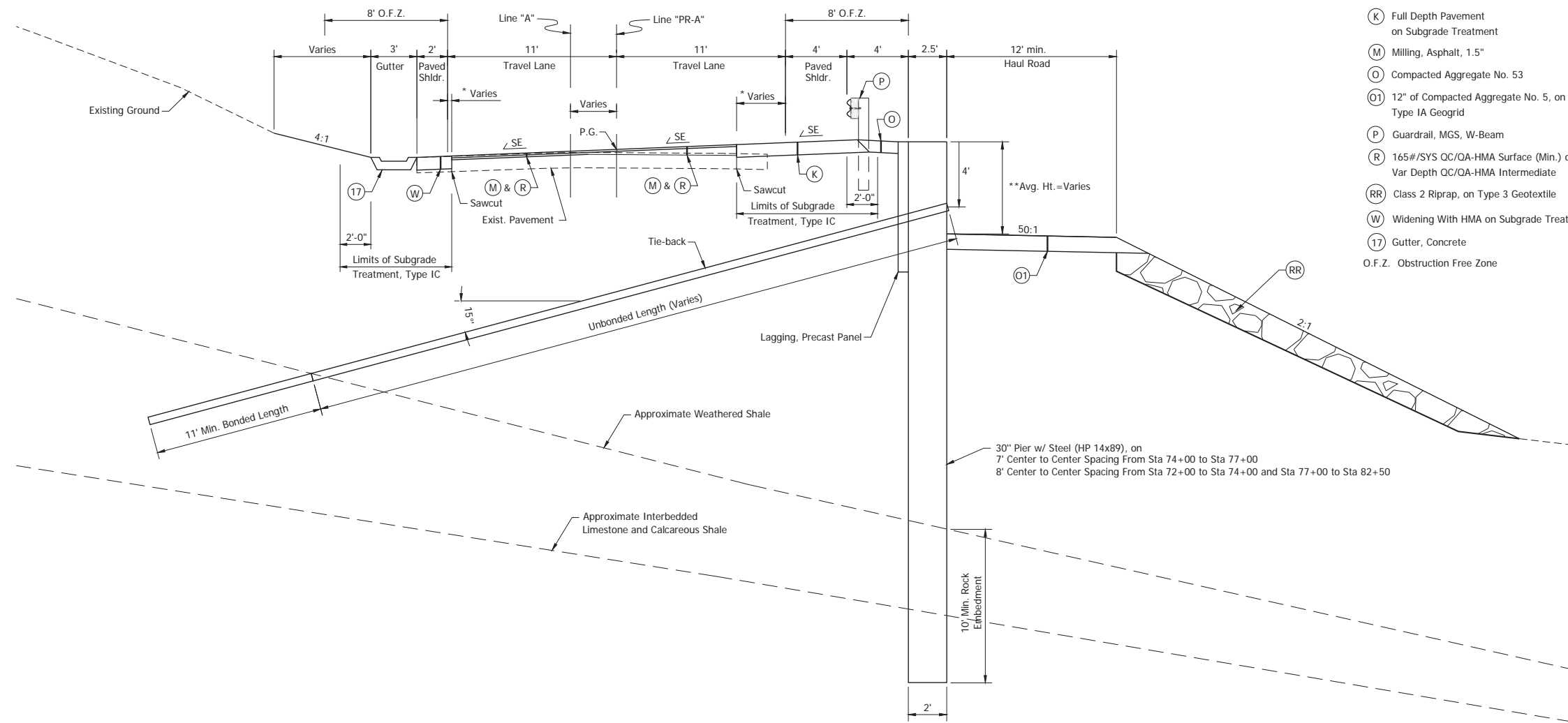


P.I. 71+76.99 P.I. 82+07.23
Radius = 1100' Curve Rt. Radius = 300' Curve Rt.
S.E. = (Varies) S.E. = (Varies)

See Superelevation Details For Additional Information

Date: May 14, 2020, 4:07pm User Name: eolson
File: S:_20181218-0341Road\CAD\Misc\DWG\Typical Section.dwg

RECOMMENDED FOR APPROVAL _____ DATE _____ DESIGN ENGINEER		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1"=5'	BRIDGE FILE
DESIGNED: _____ NAW		DRAWN: _____ CCW		VERTICAL SCALE	DESIGNATION 1700194
CHECKED: _____ JAW		CHECKED: _____ NAW		SURVEY BOOK	SHEETS 3 of 15
		TYPICAL CROSS SECTION LINE "PR-A"		CONTRACT R-40431	PROJECT 1700194



- LEGEND**
- (K) Full Depth Pavement on Subgrade Treatment
 - (M) Milling, Asphalt, 1.5"
 - (O) Compacted Aggregate No. 53
 - (O1) 12" of Compacted Aggregate No. 5, on Type IA Geogrid
 - (P) Guardrail, MGS, W-Beam
 - (R) 165#/SYS OC/OA-HMA Surface (Min.) on Var Depth OC/OA-HMA Intermediate
 - (RR) Class 2 Riprap, on Type 3 Geotextile
 - (W) Widening With HMA on Subgrade Treatment
 - (17) Gutter, Concrete
 - O.F.Z. Obstruction Free Zone

TYPICAL SUPERELEVATION SECTION

Sta. 72+97.52 to Sta. 80+38.50

P.I. 74+66.60	P.I. 79+36.42
Radius = 1100' Curve Lt.	Radius = 6000' Curve Lt.
S.E. = 4.0%	S.E. = 2.4%

See Superelevation Details For Additional Information

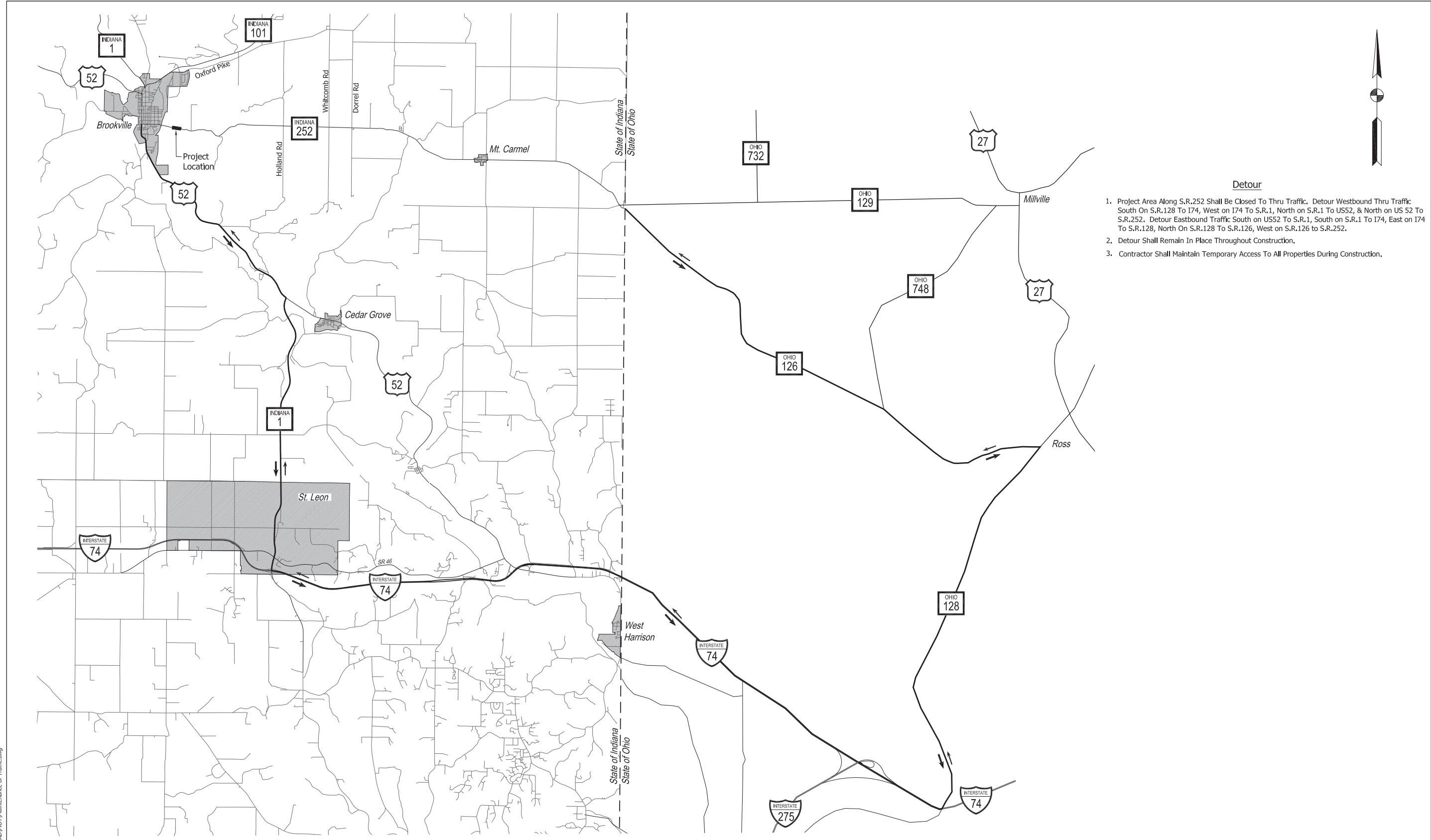
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RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: _____	NAW	DRAWN: _____	CCW
CHECKED: _____	JAW	CHECKED: _____	NAW

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTION
LINE "PR-A"

HORIZONTAL SCALE	BRIDGE FILE
1"=5'	-
VERTICAL SCALE	DESIGNATION
-	1700194
SURVEY BOOK	SHEETS
	4 of 15
CONTRACT	PROJECT
R-40431	1700194



Detour

1. Project Area Along S.R.252 Shall Be Closed To Thru Traffic. Detour Westbound Thru Traffic South On S.R.128 To I74, West on I74 To S.R.1, North on S.R.1 To US52, & North on US 52 To S.R.252. Detour Eastbound Traffic South on US52 To S.R.1, South on S.R.1 To I74, East on I74 To S.R.128, North On S.R.128 To S.R.126, West on S.R.126 to S.R.252.
2. Detour Shall Remain In Place Throughout Construction.
3. Contractor Shall Maintain Temporary Access To All Properties During Construction.

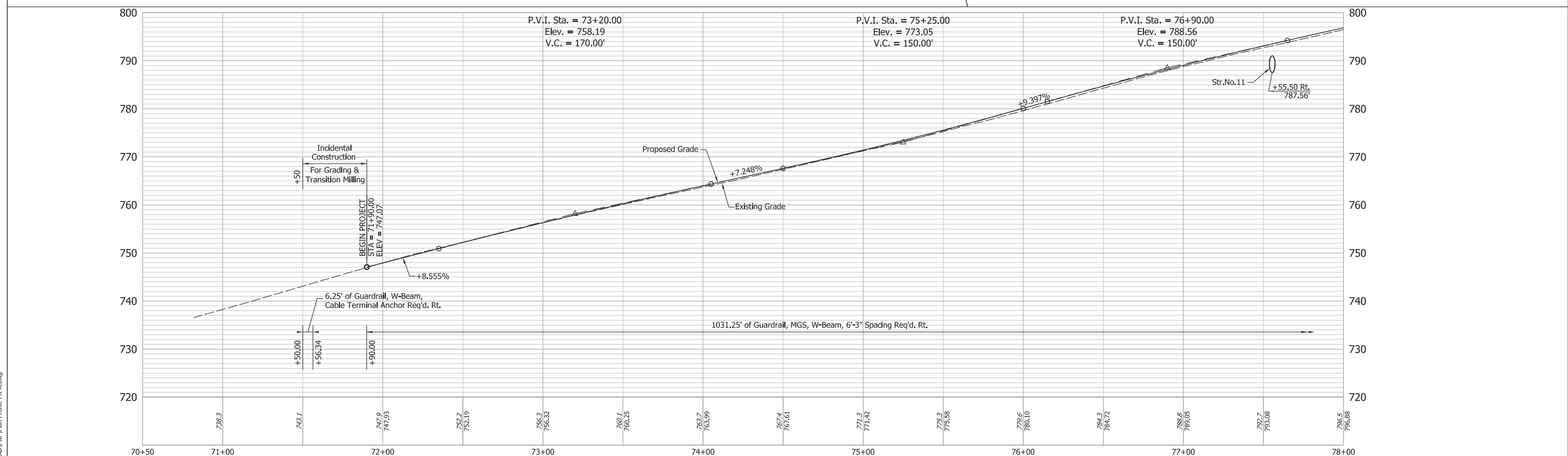
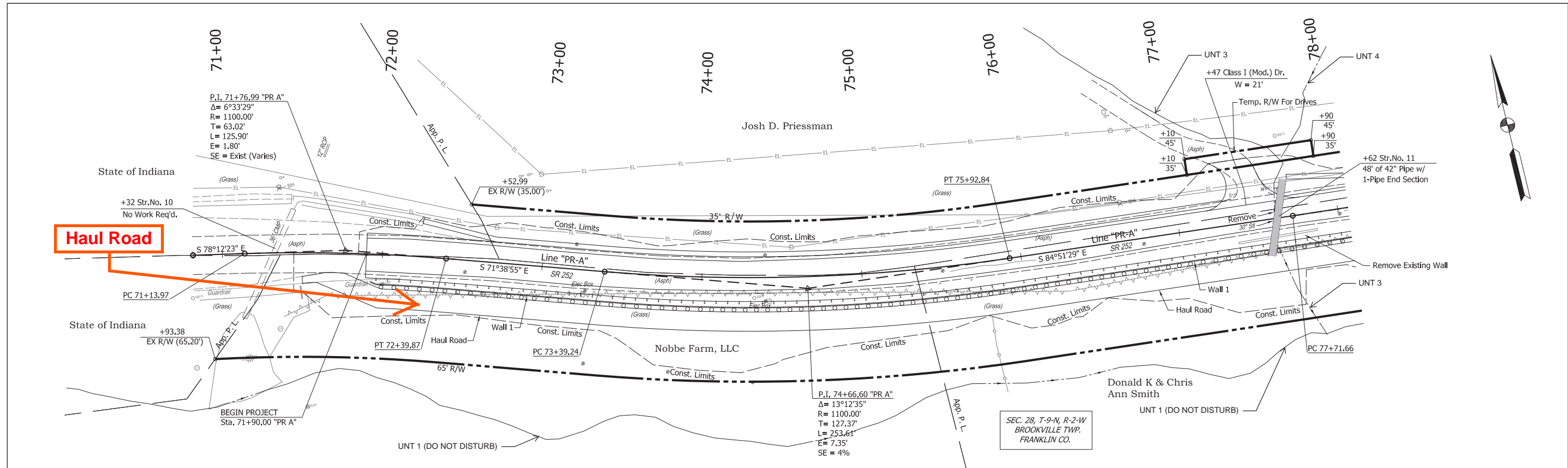
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RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE ____/____/____
DESIGNED: NAW	DRAWN: LLF	
CHECKED: JAW	CHECKED: NAW	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC

HORIZONTAL SCALE	BRIDGE FILE
1"=5000'	-
VERTICAL SCALE	DESIGNATION
"=1'	1700194
SURVEY BOOK	SHEET
	7 of 15
CONTRACT	PROJECT
R-40431	1700194

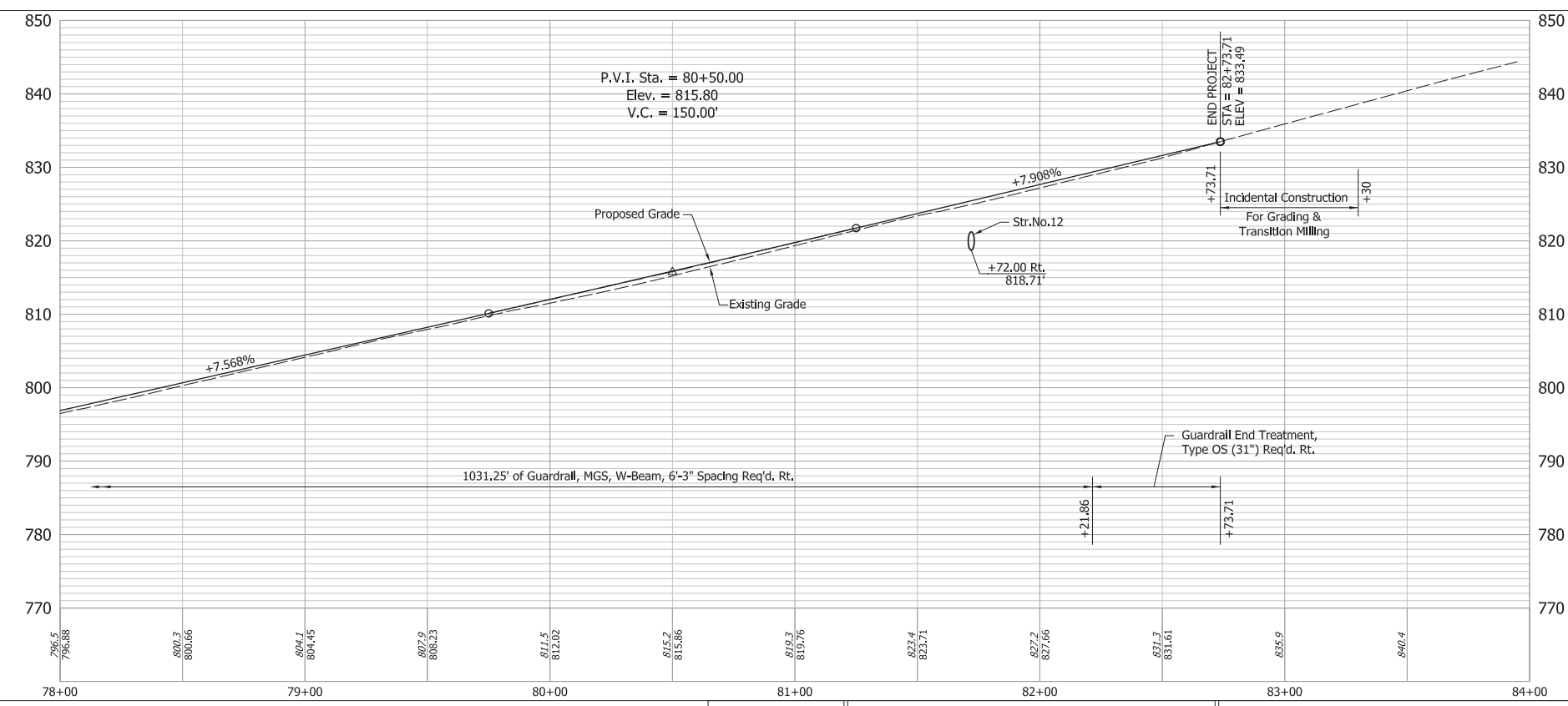
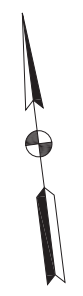
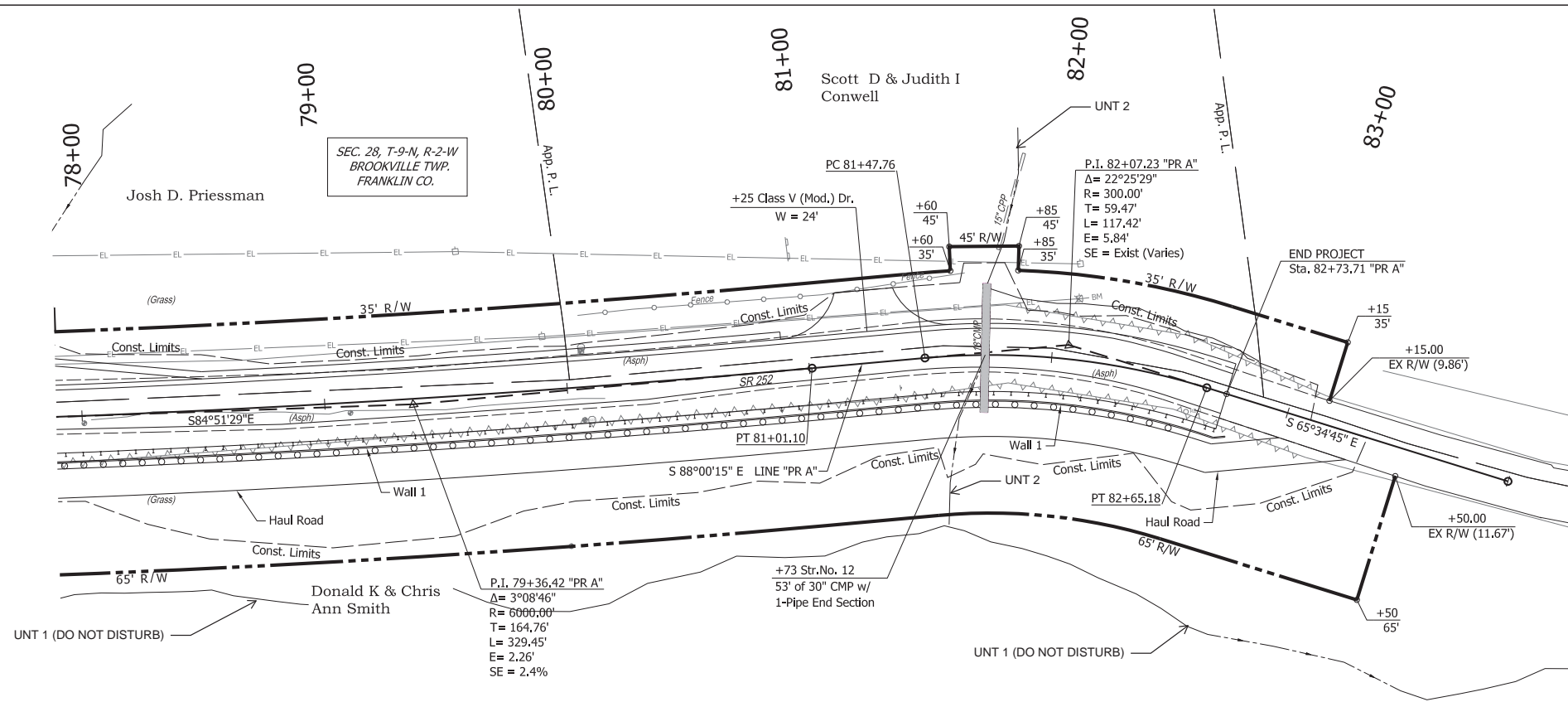


Date: May 16, 2020, 3:20pm User Name: Nick
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NAW	DRAWN: CCW	
CHECKED: JAW	CHECKED: NAW	

INDIANA
DEPARTMENT OF TRANSPORTATION
PLAN & PROFILE - LINE "PR-A"
STA. 70+00.00 TO STA. 78+00.00

HORIZONTAL SCALE	BRIDGE FILE
1"=30'	-
VERTICAL SCALE	DESIGNATION
1"=10'	1601044
SURVEY BOOK	SHEETS
	8 of --
CONTRACT	PROJECT
R-39920	1601044

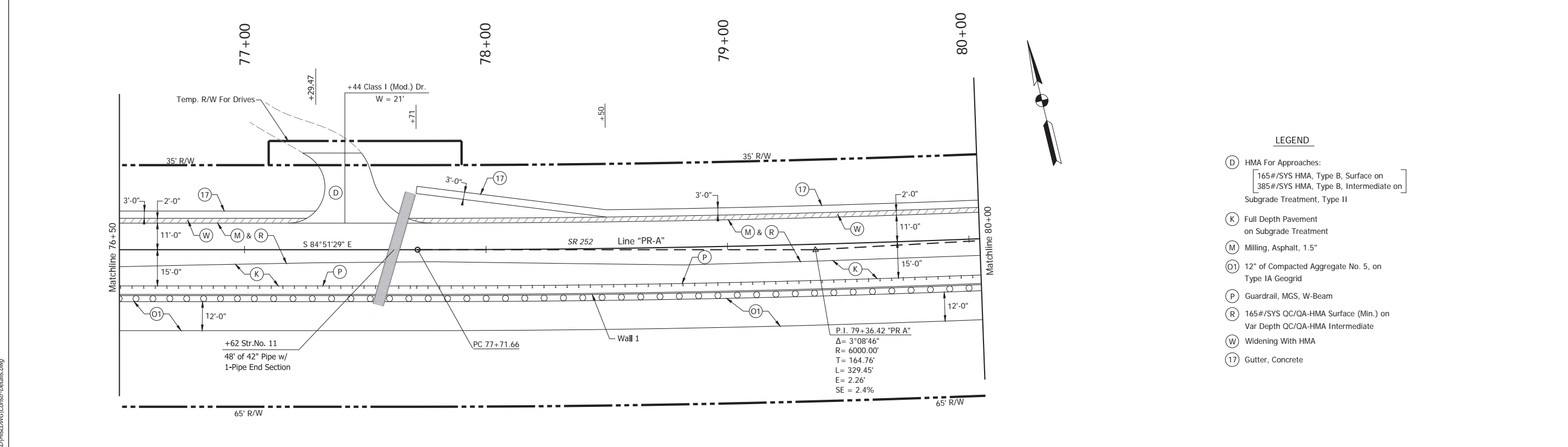
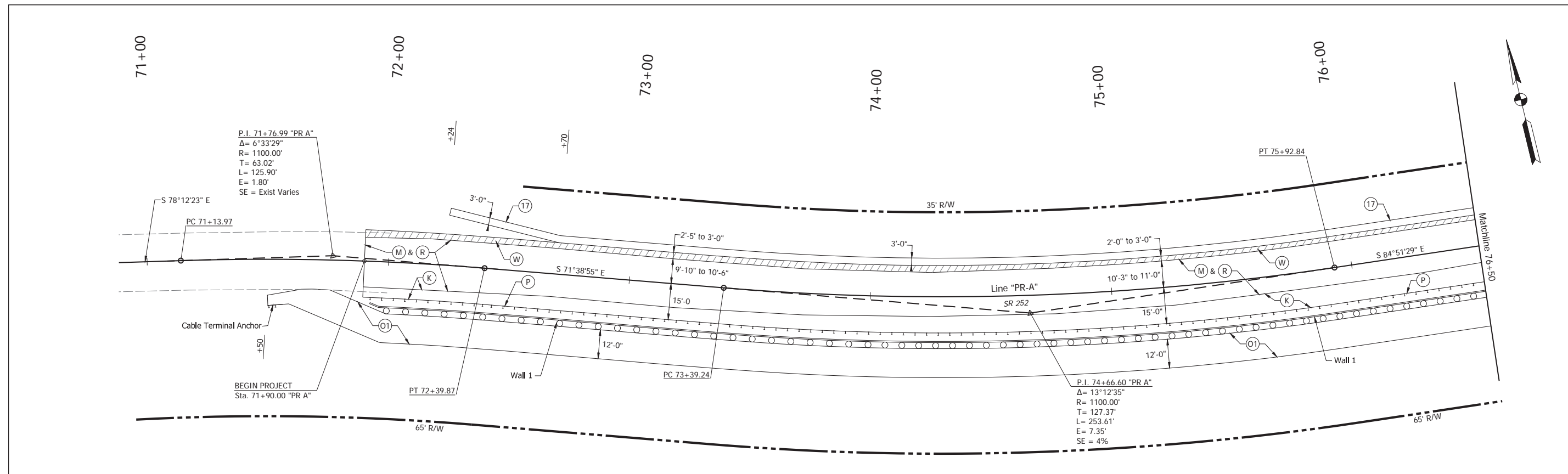


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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NAW	DRAWN: CCW	
CHECKED: JAW	CHECKED: NAW	

INDIANA
DEPARTMENT OF TRANSPORTATION
PLAN & PROFILE - LINE "PR-A"
STA. 78+00.00 TO STA. 84+00.00

HORIZONTAL SCALE	BRIDGE FILE
1"=30'	-
VERTICAL SCALE	DESIGNATION
1"=10'	1601044
SURVEY BOOK	SHEETS
	9 of --
CONTRACT	PROJECT
R-39920	1601044



- LEGEND**
- (D) HMA For Approaches:
 - 165#/SYS HMA, Type B, Surface on
 - 385#/SYS HMA, Type B, Intermediate on
 - Subgrade Treatment, Type II
 - (K) Full Depth Pavement on Subgrade Treatment
 - (M) Milling, Asphalt, 1.5"
 - (O1) 12" of Compacted Aggregate No. 5, on Type IA Geogrid
 - (P) Guardrail, MGS, W-Beam
 - (R) 165#/SYS QC/OA-HMA Surface (Min.) on Var Depth QC/OA-HMA Intermediate
 - (W) Widening With HMA
 - (17) Gutter, Concrete

Notes
 All Stationing off Line "PR-A" Unless Otherwise Noted.
 Drives Radius Per INDOT Std. Drawings Unless Otherwise Noted.

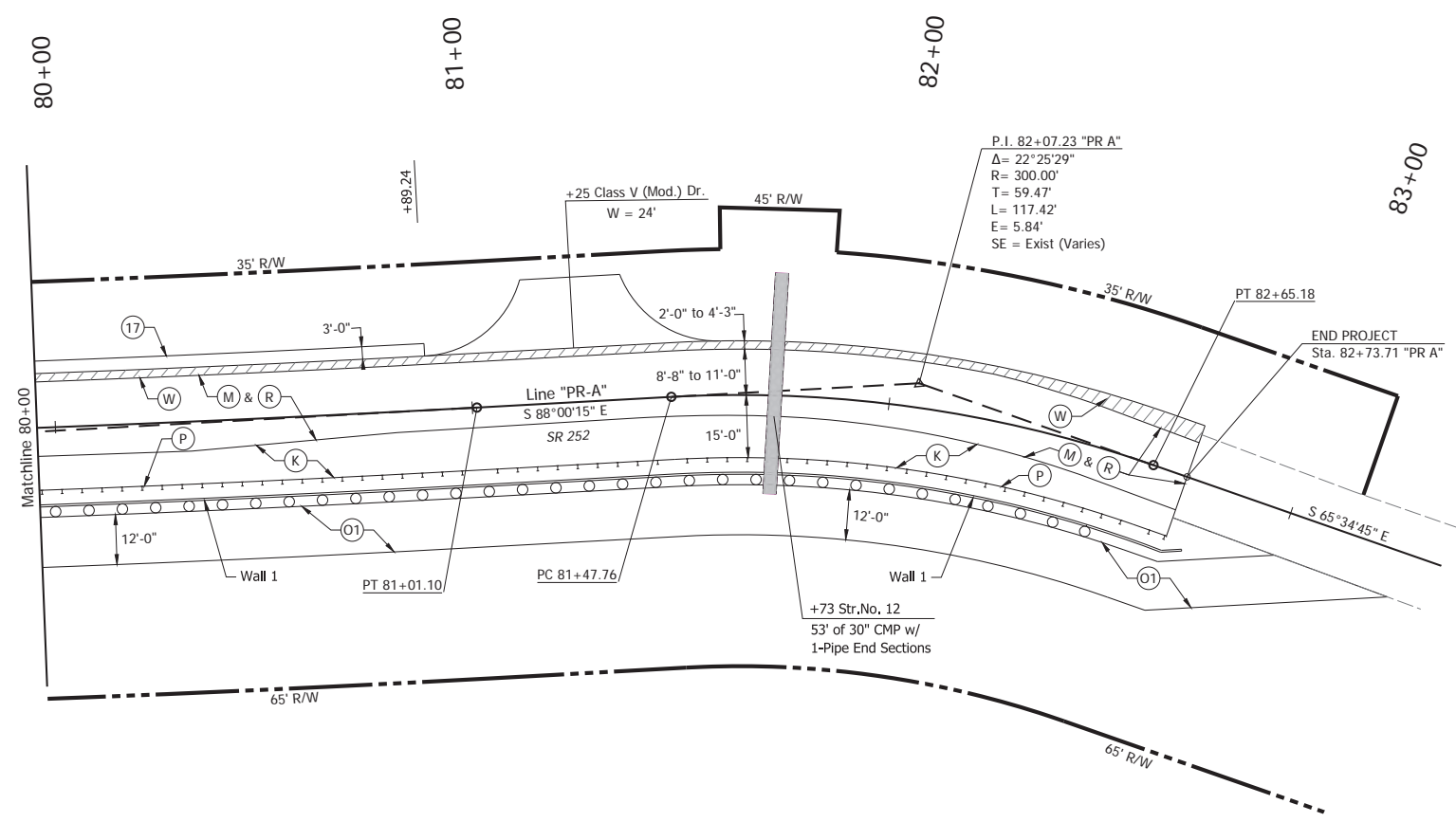
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DESIGNED: NAW	DRAWN: LLF	
CHECKED: JAW	CHECKED: NAW	

INDIANA
 DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
 STA. 71+00.00 TO STA. 80+00.00

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	-
VERTICAL SCALE	DESIGNATION
---	1700194
SURVEY BOOK	SHEETS
	10 of 15
CONTRACT	PROJECT
R-40431	1700194

Date: May 14, 2020, 4:08pm User Name: enson
 File: S:_2018\218-0534\Road\CAD\Misc\DWG\Constr-Details.dwg



LEGEND

- (D) HMA For Approaches:
 165#/SYS HMA, Type B, Surface on
 385#/SYS HMA, Type B, Intermediate on
 Subgrade Treatment, Type II
- (K) Full Depth Pavement
 on Subgrade Treatment
- (M) Milling, Asphalt, 1.5"
- (O1) 12" of Compacted Aggregate No. 5, on
 Type IA Geogrid
- (P) Guardrail, MGS, W-Beam
- (R) 165#/SYS QC/QA-HMA Surface (Min.) on
 Var Depth QC/QA-HMA Intermediate
- (W) Widening With HMA
- (17) Gutter, Concrete

Notes
 All Stationing off Line "PR-A" Unless Otherwise Noted.
 Drives Radius Per INDOT Std. Drawings Unless Otherwise Noted.

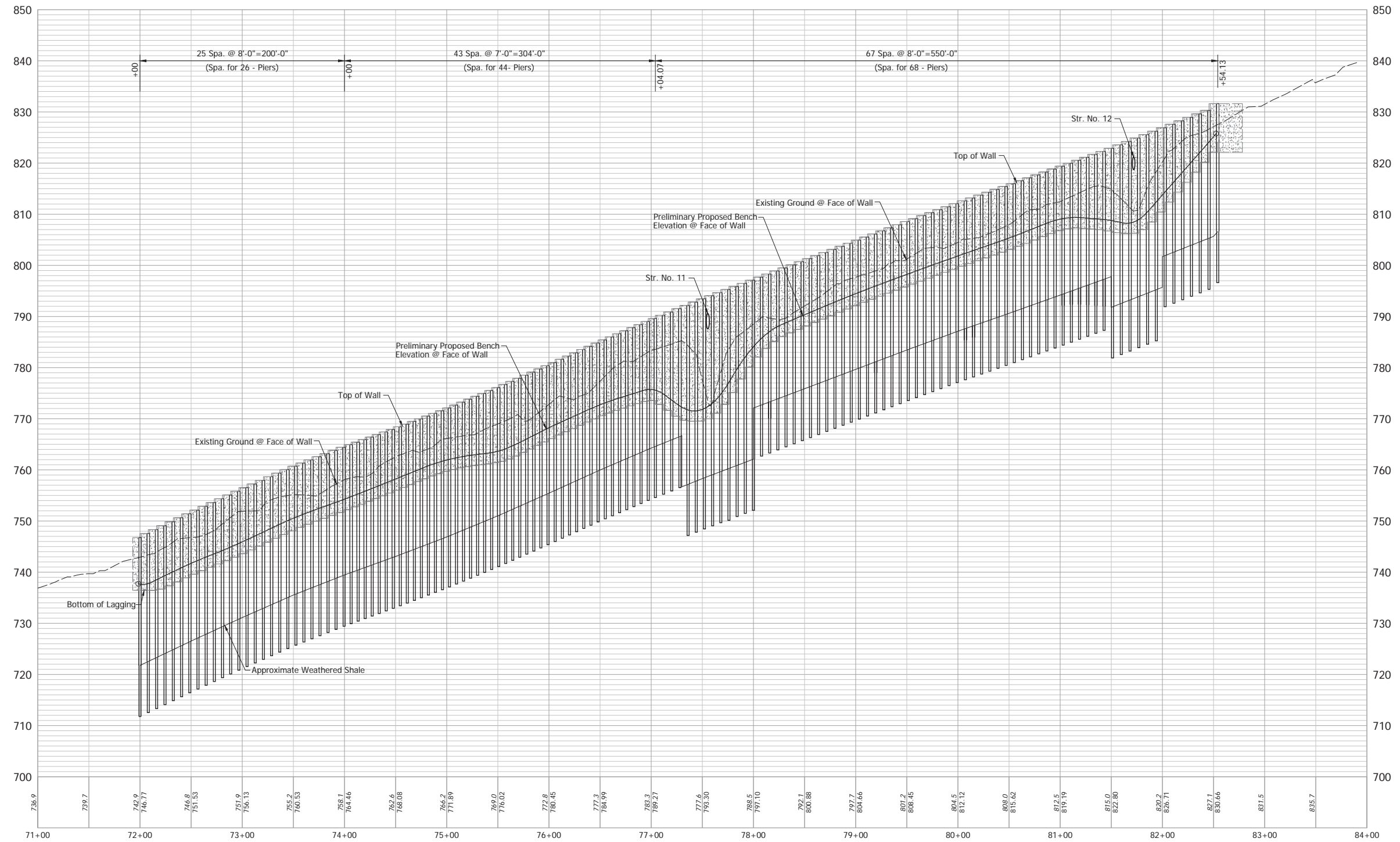
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NAW	DRAWN: LLF	
CHECKED: JAW	CHECKED: NAW	

**INDIANA
 DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAILS
 STA. 80+00.00 TO STA. 83+00.00**

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	-
VERTICAL SCALE	DESIGNATION
--	1700194
SURVEY BOOK	SHEETS
	11 of 15
CONTRACT	PROJECT
R-40431	1700194

Date: May 14, 2020, 4:08pm User Name: eolson
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Date: May 14, 2020 4:08pm User Name: eolan
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: NAW	DRAWN: CCW	
CHECKED: JAW	CHECKED: NAW	

INDIANA
 DEPARTMENT OF TRANSPORTATION

**WALL PROFILE
 LINE "PR-A"**

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	-
VERTICAL SCALE	DESIGNATION
1"=10'	1700194
SURVEY BOOK	SHEETS
	13 of 15
CONTRACT	PROJECT
R-40431	1700194

Categorical Exclusion
Appendix C
Early Coordination



July 18, 2019

Sample Early
Coordination Letter

Re: Des. No.: 1700194
State Road (SR) 252, 0.8 mile east of US 52
Slide Correction, State Project
Brookville Township, Franklin County, Indiana

Dear :

The Indiana Department of Transportation (INDOT) - Seymour District proposes to proceed with a slide correction project along SR 252 in Franklin County, Indiana (Des. No. 1700194). The project will involve stabilizing the embankment on the south side of SR 252 to prevent further damage from the land slide. This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. Please use the above Des. No. and project description in your reply and your comments will be incorporated into the formal environmental study. Your cooperation in this endeavor is appreciated.

Project Location and Existing Conditions

The proposed project is located along SR 252, approximately 0.8 mile east of US 52 and east of the Town of Brookville in Brookville Township of Franklin County, Indiana. Specifically, the project is located in Section 28, Township 9 North, Range 2 West as depicted on the Brookville and Whitcomb U. S. Geological Survey 1:24,000 quadrangles. Adjacent land use consists of residential and forested areas. Please see attachments for maps and photographs of the proposed project area.

SR 252 is functionally classified as a major collector within the project area and consists of two, 11-foot asphalt travel lanes (one in each direction) with asphalt and aggregate shoulders varying from less than 1 foot to 4 feet in width. The current speed limit along this stretch of SR 252 is 55 miles per hour (mph). Stormwater and roadside drainage is conveyed through three corrugated metal pipe (CMP) culverts underneath SR 252 that drain to the south side of the roadway.

Purpose and Need

The need for this project is evidenced by the deteriorated condition of SR 252. This condition is a result of the gradual land slide occurring along the south side of SR 252. This has caused the pavement and roadside embankment to deteriorate and fail. The purpose of the project is to correct the embankment failure and improve conditions of SR 252 to allow for continued mobility and accessibility.

3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268
PHONE: 317.222.3878 • TOLL FREE: 800.423.7422

Proposed Project

The proposed project will construct a soldier pile wall with anchor tiebacks along SR 252 where the embankment is sliding to the south of the roadway. A soldier pile wall is a retaining wall that uses steel piles installed vertically and timber lagging installed horizontally between the piles to stabilize and spread the load behind it. Installation of this new retaining wall will require the removal of the existing stone retaining wall along the south side of the roadway. The roadway within the damaged area will be reconstructed to a typical section consisting of two 11-foot asphalt travel lanes (one in each direction) with a 4-foot wide asphalt shoulder along the eastbound lane and a 2-foot wide asphalt shoulder along westbound lane. New guardrail will be installed along the south side of SR 252. It is anticipated the pavement reconstruction will consist of mill/overlay, wedge and level, and widening. The culverts conveying stormwater and roadside drainage under SR 252 will be replaced with culverts that are hydrologically sufficient in size. New riprap will be placed at the outlets, and possibly the inlets, of each new culvert for scour protection. No new permanent lighting will be installed and the use of temporary lighting during construction is not anticipated. The total length of the project along SR 252 will be approximately 1,400 feet (0.27 mile).

The maintenance of traffic (MOT) will involve the closure of SR 252 to through traffic. A detour will be required, but the designated route has not been established. The MOT will be further defined as the design advances. Access will be maintained to all properties during construction of the project. The MOT will be implemented per the *Indiana Design Manual* guidelines.

Construction is anticipated to begin in Fiscal Year (FY) 2020.

Right-of-Way (ROW)

This project will require the acquisition of approximately 1.5 acres of permanent right-of-way to construct the soldier pile wall and install the tie backs. Approximately one acre of tree clearing will likely be required to complete the project.

Environmental Resources

A Red Flag Investigation (RFI) was performed for a 0.5-mile radius for the project area. "Red Flags" were identified within the 0.5-mile search radius; however, not all will impact the proposed project. An unnamed tributary (UNT) to East Fork Whitewater River is within the project area. This project is outside the Karst Memorandum of Understanding Potential Karst Features Region.

Lochmueller Group conducted a field investigation of the project area on June 24, 2019. The investigation identified four UNTs to East Fork Whitewater River and a roadside ditch (RSD) within the survey area. Due to the presence of these streams within the survey area, a *Waters of the U.S. Determination Report* will be prepared for this project.

Section 106

The National Register of Historic Places (National Register) and the Indiana Register of Historic Sites and Structures (State Register) were reviewed using the State Historic Architectural and Archaeological Research Database (SHAARD) and SHAARD Geographic Information System (GIS)

data published online. No above-ground historical resources on either list are within the project area. The 1978 *Franklin County Interim Report: Indiana Historic Sites and Structures Inventory* (IHSSI) data was also examined; no surveyed resources from this inventory were located within the project area. No resources listed in the Interim Report are near the project area. The *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* by Mead & Hunt (2009) was reviewed. No bridges eligible for listing in the National Register are within the project area. No cemeteries were noted within the vicinity of the project area. It is anticipated that this project will qualify for the Minor Projects Programmatic Agreement (MPPA), under Category B-9 and B-10. However, due to the contextual history of the retaining wall within the project area, further Section 106 review may be necessary.

Range-wide Informal Programmatic Consultation

Franklin County is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (*Myotis septentrionalis*). The U.S. Fish and Wildlife Service (USFWS) Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB) will be completed for this project. Completion of the appropriate determination key through the USFWS Information for Planning and Consultation (IPaC) portal will occur. If a likely determination of “Not Likely to Adversely Affect,” or “Likely to Adversely Affect” is reached then additional consultation with the USFWS will occur through INDOT.

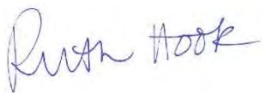
Early Coordination

This letter is part of the early coordination review process. You are asked to review this information and provide any comments you may have relative to anticipated impacts of the project on areas in which you have jurisdiction or special expertise. We will incorporate your comments into a study of the project’s environmental impacts. To facilitate the development of this project, you are asked to reply within **30 days** of receipt of this letter. If no response is received by that date, it will be assumed you have no comments at the present time.

If you have any questions regarding this project, please feel free to contact me at (317) 222-3880 or at rhook@lochgroup.com. Additionally, should you want to contact the sponsor of this project, INDOT-Seymour District please contact the Project Manager, Travis Mankin at (812) 524-3957 or a tmankin@indot.in.gov.

Thank you in advance for your input.

Sincerely,



Ruth Hook, CPESC, CESSWI
Environmental Biologist
Lochmueller Group, Inc.

Attachments:

- General Location Map
 - USGS Topographic Map
 - Red Flag Investigation Maps
 - Photo Location Map and Photographs
- Removed to avoid duplication; see Appendices B and E

Distribution List:

- USFWS, Bloomington Field Office (electronic submission)
- Natural Resources Conservation Service, Indianapolis Office (electronic submission)
- U.S. Army Corps of Engineers, Louisville District (electronic submission)
- U.S. Housing and Urban Development (electronic submission)
- National Park Service
- FHWA – Indiana Division (electronic submission)
- IDNR, Division of Fish and Wildlife (electronic submission)
- IDEM (electronic submission)
- INDOT, Office of Public Involvement (electronic submission)
- INDOT, Environmental Services (electronic submission)
- INDOT, Seymour District (electronic submission)
- INDOT, Project Manager (electronic submission)
- Indiana Geological Survey (electronic submission)
- Franklin County Highway Department (electronic submission)
- Franklin County Board of Commissioners (electronic submission)
- Franklin County Council
- Brookville Township Trustee
- Franklin County Surveyor’s Office (electronic submission)
- Brookville Town Board
- Franklin County Emergency Management Agency
- Brookville Police Department
- Franklin County Sheriff’s Department (electronic submission)
- Franklin County EMS
- Franklin County Community School Corporation
- Brookville Fire Department (electronic submission)



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

INDOT - Seymour District
Travis Mankin
185 Agrico Ln
Seymour , IN IN

Lochmueller Group
Ruth Hook
3502 Woodview Trace
Suite 150
Indianapolis , IN 46268

Date

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: The Indiana Department of Transportation (INDOT) - Seymour District proposes to proceed with a slide correction project along SR 252 in Franklin County, Indiana (Des. No. 1700194). The project will involve stabilizing the embankment on the south side of SR 252 to prevent further damage from the land slide. The proposed project is located along SR 252, approximately 0.8 mile east of US 52 and east of the Town of Brookville in Brookville Township of Franklin County, Indiana. Specifically, the project is located in Section 28, Township 9 North, Range 2 West as depicted on the Brookville and Whitcomb U. S. Geological Survey 1:24,000 quadrangles. Adjacent land use consists of residential and forested areas. Please see attachments for maps and photographs of the proposed project area. SR 252 is functionally classified as a major collector within the project area and consists of two, 11-foot asphalt travel lanes (one in each direction) with asphalt and aggregate shoulders varying from less than 1 foot to 4 feet in width. The current speed limit along this stretch of SR 252 is 55 miles per hour (mph). Stormwater and roadside drainage is conveyed through three corrugated metal pipe (CMP) culverts underneath SR 252 that drain to the south side of the roadway. The need for this project is evidenced by the deteriorated condition of SR 252. This condition is a result of the gradual land slide occurring along the south side of SR 252. This has caused the pavement and roadside embankment to deteriorate and fail. The purpose of the project is to correct the embankment failure and improve conditions of SR 252 to allow for continued mobility and accessibility. The proposed project will construct a soldier pile wall with anchor tiebacks along SR 252 where the embankment is sliding to the south of the roadway. A soldier pile wall is a retaining wall that uses steel piles installed vertically and timber lagging installed horizontally between the piles to stabilize and spread the load behind it. Installation of this new retaining wall will require the removal of the existing stone retaining wall along the south side of the roadway. The roadway within the damaged area will be reconstructed to a typical section consisting of two 11-foot asphalt travel lanes (one in each direction) with a 4-foot wide asphalt shoulder along the eastbound lane and a 2-foot wide asphalt shoulder along westbound lane. New guardrail will be installed along the south side of SR 252. It is anticipated the pavement reconstruction will consist of mill/overlay, wedge and level, and widening. The culverts conveying stormwater and roadside drainage under SR 252 will be replaced with culverts that are hydrologically sufficient in size. New riprap will be placed at the outlets, and possibly the inlets, of each new culvert for scour protection. No new permanent lighting will be installed and the use of temporary lighting during construction is not anticipated. The total length of the project along SR 252 will be approximately 1,400 feet (0.27 mile). The maintenance of traffic (MOT) will involve the closure of SR 252 to through traffic. A detour will be required, but the designated route has not been established. The MOT will be further defined as the design advances. Access will be maintained to all properties during construction of the project. The MOT will be implemented per the Indiana Design Manual guidelines. This project will require the acquisition of approximately 1.5 acres of permanent right-of-way to construct the soldier pile wall and install the tie backs. Approximately one acre of tree clearing will likely be required to complete the project. A Red Flag Investigation (RFI) was performed for a 0.5-mile radius for the project area. "Red Flags" were identified within the 0.5-mile search radius; however, not all will impact the proposed project. An unnamed tributary (UNT) to East Fork Whitewater River is within the project area. This project is outside the Karst Memorandum of Understanding Potential Karst Features Region. Lochmueller Group conducted a field investigation of the project area on June 24, 2019. The investigation identified four UNTs to East Fork Whitewater River and a roadside ditch (RSD) within the survey area. Due to the presence of these streams within the survey area, a Waters of the U.S. Determination Report will be prepared for this project.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/idem/5283.htm> (<http://www.in.gov/idem/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation,

channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm>. IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
 - IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - IC 14-28-1 Flood Control Act 310 IAC 6-1
 - IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>). Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq>) (<http://www.in.gov/idem/4917.htm#constreq>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html>) (<http://www.in.gov/isda/soil/contacts/map.html>).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>).

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf)). It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent

demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm> (<http://www.in.gov/isdh/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>) (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf) (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>.) New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm> (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm> (<http://www.in.gov/idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

The Indiana Department of Transportation (INDOT) - Seymour District proposes to proceed with a slide correction project along SR 252 in Franklin County, Indiana (Des. No. 1700194). The project will involve stabilizing the embankment on the south side of SR 252 to prevent further damage from the land slide. The proposed project is located along SR 252, approximately 0.8 mile east of US 52 and east of the Town of Brookville in Brookville Township of Franklin County, Indiana. Specifically, the project is located in Section 28, Township 9 North, Range 2 West as depicted on the Brookville and Whitcomb U. S. Geological Survey 1:24,000 quadrangles. Adjacent land use consists of residential and forested areas. Please see attachments for maps and photographs of the proposed project area. SR 252 is functionally classified as a major collector within the project area and consists of two, 11-foot asphalt travel lanes (one in each direction) with asphalt and aggregate shoulders varying from less than 1 foot to 4 feet in width. The current speed limit along this stretch of SR 252 is 55 miles per hour (mph). Stormwater and roadside drainage is conveyed through three corrugated metal pipe (CMP) culverts underneath SR 252 that drain to the south side of the roadway. The need for this project is evidenced by the deteriorated condition of SR 252. This condition is a result of the gradual land slide occurring along the south side of SR 252. This has caused the pavement and roadside embankment to deteriorate and fail. The purpose of the project is to correct the embankment failure and improve conditions of SR 252 to allow for continued mobility and accessibility. The proposed project will construct a soldier pile wall with anchor tiebacks along SR 252 where the embankment is sliding to the south of the roadway. A soldier pile wall is a retaining wall that uses steel piles installed vertically and timber lagging installed horizontally between the piles to stabilize and spread the load behind it. Installation of this new retaining wall will require the removal of the existing stone retaining wall along the south side of the roadway. The roadway within the damaged area will be reconstructed to a typical section consisting of two 11-foot asphalt travel lanes (one in each direction) with a 4-foot wide asphalt shoulder along the eastbound lane and a 2-foot wide asphalt shoulder along westbound lane. New guardrail will be installed along the south side of SR 252. It is anticipated the pavement reconstruction will consist of mill/overlay, wedge and level, and widening. The culverts conveying stormwater and roadside drainage under SR 252 will be replaced with culverts that are hydrologically sufficient in size. New riprap will be placed at the outlets, and possibly the inlets, of each new culvert for scour protection. No new permanent lighting will be installed and the use of temporary lighting during construction is not anticipated. The total length of the project along SR 252 will be approximately 1,400 feet (0.27 mile). The maintenance of traffic (MOT) will involve the closure of SR 252 to through traffic. A detour will be required, but the designated route has not been established. The MOT will be further defined as the design advances. Access will be maintained to all properties during construction of the project. The MOT will be implemented per the Indiana Design Manual guidelines. This project will require the acquisition of approximately 1.5 acres of permanent right-of-way to construct the soldier pile wall and install the tie backs. Approximately one acre of tree clearing will likely be required to complete the project. A Red Flag Investigation (RFI) was performed for a 0.5-mile radius for the project area. "Red Flags" were identified within the 0.5-mile search radius; however, not all will impact the proposed project. An unnamed tributary (UNT) to East Fork Whitewater River is within the project area. This project is outside the Karst Memorandum of Understanding Potential Karst Features Region. Lochmueller Group conducted a field investigation of the project area on June 24, 2019. The investigation identified four UNTs to East Fork Whitewater River and a roadside ditch (RSD) within the survey area. Due to the presence of these streams within the survey area, a Waters of the U.S. Determination Report will be prepared for this project.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 7/19/19

Signature of the INDOT
Project Engineer or Other Responsible Agent Travis Mankin

Travis Mankin

Date: 07/19/2019

Signature of the
For Hire Consultant Ruth Hook

Ruth Hook

Organization and Project Information

Project ID:
Des. ID: 1700194
Project Title: SR 252 Slide Correction Project
Name of Organization: Lochmueller Group
Requested by: Chris Kunkel

Environmental Assessment Report

1. Geological Hazards:
 - High liquefaction potential
 - 1% Annual Chance Flood Hazard
 - Potential Slope Instability
2. Mineral Resources:
 - Bedrock Resource: Low Potential
 - Sand and Gravel Resource: Low Potential
3. Active or abandoned mineral resources extraction sites:
 - None documented in the area

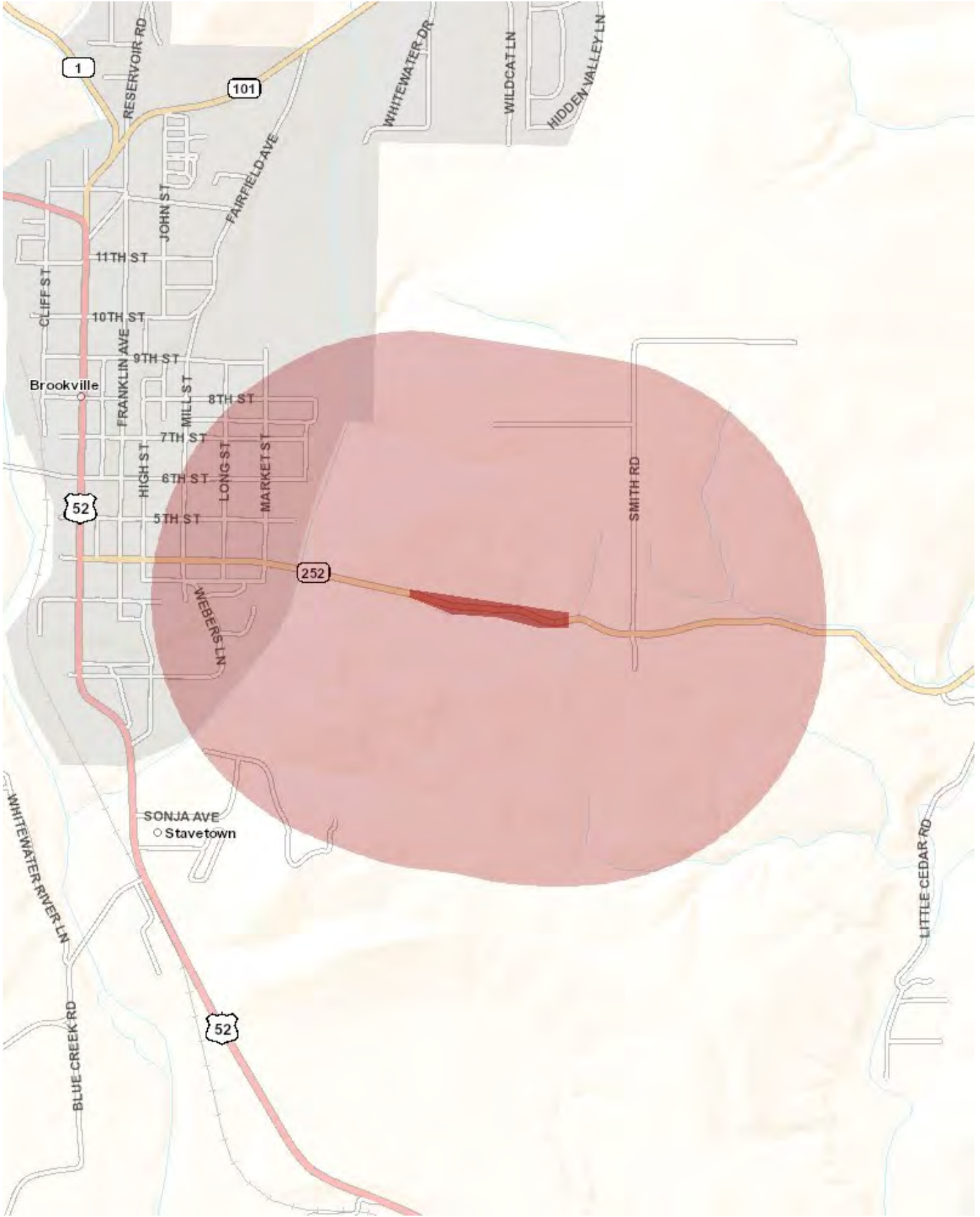
*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey
Address: 420 N. Walnut St., Bloomington, IN 47404
Email: IGSEnvir@indiana.edu
Phone: 812 855-7428

Date: July 18, 2019



Metadata:

- https://maps.indiana.edu/metadata/Geology/Seismic_Earthquake_Liquefaction_Potential.html
- https://maps.indiana.edu/metadata/Geology/Industrial_Minerals_Sand_Gravel_Resources.html
- https://maps.indiana.edu/metadata/Hydrology/Floodplains_FIRM.html
- https://maps.indiana.edu/metadata/Geology/Bedrock_Geology.html

Kunkel, Chris

From: Wright, Mary <MWRIGHT@indot.IN.gov>
Sent: Monday, July 22, 2019 9:49 AM
To: Kunkel, Chris
Subject: RE: SR 252 Slide Correction Project (Des. No. 1700194) ECL

Early Coordination and Creating a Public Involvement Plan (PIP)

We have received your early coordination notification packet for the above referenced project(s). Our office prefers to be notified at the early coordination stage in order to encourage early and ongoing public involvement aside from the specific legal requirements as outlined in our Public Involvement Manual <http://www.in.gov/indot/2366.htm>. Seeking the public's understanding of transportation improvement projects early in the project development stage can allow the opportunity for the public to express their concerns, comments, and to seek buy-in. Early coordination is the perfect opportunity to examine the proposed project and its impacts to the community along with the many ways and or tools to inform the public of the improvements and seek engagement. A good public involvement plan, or PIP, should consider the type, scope, impacts, and the level of public awareness that should, or could, be implemented. In other words, although there are cases where no public involvement is legally required, sometimes it is simply the right thing to do in order to keep the public informed.

The public involvement office is always available to provide support and resources to bolster any public involvement activities you may wish to implement or discuss. Please feel free to contact our office anytime should you have any questions or concerns. Thank you for notifying our office about your proposed project. We trust you will not only analyze the appropriate public involvement required, but also consider the opportunity to do go above and beyond those requirements in creating a good PIP.

Rickie Clark, Manager
100 North Senate Avenue, Room N642
Indianapolis, IN 46204
Phone: 317-232-6601
Email: rclark@indot.in.gov

Mary Wright, Hearing Examiner
Phone: 317-234-0796
Email: mwright@indot.in.gov

From: Kunkel, Chris [mailto:CKunkel@lochgroup.com]
Sent: Thursday, July 18, 2019 1:32 PM
To: Clark, Rickie <RCLARK@indot.IN.gov>; Wright, Mary <MWRIGHT@indot.IN.gov>
Cc: Hook, Ruth <RHook@lochgroup.com>
Subject: SR 252 Slide Correction Project (Des. No. 1700194) ECL

Good afternoon,

Please see the attached early coordination letter and associated attachments for the slide correction project in Franklin County, Indiana.

Please contact myself or Ruth Hook (rhook@lochgroup.com) should you have any questions or comments regarding this project.

Kunkel, Chris

From: McWilliams, Robin <robin_mcwilliams@fws.gov>
Sent: Tuesday, July 30, 2019 3:42 PM
To: Kunkel, Chris
Subject: Re: [EXTERNAL] SR 252 Slide Correction Project (Des. No. 1700194) ECL

Dear Chris.

This responds to your recent letter, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The project is within the range of the Indiana bat (*Myotis sodalis*) and northern long-eared bat (*Myotis septentrionalis*) and should follow the new Indiana bat/northern long-eared bat programmatic consultation process, if applicable (*i.e.* a federal transportation nexus is established). We will review that information once it is received.

Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no objections to the project as currently proposed. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinstate consultation. Standard recommendations are provided below.

We appreciate the opportunity to comment at this early stage of project planning. If project plans change such that fish and wildlife habitat may be affected, please re-coordinate with our office as soon as possible. If you have any questions about our recommendations, please call (812) 334-4261 x. 207.

Sincerely,
Robin McWilliams Munson

Standard Recommendations:

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the "tree clearing" restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.

Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottomed culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.

3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.

4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If rip rap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel (in perennial streams and larger intermittent streams) during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing.

Robin McWilliams Munson
U.S. Fish and Wildlife Service
620 South Walker Street
Bloomington, Indiana 46403
812-334-4261 x. 207 Fax: 812-334-4273

Monday, Tuesday - 7:30a-3:00p
Wednesday, Thursday - telework 8:30a-3:00p

On Thu, Jul 18, 2019 at 1:21 PM Kunkel, Chris <CKunkel@lochgroup.com> wrote:

Good afternoon,

Please see the attached early coordination letter and associated attachments for the slide correction project in Franklin County, Indiana.

Please contact myself or Ruth Hook (rhook@lochgroup.com) should you have any questions or comments regarding this project.

Thank you for your time and have a great day,



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

May 15, 2020

Consultation Code: 03E12000-2019-SLI-1352

Event Code: 03E12000-2020-E-06828

Project Name: SR 252 Slide Correction (Des. No. 1700194)

Subject: Updated list of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2019-SLI-1352

Event Code: 03E12000-2020-E-06828

Project Name: SR 252 Slide Correction (Des. No. 1700194)

Project Type: TRANSPORTATION

Project Description: The Indiana Department of Transportation (INDOT) - Seymour District proposes to proceed with a slide correction project along SR 252 in Franklin County, Indiana (Des. No. 1700194). The project will involve stabilizing the embankment on the south side of SR 252 to prevent further damage from the land slide. The proposed project is located along SR 252, approximately 0.8 mile east of US 52 and east of the Town of Brookville in Brookville Township of Franklin County, Indiana. Specifically, the project is located in Section 28, Township 9 North, Range 2 West as depicted on the Brookville and Whitcomb U. S. Geological Survey 1:24,000 quadrangles. Adjacent land use consists of residential and forested areas.

SR 252 is functionally classified as a major collector within the project area and consists of two, 11-foot asphalt travel lanes (one in each direction) with asphalt and aggregate shoulders varying from less than 1 foot to 4 feet in width. The current speed limit along this stretch of SR 252 is 55 miles per hour (mph). Stormwater and roadside drainage is conveyed through three corrugated metal pipe (CMP) culverts underneath SR 252 that drain to the south side of the roadway.

The proposed project will construct a soldier pile wall with anchor tiebacks along SR 252 where the embankment is sliding to the south of the roadway. A soldier pile wall is a retaining wall that uses steel piles installed vertically and timber lagging installed horizontally between the piles to stabilize and spread the load behind it. Installation of this new retaining wall will require the removal of the existing stone retaining wall along the south side of the roadway. The roadway within the damaged area will be reconstructed to a typical section consisting of two 11-foot asphalt travel lanes (one in each direction) with a 4-foot wide asphalt shoulder along the eastbound lane and a 2-foot wide asphalt shoulder along westbound lane. New guardrail will be installed along the south side of SR 252. It is anticipated the pavement reconstruction will consist of mill/overlay, wedge and level, and widening. The culverts conveying stormwater and roadside drainage under SR 252 will be replaced with culverts that are hydrologically sufficient in size. New riprap will be

placed at the outlets, and possibly the inlets, of each new culvert for scour protection. No new permanent lighting will be installed. The use of temporary lighting during construction is not anticipated. The total length of the project along SR 252 will be approximately 1,400 feet (0.27 mile).

Construction is expected to begin in Spring 2020.

This project will require the acquisition of approximately 1.5 acres of permanent right-of-way to construct the soldier pile wall and install the tie backs. Approximately one acre of tree clearing will likely be required to complete the project.

INDOT Seymour District reviewed the USFWS database for documented endangered bat species or their hibernacula within 0.5 mile of the project area on May 3, 2019. No endangered bat species were identified in or within 0.5 mile of the project area.

The culverts associated with this project were inspected for evidence of the presence of bats on June 24, 2019, and no evidence of the presence of bats was found.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/39.4171510624682N84.99840473212078W>



Counties: Franklin, IN

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

July 29, 2019

Consultation Code: 03E12000-2019-I-1352

Event Code: 03E12000-2019-E-06298

Project Name: SR 252 Slide Correction (Des. No. 1700194)

Subject: Concurrence verification letter for the 'SR 252 Slide Correction (Des. No. 1700194)' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated to verify that the **SR 252 Slide Correction (Des. No. 1700194)** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

SR 252 Slide Correction (Des. No. 1700194)

Description

The Indiana Department of Transportation (INDOT) - Seymour District proposes to proceed with a slide correction project along SR 252 in Franklin County, Indiana (Des. No. 1700194). The project will involve stabilizing the embankment on the south side of SR 252 to prevent further damage from the land slide. The proposed project is located along SR 252, approximately 0.8 mile east of US 52 and east of the Town of Brookville in Brookville Township of Franklin County, Indiana. Specifically, the project is located in Section 28, Township 9 North, Range 2 West as depicted on the Brookville and Whitcomb U. S. Geological Survey 1:24,000 quadrangles. Adjacent land use consists of residential and forested areas.

SR 252 is functionally classified as a major collector within the project area and consists of two, 11-foot asphalt travel lanes (one in each direction) with asphalt and aggregate shoulders varying from less than 1 foot to 4 feet in width. The current speed limit along this stretch of SR 252 is 55 miles per hour (mph). Stormwater and roadside drainage is conveyed through three corrugated metal pipe (CMP) culverts underneath SR 252 that drain to the south side of the roadway.

The proposed project will construct a soldier pile wall with anchor tiebacks along SR 252 where the embankment is sliding to the south of the roadway. A soldier pile wall is a retaining wall that uses steel piles installed vertically and timber lagging installed horizontally between the piles to stabilize and spread the load behind it. Installation of this new retaining wall will require the removal of the existing stone retaining wall along the south side of the roadway. The roadway within the damaged area will be reconstructed to a typical section consisting of two 11-foot asphalt travel lanes (one in each direction) with a 4-foot wide asphalt shoulder along the eastbound lane and a 2-foot wide asphalt shoulder along westbound lane. New guardrail will be installed along the south side of SR 252. It is anticipated the pavement reconstruction will consist of mill/overlay, wedge and level, and widening. The culverts conveying stormwater and roadside drainage under SR 252 will be replaced with culverts that are hydrologically sufficient in size. New riprap will be placed at the outlets, and possibly the inlets, of each new culvert for scour protection. No new permanent lighting will be installed. The use of temporary lighting during construction is not anticipated. The total length of the project along SR 252 will be approximately 1,400 feet (0.27 mile).

Construction is expected to begin in Spring 2020.

This project will require the acquisition of approximately 1.5 acres of permanent right-of-way to construct the soldier pile wall and install the tie backs. Approximately one acre of tree clearing will likely be required to complete the project.

INDOT Seymour District reviewed the USFWS database for documented endangered bat species or their hibernacula within 0.5 mile of the project area on May 3, 2019. No endangered bat species were identified in or within 0.5 mile of the project area.

The culverts associated with this project were inspected for evidence of the presence of bats on June 24, 2019, and no evidence of the presence of bats was found.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will the tree removal alter *any* **documented** Indiana bat or NLEB roosts and/or alter any surrounding summer habitat **within** 0.25 mile of a documented roost?

No

20. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

21. Are *all* trees that are being removed clearly demarcated?

Yes

22. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

No

23. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

24. Does the project include slash pile burning?

No

25. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

26. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

27. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *UNT 3 Culvert_Bat Assessment_2019-06-24.pdf* <https://ecos.fws.gov/ipac/project/QBIAGQVKEZBYZNV3RO6LBHHAOQ/projectDocuments/17511814>
- *West Culvert_Bat Assessment_2019-06-24.pdf* <https://ecos.fws.gov/ipac/project/QBIAGQVKEZBYZNV3RO6LBHHAOQ/projectDocuments/17511815>
- *UNT 4 Culvert_Bat Assessment_2019-06-24.pdf* <https://ecos.fws.gov/ipac/project/QBIAGQVKEZBYZNV3RO6LBHHAOQ/projectDocuments/17511816>

28. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

29. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

30. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

31. Will the project involve the use of **temporary** lighting *during* the active season?

No

32. Will the project install new or replace existing **permanent** lighting?

No

33. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

No

34. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

35. Will the project raise the road profile **above the tree canopy**?

No

36. Are the project activities that are not associated with habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives consistent with a No Effect determination in this key?

Automatically answered

Yes, other project activities are limited to actions that DO NOT cause any additional stressors to the bat species as described in the BA/BO

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost

39. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

40. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

41. Tree Removal AMM 1

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word “trees” as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS’ current summer survey guidance for our latest definitions of suitable habitat.

Yes

42. Tree Removal AMM 2

Can *all* tree removal activities be restricted to when Indiana bats are not likely to be present (e.g., the inactive season)^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Automatically answered

Yes

43. Tree Removal AMM 2

Can *all* tree removal activities be restricted to when Northern long-eared bats are not likely to be present (e.g., the inactive season)^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Automatically answered

Yes

44. Tree Removal AMM 3

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

45. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

46. **Lighting AMM 1**

Will *all* **temporary** lighting used during the removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be directed away from suitable habitat during the active season?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

No

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

1.0

4. Please describe the proposed bridge work:

Three culverts will be replaced with new culverts that are hydrologically sufficient in size along SR 252 as a part of this slide correction project.

5. Please state the timing of all proposed bridge work:

The culvert replacement portion of the project will likely occur in the Summer of 2020.

6. Please enter the date of the bridge assessment:

June 24, 2019

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

Determination Key Description: FHW A, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on March 16, 2018. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

APPENDIX D: Bridge/Structure Assessment Form

This form will be completed and submitted to the District Environmental Manager by the Contractor prior to conducting any work below the deck surface either from the underside; from activities above that bore down to the underside; from activities that could impact expansion joints; from deck removal on bridges; or from structure demolition for bridges/structures within 1000 feet of suitable bat habitat.

DOT Project # 1700194	Water Body UNT 3 to East Fork Whitewater River	Date/Time of Inspection 6/24/2019 10:45 AM	Within 1,000ft of suitable bat habitat (circle one) Yes No
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Route	County	Federal Structure ID
SR 252	Franklin	N/A (culvert conveying UNT 3 to East Fork Whitewater River)

If the bridge/structure is 1,000 feet or more from suitable bat habitat (e.g., an urban or agricultural area without suitable foraging habitat or corridors linking the bridge to suitable foraging habitat), check box and STOP HERE. No assessment required.

Please submit to the U.S. Fish and Wildlife Service.

Areas Inspected (Check all that apply)

Bridges		Culverts/Other Structures		Summary Info (circle all that apply)			
All vertical crevices sealed at the top and 0.5-1.25" wide & ≥4" deep		Crevices, rough surfaces or imperfections in concrete	X	Human disturbance or traffic under bridge/in culvert or at the structure	High	Low	None
All crevices >12" deep & not sealed		Spaces between walls, ceiling joists	X	Possible corridors for netting	None/poor	Marginal	Excellent
All guardrails							
All expansion joints							
Spaces between concrete end walls and the bridge deck							

Last Revised May 31, 2017

Vertical surfaces on concrete I-beams							
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Evidence of Bats (Circle all that apply) Presence of one or more indicators is sufficient evidence that bats may be using the structure.

None

Visual (e.g. survey, thermal, emergent etc.)

- Live __ number seen
- Dead __ number seen

Photo documentation Y/N

Guano

Odor Y/N

Photo documentation Y/N

Staining definitively from bats

Photo documentation Y/N

Audible

Assessment Conducted By: <u>Chris Kunkel</u> Signature(s): <u><i>Chris Kunkel</i></u>
District Environmental Use Only: Date Received by District Environmental Manager: _____

DOT Bat Assessment Form Instructions

1. Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges, regardless of whether assessments have been conducted in the past.
2. Any bridge/structure suspected of providing habitat for any species of bat will be removed from work schedules until such time that the DOT has coordinated with the USFWS. Additional studies may be undertaken by the DOT to determine what species may be utilizing each structure identified as supporting bats prior to allowing any work to proceed.
3. Any questions should be directed to the District Environmental Manager.

Last Revised June 2017

APPENDIX D: Bridge/Structure Assessment Form

This form will be completed and submitted to the District Environmental Manager by the Contractor prior to conducting any work below the deck surface either from the underside; from activities above that bore down to the underside; from activities that could impact expansion joints; from deck removal on bridges; or from structure demolition for bridges/structures within 1000 feet of suitable bat habitat.

DOT Project # 1700194	Water Body UNT 4 to East Fork Whitewater River	Date/Time of Inspection 6/24/2019 11:15 AM	Within 1,000ft of suitable bat habitat (circle one) Yes No
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Route	County	Federal Structure ID
SR 252	Franklin	N/A (culvert conveying UNT 4 to East Fork Whitewater River)

If the bridge/structure is 1,000 feet or more from suitable bat habitat (e.g., an urban or agricultural area without suitable foraging habitat or corridors linking the bridge to suitable foraging habitat), check box and STOP HERE. No assessment required.

Please submit to the U.S. Fish and Wildlife Service.

Areas Inspected (Check all that apply)

Bridges		Culverts/Other Structures		Summary Info (circle all that apply)			
All vertical crevices sealed at the top and 0.5-1.25" wide & ≥4" deep		Crevices, rough surfaces or imperfections in concrete	X	Human disturbance or traffic under bridge/in culvert or at the structure	High	Low	None
All crevices >12" deep & not sealed		Spaces between walls, ceiling joists	X	Possible corridors for netting	None/poor	Marginal	Excellent
All guardrails							
All expansion joints							
Spaces between concrete end walls and the bridge deck							

Last Revised May 31, 2017

Vertical surfaces on concrete I-beams							
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Evidence of Bats (Circle all that apply) Presence of one or more indicators is sufficient evidence that bats may be using the structure.

None

Visual (e.g. survey, thermal, emergent etc.)

- Live __ number seen
- Dead __ number seen

Photo documentation Y/N

Guano

Odor Y/N

Photo documentation Y/N

Staining definitively from bats

Photo documentation Y/N

Audible

Assessment Conducted By: <u>Chris Kunkel</u> Signature(s): <u><i>Chris Kunkel</i></u>
District Environmental Use Only: Date Received by District Environmental Manager: _____

DOT Bat Assessment Form Instructions

1. Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges, regardless of whether assessments have been conducted in the past.
2. Any bridge/structure suspected of providing habitat for any species of bat will be removed from work schedules until such time that the DOT has coordinated with the USFWS. Additional studies may be undertaken by the DOT to determine what species may be utilizing each structure identified as supporting bats prior to allowing any work to proceed.
3. Any questions should be directed to the District Environmental Manager.

Last Revised June 2017

APPENDIX D: Bridge/Structure Assessment Form

This form will be completed and submitted to the District Environmental Manager by the Contractor prior to conducting any work below the deck surface either from the underside; from activities above that bore down to the underside; from activities that could impact expansion joints; from deck removal on bridges; or from structure demolition for bridges/structures within 1000 feet of suitable bat habitat.

DOT Project # 1700194	Water Body N/A	Date/Time of Inspection 6/24/2019 10:30 AM	Within 1,000ft of suitable bat habitat (circle one) Yes No
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Route	County	Federal Structure ID
SR 252	Franklin	N/A (westernmost culvert)

If the bridge/structure is 1,000 feet or more from suitable bat habitat (e.g., an urban or agricultural area without suitable foraging habitat or corridors linking the bridge to suitable foraging habitat), check box and STOP HERE. No assessment required.

Please submit to the U.S. Fish and Wildlife Service.

Areas Inspected (Check all that apply)

Bridges		Culverts/Other Structures		Summary Info (circle all that apply)			
All vertical crevices sealed at the top and 0.5-1.25" wide & ≥4" deep		Crevices, rough surfaces or imperfections in concrete	X	Human disturbance or traffic under bridge/in culvert or at the structure	High	Low	None
All crevices >12" deep & not sealed		Spaces between walls, ceiling joists	X	Possible corridors for netting	None/poor	Marginal	Excellent
All guardrails							
All expansion joints							
Spaces between concrete end walls and the bridge deck							

Last Revised May 31, 2017

Vertical surfaces on concrete I-beams							
---------------------------------------	--	--	--	--	--	--	--

Evidence of Bats (Circle all that apply) Presence of one or more indicators is sufficient evidence that bats may be using the structure.

None

Visual (e.g. survey, thermal, emergent etc.)

- Live __ number seen
- Dead __ number seen

Photo documentation Y/N

Guano

Odor Y/N

Photo documentation Y/N

Staining definitively from bats

Photo documentation Y/N

Audible

Assessment Conducted By: <u>Chris Kunkel</u> Signature(s): <u><i>Chris Kunkel</i></u>
District Environmental Use Only: Date Received by District Environmental Manager: _____

DOT Bat Assessment Form Instructions

1. Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges, regardless of whether assessments have been conducted in the past.
2. Any bridge/structure suspected of providing habitat for any species of bat will be removed from work schedules until such time that the DOT has coordinated with the USFWS. Additional studies may be undertaken by the DOT to determine what species may be utilizing each structure identified as supporting bats prior to allowing any work to proceed.
3. Any questions should be directed to the District Environmental Manager.

Last Revised June 2017



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Joe McGuinness, Commissioner

Ruth Hook
Lochmueller Group, Inc.
(317) 222-3880
rhook@lochgroup.com

August 8, 2019

Re: Early Coordination Review, Des. 1700194

State Road (SR) 252, 0.8 mile east of US 52 – Slide Correction, State Project, Brookville Township, Franklin County, Indiana

Dear Ms. Hook:

The Indiana Department of Transportation (INDOT) Environmental Services Division (ESD) appreciates the opportunity to assist you on the project referenced above. Pursuant to your early coordination request for an environmental review, we have performed a preliminary search of the project area.

There appears to be at least one active project you should be aware of that is adjacent to Des. 1700194's project area. A summary of this project is provided below. Contact information for the project manager is listed below if you would like to request additional information.

DES: 1801067: SR 252 HMA Overlay, Preventive Maintenance, 0.42 miles East of US 52 (Bridge over Whitewater River) to District line
Project Sponsor: Indiana Department of Transportation
Project Manager: Nicole Curry
Timeline: Letting scheduled 07/08/2020

Appropriate hazardous materials investigations should be conducted in areas of excavation. If during the hazardous material investigation, sites are identified that have a reasonable potential to impact the project area(s), ESD recommends that the Indiana Department of Environmental Management's (IDEM) Virtual File Cabinet (VFC) be consulted. The VFC will provide information that is useful in assessing the risk of impacts.

As always, be sure to follow all applicable processes as well as federal and state laws and local requirements. Thank you for the opportunity to assist you with your project. If you have any questions, please contact a member of my staff, Terri Fair: 317-232-0680 or TFair@indot.IN.gov.

Sincerely,

Ron Bales
Environmental Policy Manager,
Environmental Services Division

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-21687

Request Received: July 18, 2019

Requestor: Lochmueller Group Inc
Ruth Hook
3502 Woodview Trace, Suite 150
Indianapolis, IN 46268

Project: SR 252 slide correction along about 1400' of roadway, and replacement of 3 small structures, about 0.8 mile east of US 52; Des #1700194

County/Site info: Franklin

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Stream Crossings:

The Environmental Unit recommends bridges rather than culverts and bottomless culverts rather than box or pipe culverts. Wide culverts are better than narrow culverts, and culverts with shorter through lengths are better than culverts with longer through lengths. If box or pipe culverts are used, the bottoms should be buried a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the OHWM width); maintain the natural stream substrate within the structure; have a minimum openness ratio (height x width / length) of 0.25; and have stream depth, channel width, and water velocities during low-flow conditions that are approximate to those in the natural stream channel.

2) Bank Stabilization & Wildlife Passage:

The replacement structures and any bank stabilization should not create conditions that are less favorable for wildlife passage compared to current conditions. Minimize the use of riprap and use alternative erosion protection materials whenever possible. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Where riprap must be used, we recommend placing only enough riprap to provide stream bank toe protection, such as from the toe of the bank up to the ordinary high water mark (OHWM). The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to the area and specifically for

State of Indiana
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Division of Fish and Wildlife
Early Coordination/Environmental Assessment

stream bank/floodway stabilization purposes as soon as possible upon completion.

While hard armoring alone (e.g. riprap or glacial stone) may be needed in certain instances, soft armoring and bioengineering techniques should be considered first. In many instances, one or more methods are necessary to increase the likelihood of vegetation establishment. Combining vegetation with most bank stabilization methods can provide additional bank protection and help reduce impacts upon fish and wildlife. If hard armoring is needed, wildlife passage can be facilitated by using a smooth-surfaced armoring material instead of riprap, such as articulated concrete block mats, fabric-formed concrete mats, or other similar smooth-surfaced material.

Information about bioengineering techniques can be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Also, the following is a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: <http://directives.sc.egov.usda.gov/17553.wba>.

3) Riparian Habitat:

We recommend a mitigation plan be developed for any unavoidable habitat impacts that will occur. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at: <http://www.in.gov/legislative/iac/20190130-IR-312190041NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees).

4) Wetland Habitat:

Due to the presence or potential presence of wetland habitat on site, we recommend contacting and coordinating with the Indiana Department of Environmental Management (IDEM) 401 program and also the US Army Corps of Engineers (USACE) 404 program. Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT/IDNR/USFWS Memorandum of Understanding.

5) Cofferdams:

Project designs should avoid inclusion of a cofferdam. Such features result in impacts to the stream and surrounding habitat. If a cofferdam is deemed critical for the construction to occur, justification must be provided with any permit application. Any proposed dewatering must be detailed using the following guidelines:

- a. Dewatering must be limited to one side or streambank of the creek/bridge construction site at a time so at least half of the creek is always flowing naturally. On larger streams both sides can be dammed at once as long as the center of the channel is allowed to flow naturally.
- b. Do not dewater directly into the stream. Dewater into a sediment bag into a roll off box and onto a riprap apron or similar system.
- c. Cofferdam materials and methods can vary (sheet piling, water-filled bags or tubes, sand bags, silt or sediment bags, riprap dams, removable dam systems, etc.). Self-contained and encapsulated materials and methods are preferred. Anything filled with water is better than particulate filled systems where there is a potential for leaking or failure of the system due to length of use or accidents.
- d. Dewatering pumps must incorporate filters or bypasses to avoid injuring or killing fish and other aquatic organisms.

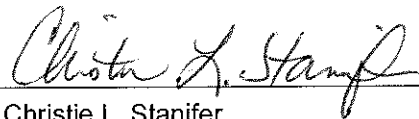
State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that will not be mowed and maintained with a mixture of grasses, sedges, wildflowers, shrubs and trees native to Eastern Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in regularly mowed areas only.
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure.
6. Do not construct any temporary runarounds or causeways.
7. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.
8. Do not use broken concrete as riprap.
9. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
10. Minimize the movement of resuspended bottom sediment from the immediate project area.
11. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
12. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: August 16, 2019

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

December 30, 2019

Ruth Hook
Lochmueller Group, Inc.
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Dear Ms. Hook:

The proposed project to address the deteriorating condition of State Road 252 because of a gradual land slide occurring along this road in Brookville Township, Franklin County, Indiana, (Des No 1700194), as referred to in your letter received July 18, 2019, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

JERRY RAYNOR Digitally signed by JERRY
RAYNOR
Date: 2020.01.06 22:42:14 -05'00'

JERRY RAYNOR
State Conservationist

Helping People Help the Land.



USDA is an equal opportunity provider, employer and lender.

Categorical Exclusion

Appendix D

**Section 106 of the National Historic
Preservation Act (NHPA)**

Minor Projects PA Project Assessment Form

Date: 7/5/2019

Project Designation Number: 1700194

Route Number: SR 252

Project Description: Slide Correction, 0.8 miles east of US 52

The Indiana Department of Transportation (INDOT) Seymour District with funding from the Federal Highway Administration (FHWA) is proposing to proceed with a slide correction along SR 252 approximately 0.3 mile east of Brookville, Indiana in Franklin County. The project is located within Section 28 of Township 9 North, Range 2 West of the USGS 7.5' series Brookville and Whitcomb, Indiana, topographic quadrangle maps.

SR 252 roadway is a two-lane rural (minor) arterial roadway. The project location is approximately 1,000 feet long and consists of two 12 foot wide through lanes. The SR 252 roadway embankment is failing due to landslides and slope movements.

The proposed improvement includes stabilizing the road side embankment with slide correction measures including a drilled shaft wall with tiebacks. As design progresses, designers will determine the specific details of the slide correction measure. The project location contains two corrugated metal pipe culverts, one of which is embedded in a large stone retaining wall. Design specifications have not been finalized, but the culverts may require replacement or rehabilitation as part of this project. Additional right-of-way (R/W) will be necessary to accommodate the slide correction, but the amounts have yet to be determined.

Feature crossed (if applicable):

Township: Brookville Township

City/County: Franklin County

Information reviewed (please check all that apply):

General project location map USGS map Aerial photograph

Written description of project area General project area photos

Previously completed archaeology reports Interim Report

Previously completed historic property reports

Soil survey data Bridge inspection information

Other (please specify): SHAARD; SHAARD GIS; project information submitted by Weintraut & Associates, dated 6/24/2019 (on file at INDOT-CRO)

Last revised 1-2-07

Does the project appear to fall under the Minor Projects PA? yes no

If yes, please specify category and number (**applicable conditions are highlighted**):

A-9. Installation, repair, or replacement of erosion control measures along roadways, waterways and bridge piers within previously disturbed soils; and

9. Installation, replacement, repair, lining, or extension of culverts and other drainage structures under the conditions listed below [**BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied**]:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. **Work occurs in previously disturbed soils; OR**
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

One of the conditions below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work does not involve installation of a new culvert and other drainage structure, and there are no impacts to unusual features, including but not limited to historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and retaining walls, under one of the following conditions (*Condition a, Condition b, or Condition c must be satisfied*):
 - a. The structure exhibits no wood, stone, or brick structures or parts therein; *OR*
 - b. The structure exhibits only modern wood, stone, or brick structures or parts therein; *OR*
 - c. The structure exhibits non-modern wood, stone, or brick structures or parts therein and the following conditions are met (*BOTH Condition 1 AND Condition 2 must be met*):
 1. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; *AND*
 2. The structure lacks sufficient integrity and/or a context that suggests it might have engineering or historical significance. Under this condition, a qualified professional (meeting the Secretary of Interior's Professional Qualification standards [48 Federal Register (FR) 44716]) must prepare an analysis and justification that the structure lacks sufficient integrity and/or a context that suggests it might have engineering or historical significance. This documentation must be reviewed and approved by INDOT Cultural Resources Office.
- ii. **Work involves the installation of a new culvert and other drainage structures AND/OR there may be impacts to unusual features, including historic brick or stone sidewalks, curbs or curb ramps, stepped or elevated sidewalks and retaining walls, under the following conditions (*BOTH Condition a and Condition b must be satisfied*):**
 - a. **Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; AND**
 - b. The subject structure exhibits one of the characteristics described below (*Condition 1, Condition 2 or Condition 3 must be satisfied*).
 1. The structure exhibits no wood, stone, or brick structures or parts therein; *OR*
 2. The structure exhibits only modern wood, stone, or brick structures or parts therein; *OR*

3. The structure exhibits non-modern wood, stone, or brick structures or parts therein but lacks sufficient integrity and/or a context that suggests it might have engineering or historical significance. Under this condition, a qualified professional (meeting the Secretary of Interior's Professional Qualification standards [48 Federal Register (FR) 44716]) must prepare an analysis and justification that the structure lacks sufficient integrity and/or a context that suggests it might have engineering or historical significance. This documentation must be reviewed and approved by INDOT Cultural Resources Office.

If no, please explain:

Additional comments:

With regard to above-ground resources, an INDOT Cultural Resources historian who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Franklin County. The project area is not located within or adjacent to any listed resources. Brookville Historic District (NR-0028) is located approximately 1,200 feet west of the project area.

The *Franklin County Interim Report* (2011; Brookville Township Scattered Sites) of the Indiana Historic Sites and Structures Inventory (IHSSI) was also consulted. The National Register & IHSSI information is available in the Indiana State Historic Architectural and Archaeological Research Database (SHAARD), and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). The SHAARD and IHBBCM information was checked against the Interim Report hard copy maps. No IHSSI properties are located within or adjacent to the project area.

The area surrounding the project area is hilly and densely wooded, which significantly limits the area of potential effects. No aboveground structures are located adjacent to the project area.

The project involves the rehabilitation or replacement of two corrugated metal pipe culverts. Structure numbers for these pipes could not be found.

Due to the presence of a stone retaining wall that may be impacted by the project, Condition B-ii-b-3 requires an assessment by a Qualified Professional (QP) historian. A QP historian from Weintraut & Associates (W&A), who are under contract to advance the environmental documentation for this project, assessed the potential significance and integrity of the stone wall:

Research Summary:

W&A was unable to locate any prior plans or specific information regarding this culvert and retaining wall structure. W&A consulted with INDOT Research and Documents Library but no plans or surveys for this structure were located. W&A also received information from John Kurtz of INDOT's Southeast District regarding the history of the State Road 252 roadway (previously known as the Colerain Turnpike and Mt. Carmel Road). The information from Mr. Kurtz indicated that the general route has been utilized since the 1830s and was an early turnpike.¹ County records obtained by INDOT indicate the road was an early state road but offer no

¹ "Turnpike Charters," *Indiana American* (Brookville, Ind: March 1, 1839), 1, obtained from John Kurtz, INDOT Southeast District.

information about its width.² The roadway was officially absorbed into the new State Highway system and designated SR 252 in 1931.³

Field Observations:

The structure involved in this project is a large, stacked stone retaining wall pierced by a corrugated metal pipe (CMP) on the south side of SR 252 (downslope, photos 1-3) and a head wall constructed of similar stone with a recessed smooth iron pipe on the north side (upslope, photos 4-6).

The south side of the CMP is positioned in the stone retaining wall about ten feet above the sloped surface of the creek and about three to four feet below the surface of the roadway. The wall's stonework lies directly against the sides of the CMP and appears to have been constructed specifically to accommodate the CMP's diameter (photos 7-10.) Additionally, the stone retaining wall exhibits no evidence of having been disturbed to insert the CMP. These observations suggest that the wall was constructed at the same time the pipe was inserted—likely the later twentieth century (photo 10.)

Cement mortar is evident in many spaces between stones in the wall. (See photos 11-13.) Also, the top edge of the retaining wall sits back slightly from the rest of the wall and has some larger patches of cement, perhaps indicating the former location of a support or retaining apparatus (photos 1-3.) The historians observed stones in the bottom of the creek bed beneath the pipe that looked similar to those in the retaining wall. Historians speculate that the large stone retaining wall, which is positioned in a severely sloped area and is subject to a high volume of runoff, washed out in the past and was rebuilt with existing stones and that its culvert pipe was replaced with a corrugated metal pipe. Alternatively, the wall and CMP were installed as part of a road widening project in the mid-to-late twentieth century that extended the existing smooth iron pipe structure. The use of modern design, workmanship, and materials in the structure, as exhibited by the presence of a modern corrugated metal pipe and a reconstructed or significantly altered stone wall on its southern end, diminishes the integrity of the culvert and the historic feeling and association between the structure and the early SR 252 roadway.

Conclusion:

After a site visit and physical inspection of the CMP culvert and stone retaining wall on May 17, 2019, the stone retaining wall on the structure's south elevation appears to be of mid-to-late twentieth century construction. The stone headwall and smooth iron culvert pipe on the structure's north elevation likely dates to the late nineteenth or early twentieth century. Based upon the previously mentioned alterations and upgrades, the culvert lacks integrity and/or a context that suggests it might have engineering or historical significance. Therefore, the undertaking is qualified to proceed under Category B-9, Condition B (ii)(b)(3) of the MPPA.

After review of the information provided by W&A, INDOT-CRO concurs with the assessment. Maps and photographs provided by W&A are attached to this form.

Based on the available information, as summarized above, no aboveground concerns exist.

With regard to archaeological resources, the proposed project is limited to replacing two corrugated metal pipes under SR 252 and slide repairs within the proposed project area. Within the project area SR 252 is cut into a steep slope that grades north to south. Though the roadbed is

² "Brookville-Scipio State Road," Road Record #1, 219-222 (State Road Record, 58), May 5, 1837, obtained from John Kurtz, INDOT Southeast District.

³ "Roads Added to State System," *The Daily Reporter* (Greenfield, Ind: May 15, 1931), 1, accessed June 3, 2019, available at newspapers.com.

level, the land to either side is not. All work will occur in disturbed or steeply sloping soils. Soils within the project area range in slope from 25-50% which is too steep for archaeological deposits to exist. According to SHAARD, no archaeological sites are documented within or adjacent to the project area. Since work is confined to replacing and repairing existing structures in previously disturbed and sloping soils, there are no archaeological concerns.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction in the immediate area of the find will be stopped, and the INDOT Cultural Resources Section and the Division of Historic Preservation and Archaeology will be notified immediately.

INDOT Cultural Resources staff reviewer(s): Anthony Ross and Shaun Miller

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

Last revised 1-2-07

Categorical Exclusion

Appendix E

**Red Flag Investigation
& Hazardous Materials**



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 232-5113
FAX: (317) 233-4929

Eric Holcomb, Governor
Joe McGuinness,
Commissioner

Date: June 17, 2019

To: Site Assessment & Management
Environmental Policy Office - Environmental Services Division
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

From: Ruth Hook
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, IN
rhook@lochgroup.com

Re: RED FLAG INVESTIGATION
DES 1700194, State Project
Slide Correction Project
SR 252
Franklin County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The Federal Highway Administration and the INDOT – Seymour District propose to proceed with a slide correction project along SR 252 east of the town of Brookville in Franklin County, Indiana. The proposed improvement includes stabilizing the road side embankment with slide correction measures including a drilled shaft wall with tiebacks. As design progresses, designers will determine the specific details of the slide correction measure. The project location contains two corrugated metal pipe culverts, one of which is embedded in a large stone retaining wall. Design specifications have not been finalized, but the culverts may require replacement or rehabilitation as part of this project.

Bridge and/or Culvert Project: Yes No Structure # Not applicable

If this is a bridge project, is the bridge Historical? Yes No , Select Non-Select

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary # Acres _____ Permanent # Acres 1.5, Not Applicable

Type of excavation: Excavation will occur to drill shafts down approximately 30 feet as a part of the wall. Additional excavation to replace pavement and the existing wall will occur to a maximum depth of 10 feet.

Maintenance of traffic: Maintenance of traffic has not been finalized but will likely involve the closure of SR 252 within the project area. If a closure is required a signed detour will be established. The maintenance of traffic will follow *INDOT Design Manual* guidelines.

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Work in waterway: Yes No Below ordinary high water mark: Yes No

State Project: LPA:

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	3*	Recreational Facilities	1
Airports ¹	N/A	Pipelines	N/A
Cemeteries	N/A	Railroads	N/A
Hospitals	N/A	Trails	1
Schools	N/A	Managed Lands	N/A

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

Explanation:

Religious Facilities*: One (1) religious facility is mapped within the 0.5 mile search radius. A review of recent aerial mapping and the USGS topographic map indicate that there are two (2) additional religious facilities located within the 0.5 mile search radius. The nearest religious facility, First Baptist Church, is located 0.3 mile northwest of the project area. No impact is expected.

Recreational Facilities: One (1) recreational facility is located within the 0.5 mile search radius. The recreational facility, Randolph Playground, is located 0.45 mile northwest of the project area. A review of recent aerial mapping indicate that this point in is in the wrong location. No impact is expected.

Trails: One (1) trail is located within the 0.5 mile search radius. The trail, Brookville Parks Trails, is located 0.34 mile northwest of the project area. No impact is expected.

WATER RESOURCES TABLE AND SUMMARY

Water Resources			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	8
Canal Structures – Historic	N/A	Lakes	6
NPS NRI Listed	N/A	Floodplain - DFIRM	1
NWI-Lines	N/A	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	N/A	Sinkhole Areas	N/A
Rivers and Streams	10	Sinking-Stream Basins	N/A

Explanation:

Rivers and Streams: Ten (10) river and stream segments are located within the 0.5 mile search radius. One unnamed stream is located within the project area. A Waters of the US Report will be prepared and coordination with INDOT ES Ecology and Waterway Permitting will occur.

National Wetland Inventory (NWI) – Wetlands: Eight (8) NWI – wetlands are located within the 0.5 mile search radius. The closest wetland is located 0.07 acre north of the project area. No impact is expected.

Lakes: Six (6) lakes are located within the 0.5 mile search radius. The closest lake is located 0.15 acre southeast of the project area. No impact is expected.

Floodplain – DFIRM: One (1) floodplain is located within the 0.5 mile search radius. The floodplain is located 0.20 mile west of the project area. No impact is expected.

URBANIZED AREA BOUNDARY SUMMARY

Explanation: This project is not located within an urbanized area boundary.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	2	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation:

Petroleum Wells: Two (2) petroleum wells are located within the 0.5 mile search radius. The closest well is located 0.42 mile west of the project area. No impact is expected.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	1	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	N/A
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	N/A

Leaking Underground Storage (LUST) Sites	N/A	Notice of Contamination Sites	N/A
--	-----	-------------------------------	-----

Explanation:

UST Sites: One (1) UST is located within the 0.5 mile search radius. The UST site, the Pepsi Cola Bottling Company (Agency Interest ID# 31200), is located 0.35 mile west of the project area. Due to the distance from the project area, no impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Franklin County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did not indicate the presence of ETR species within the 0.5 mile search radius. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

An inquiry using the USFWS Information for Planning and Consultation (IPaC) website did not indicate the presence of the federally endangered species, the Rusty Patched Bumble Bee, in or within 0.5 mile of the project area. No impact is expected.

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE: N/A

WATER RESOURCES: The presence of the following water resources will require the preparation of a Waters of the US Report and coordination with INDOT ES Ecology and Waterway Permitting:

- One (1) unnamed stream segment is located within the project area.

URBANIZED AREA BOUNDARY: N/A

MINING/MINERAL EXPLORATION: N/A

HAZMAT CONCERNS: N/A

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

INDOT Environmental Services concurrence: _____ (Signature)
 Nicole Fohey Breting Digitally signed by Nicole Fohey-Breting
 Date: 2019.08.16 14:16:47 -04'00'

Prepared by:



Ruth Hook, CPESC, CESSWI
Environmental Biologist
Lochmueller Group

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

INFRASTRUCTURE: YES

WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: N/A

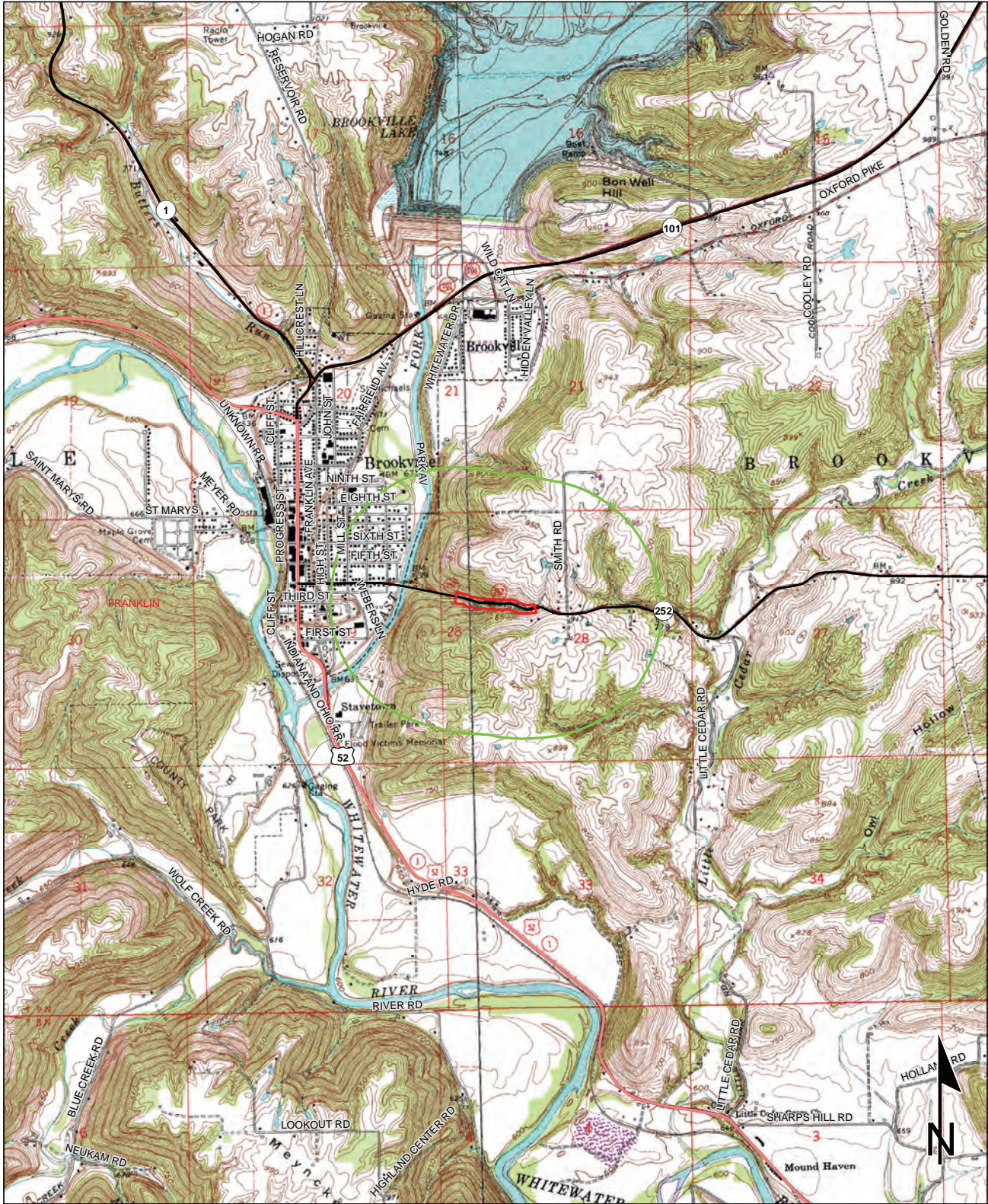
MINING/MINERAL EXPLORATION: YES

HAZMAT CONCERNS: YES

Additional Attachments

Franklin County ETR List

Red Flag Investigation - Site Location
 SR 252, 0.8 mile east of US 52
 Des. No. 1700194, Slide Correction
 Franklin County, Indiana



Sources: 0.5 0.25 0 0.5 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**BROOKVILLE & WHITCOMB
 QUADRANGLES
 INDIANA 7.5 MINUTE SERIES
 (TOPOGRAPHIC)**

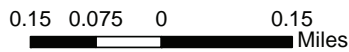
Red Flag Investigation - Infrastructure
 SR 252, 0.8 mile east of US 52
 Des. No. 1700194, Slide Correction
 Franklin County, Indiana



Sources: 0.15 0.075 0 0.15 Miles
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
 Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

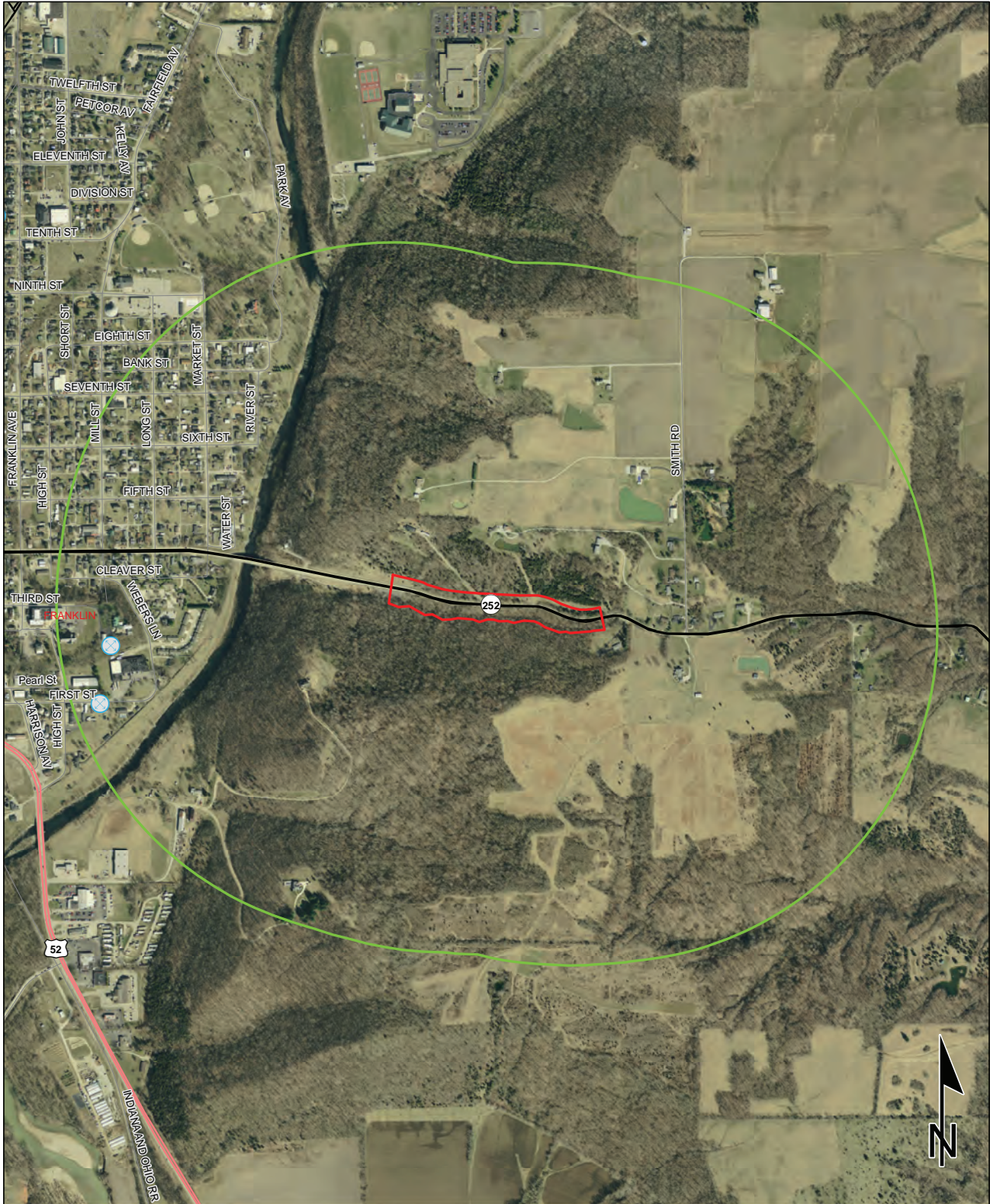
Red Flag Investigation - Water Resources
 SR 252, 0.8 mile east of US 52
 Des. No. 1700194, Slide Correction
 Franklin County, Indiana



Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

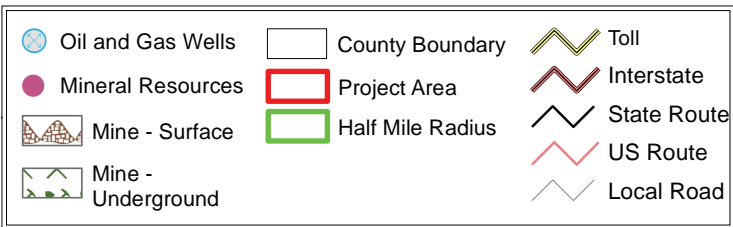
NW1 - Point	Wetlands	Project Area
Karst Spring	Lake	Half Mile Radius
NW1 - Line	Floodplain - DFIRM	Toll
Impaired_Stream_Lake	Cave Entrance Density	Interstate
NPS NRI listed	Sinkhole Area	State Route
River	Sinking-Stream Basin	US Route
Canal Structure - Historic	County Boundary	Local Road
Canal Route - Historic		

Red Flag Investigation - Mining/Mineral Resources
 SR 252, 0.8 mile east of US 52
 Des. No. 1700194, Slide Correction
 Franklin County, Indiana

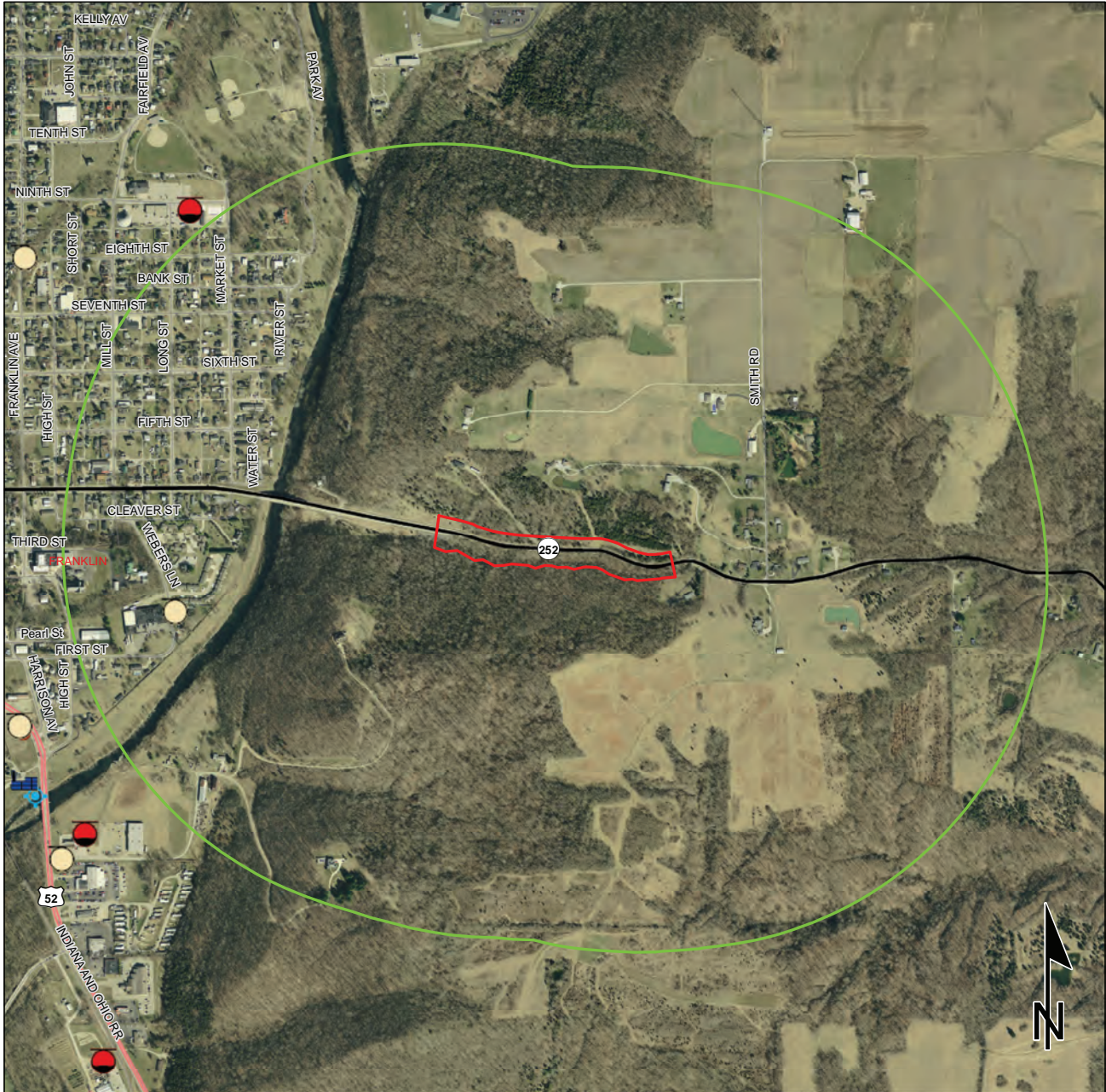


Sources: 0.15 0.075 0 0.15 Miles

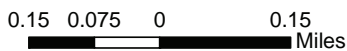
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Red Flag Investigation - Hazardous Materials Concerns
 SR 252, 0.8 mile east of US 52
 Des. No. 1700194, Slide Correction
 Franklin County, Indiana



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation		Septage Waste Site		Project Area
	Notice_of_Contamination		Solid Waste Landfill		Half Mile Radius
	Construction/Demolition Site		State Cleanup Site		Toll
	Infectious/Medical Waste Site		Superfund		Interstate
	Leaking Underground Storage Tank		Tire Waste Site		State Route
	Manufactured Gas Plant		Underground Storage Tank		US Route
	NPDES Facilities		Voluntary Remediation Program		Local Road
	NPDES Pipe Locations		Waste Transfer Station		
	Open Dump Waste Site				



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
 Map Projection: UTM Zone 16 N Map Datum: NAD83

Indiana County Endangered, Threatened and Rare Species List

County: Franklin

Species Name	Common Name	FED	STATE	GRANK	SRANK
Mollusk: Bivalvia (Mussels)					
Ptychobranhus fasciolaris	Kidneyshell		SSC	G4G5	S2
Insect: Coleoptera (Beetles)					
Cicindela marginipennis	Cobblestone Tiger Beetle	C	SE	G2	S1
Insect: Lepidoptera (Butterflies & Moths)					
Polygonia progne	Gray Comma		SR	G5	S2
Fish					
Clinostomus elongatus	Redside Dace		SE	G3G4	S1
Etheostoma variatum	Variagate Darter		SE	G5	S1
Amphibian					
Cryptobranhus alleganiensis alleganiensis	Eastern Hellbender	C	SE	G3G4T3T4	S1
Bird					
Aimophila aestivalis	Bachman's Sparrow			G3	SXB
Haliaeetus leucocephalus	Bald Eagle		SSC	G5	S2
Pandion haliaetus	Osprey		SE	G5	S1B
Setophaga magnolia	Magnolia Warbler			G5	SNA
Mammal					
Taxidea taxus	American Badger		SSC	G5	S2
Vascular Plant					
Acalypha deamii	Mercury		SR	G4?	S2
Cypripedium calceolus var. parviflorum	Small Yellow Lady's-slipper		SR	G5	S2
Gentiana alba	Yellow Gentian		SR	G4	S2
Lilium canadense	Canada Lily		SR	G5	S2
Onosmodium hispidissimum	Shaggy False-gromwell		SE	G4G5T4	S1
Rubus centralis	Illinois Blackberry		SE	G2?Q	S1
Rubus deamii	Deam Dewberry		SX	G4?	SX
Scutellaria parvula var. parvula	Small Skullcap		SE	G4T4	S1
Viburnum molle	Softleaf Arrow-wood		SR	G5	S2
Waldsteinia fragarioides	Barren Strawberry		SR	G5	S2
Zizia aptera	Golden Alexanders		SR	G5	S2
High Quality Natural Community					
Barrens - bedrock limestone	Limestone Glade		SG	G4	S2S3
Forest - upland mesic Bluegrass	Bluegrass Mesic Upland Forest			GNR	S3
Other Significant Feature					
Geomorphic - Nonglacial Erosional Feature - Water Fall and Cascade	Water Fall and Cascade			GNR	SNR

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Categorical Exclusion
Appendix F
Water Resources

Waters Report
State Road 252 Slide Correction Project
0.80 mi. E of US 52
Franklin County, Indiana
Des. No. 1700194



Prepared By:



3502 Woodview Trace, Suite 150
Indianapolis, IN, 46268
Ph: 317-222-3880

Prepared For:

INDOT – Seymour District
185 Agrico Ln.
Seymour, Indiana 47274

August 19, 2019

Waters of the U.S. Determination Report
State Road 252 Slide Correction
0.80 mi. E of US 52
Franklin County, Indiana
Des. No. 1700194

Date of Waters Investigation

June 24, 2019

Location

The project is located in central Franklin County, 0.80 mile east of US 52 in Franklin County, Indiana (Attachment A1).

- Brookville Township, Franklin County, Indiana
- Section 28, Township 9 North, Range 2 West
- Brookville & Whitcomb 1:24,000 United States Geological Survey (USGS) Quadrangles (Attachment A2 and A3)
- Latitude: 39.417128° Longitude: -84.998242° (center of project area along SR 252)

Project Description

The Indiana Department of Transportation – Seymour District proposes to proceed with a slide correction project in central Franklin County, Indiana. The proposed project will involve the stabilization of the roadway side slopes along State Road (SR) 252, 0.80 mile east of US 52. The proposed project will reconstruct the roadway, construct a pier wall with pier shafts, and the replacement of the existing culverts conveying drainage under US 252. The maintenance of traffic will likely require a full closure of SR 252 and a detour will be established. The project investigation area is generally upland forested with a significant slope from the north side of SR 252 to the south side.

Soils

According to the Soil Survey Geographic (SSURGO) database for Franklin County, Indiana, the project area does not contain soil areas with nationally listed hydric soils. The table below includes the following mapped soil series within the SR 252 Slide Correction Project (Attachments A7 to A11).

Soil Name	Map Abbreviation	Hydric Range
Eden flaggy silty clay, eroded	EbE2	Not Hydric (0)
Eden flaggy silty clay	EdG	Not Hydric (0)

National Wetlands Inventory (NWI)

Based on the U.S. Fish and Wildlife National Wetlands Inventory (NWI) data (www.fws.gov/wetlands/Data/State-Downloads.html) there is one wetland polygon mapped within the project area (Attachment A13). The mapped wetland represents a reach of an unnamed tributary (UNT) to East Fork Whitewater River. This NWI wetland is a riverine, intermittent, streambed, seasonally flooded (R4SBC) feature. There are 7 additional NWI wetland resources mapped near the project area:



Wetland Type	Description	Location: Lat/Long
R2UBH	Riverine, lower perennial, unconsolidated bottom, permanently flooded	39.418164, -85.005071
R4SBC	Riverine, intermittent, streambed, seasonally flooded	39.417849, -84.991467
R4SBC	Riverine, intermittent, streambed, seasonally flooded	39.411694, -84.994234
PUBGh	Palustrine, unconsolidated bottom, intermittently exposed, diked/impounded	39.419125, -84.994085
PUBGh	Palustrine, unconsolidated bottom, intermittently exposed, diked/impounded	39.418529, -84.992111
PUBGh	Palustrine, unconsolidated bottom, intermittently exposed, diked/impounded	39.415744, -84.990952
PUBG	Palustrine, unconsolidated bottom, intermittently exposed, excavated	39.418412, -84.997818

12-Digit Hydrologic Unit Code (HUC):

The entirety of the SR 252 Slide Correction project area is within the Brookville Lake-East Fork Whitewater River 12-Digit HUC (050800030804).

Attached Documents

- Indiana State Location Map
 - USGS Topographic Maps
 - Water Resources Map
 - USDA Soil Map
 - Franklin County Hydric Soil List and Components
 - USGS StreamStats Watershed Map
 - NWI Map
 - FEMA FIRM
 - Photo Location Map and Project Photos
 - USACE Preliminary Jurisdictional Determination Form
- Removed to avoid duplication; see Appendix B

Field Reconnaissance

Lochmueller Group conducted a field review for streams and wetlands within the investigation area for the SR 252 Slide Correction project on June 24, 2019. Four unnamed tributaries (UNTs) to East Fork Whitewater River and one roadside ditch (RSD) were identified within the project area. The reach of each waterway was delineated using a Trimble R1 GIS receiver (sub-meter accuracy) and ESRI Collector. No wetland features were identified within the investigation area. The investigation area limits were established based on the area that may be affected by the project work included in the scope of the project. Identified features from the field reconnaissance can be seen in photos in the Attachments, pages A18 to A35.



Stream Analysis

The June 2019 field investigation for the SR 252 Slide Correction Project resulted in the evaluation of four likely jurisdictional stream features. One RSD not exhibiting an ordinary high water mark (OHWM) was observed.

UNT 1 to East Fork Whitewater River

UNT 1 to East Fork Whitewater River is the only waterway within the investigation area that shows as an intermittent blue-line feature on the Brookville and Whitcomb 1:24,000 scale USGS Topographic Maps. According to USGS StreamStats (<https://water.usgs.gov/osw/streamstats/>), the drainage area upstream of the project location is approximately 0.144 square mile. UNT 1 to East Fork Whitewater River flows from east to west along the south side of SR 252 at the base of a steep slope. Approximately 2,319 feet of UNT 1 to East Fork Whitewater River are within the investigation area. Approximately 0.21 mile west of the project survey area, UNT 1 to East Fork Whitewater River flows into East Fork Whitewater River which flows into the Whitewater River, a traditionally navigable water (TNW).

The reach of UNT 1 to East Fork Whitewater River within the survey area is a high gradient, intermittent stream that has riffles with limited shallow pools. The streambed is predominantly gravel with cobble. Both banks of the stream are steeply sloped and consist of habitat consistent with an upland forest. Minimal erosion was observed on both sides of the bank. The ordinary high water mark (OHWM) width of UNT 1 of East Fork Whitewater River is 11.1 feet with a depth of 1.3 feet. This reach of UNT 1 to East Fork Whitewater River is considered to exhibit good quality because of its wide forested riparian habitat and gravel substrate.

UNT 1 to East Fork Whitewater River is likely to fall under the jurisdiction of the US Army Corps of Engineers (USACE) under Section 404 of the Clean Water Act (CWA) due to a defined bed and bank and direct connection with the Whitewater River, a TNW.

UNT 2 to East Fork Whitewater River

UNT 2 to East Fork Whitewater River is not represented by a blue-line feature on the Brookville and Whitcomb 1:24,000 scale USGS Topographic Maps. The origin of this resource is within a residential area at the top of a slope north of SR 252 and flows south where it connects with UNT 1 to East Fork Whitewater River south of SR 252. A portion of its reach is conveyed by two separate corrugated metal pipe (CMP) culverts, one of which flows underneath SR 252. Approximately 217 feet of UNT 2 to East Fork Whitewater River is within the investigation area.

UNT 2 to East Fork Whitewater River is a high gradient, natural, ephemeral stream with no discernible pool or riffle complexes. No water was present at the time of the field investigation. The stream bed is predominantly sand with gravel in the substrate. The floodplain upstream of UNT 2 to East Fork Whitewater River consists of residential lawns and downstream the floodplain consists of upland forest along a steep slope where it outlets into UNT 1 to East Fork Whitewater River. The OHWM width is 6.2 feet with a depth of 0.4 feet. This reach of UNT 2 to East Fork Whitewater River is considered to exhibit average quality because of its wide forested riparian habitat, the sandy substrate, and its ephemeral nature.

UNT 2 to East Fork Whitewater River is likely to fall under the jurisdiction of the USACE under Section 404 of the CWA due to a defined bed and bank and connection with the Whitewater River, a TNW.



UNT 3 to East Fork Whitewater River

UNT 3 to East Fork Whitewater River is not represented by a blue-line feature on the Brookville and Whitcomb 1:24,000 scale USGS Topographic Maps. This resource flows along the east side of a residential driveway in a southeasterly direction toward SR 252 at which point it is conveyed through a CMP under SR 252 where it outlets into UNT 1 to East Fork Whitewater River. Approximately 393 feet of this resource is within the investigation area.

UNT 3 to East Fork Whitewater River is a high gradient, ephemeral stream with no discernible pool or riffle complexes. No water was present at the time of the field investigation. The stream bed is predominantly sand with gravel in the substrate. The floodplain upstream of UNT 3 to East Fork Whitewater River consists of residential lawns and downstream the floodplain consists of upland forest along a steep slope where it outlets into UNT 1 to East Fork Whitewater River. The upper reach of this resource appears to have been channelized as drainage for the residential driveway. The OHWM width is 4.2 feet with a depth of 0.7 feet. This reach of UNT 3 to East Fork Whitewater River is considered to exhibit average quality because of its wide forested riparian habitat, the sandy substrate, and its ephemeral nature.

UNT 3 to East Fork Whitewater River is likely to fall under the jurisdiction of the USACE under Section 404 of the CWA due to a defined bed and bank and connection with the Whitewater River, a TNW.

UNT 4 to East Fork Whitewater River

UNT 4 to East Fork Whitewater River is not represented by a blue-line feature on the Brookville and Whitcomb 1:24,000 scale USGS Topographic Maps. This resource flows south down a forested hillside toward SR 252 at which point it outlets into UNT 3 to East Fork Whitewater River. Approximately 142 feet of this resource is within the investigation area.

UNT 4 to East Fork Whitewater River is a high gradient, ephemeral stream with no discernible pool or riffle complexes. The stream bed is predominantly gravel with cobble in the substrate. The floodplain upstream of UNT 4 to East Fork Whitewater River consists of residential lawns and downstream the floodplain consists of upland forest along a steep slope where it outlets into UNT 3 to East Fork Whitewater River. The OHWM width is 4.6 feet with a depth of 0.8 feet. This reach of UNT 3 to East Fork Whitewater River is considered to exhibit average quality because of its wide forested riparian habitat, gravel substrate, and its ephemeral nature.

UNT 4 to East Fork Whitewater River is likely to fall under the jurisdiction of the USACE under Section 404 of the CWA due to a defined bed and bank and connection with the Whitewater River, a TNW.



Table 1: Stream Summary

Stream Name	Photos	Lat/Long	OHW Width (feet)	OHW Depth (feet)	USGS Blue- line? Type?	Riffles? Pools?	Substrate	Water of the U.S.?
UNT 1 to East Fork Whitewater River	5-8, 16-17, 37-38	39.4172° N -85.0010° W	11.1	1.3	Yes; Intermittent	Yes Yes	Cobble/ gravel	Yes
UNT 2 to East Fork Whitewater River	28-29, 32-36	39.4172° N -84.9968° W	6.2	0.4	No	No	Gravel/ sand	Yes
UNT 3 to East Fork Whitewater River	18-23	39.4173° N -84.9983° W	4.2	0.7	No	No	Gravel/ sand	Yes
UNT 4 to East Fork Whitewater River	24-25	39.4173° N -84.9983° W	4.6	0.8	No	No	Cobble/ gravel	Yes

Wetland Analysis

The June 2019 field investigation for the SR 252 Slide Correction project did not identify any wetlands within the investigation area.

Other Features

The June 2019 field investigation identified one RSD, identified as RSD 1 on the attached map (A6). RSD 1 appears to convey drainage into UNT 2 to East Fork Whitewater River. This feature has a fully vegetated bottom, lacks a defined bed and bank, and does not display an OHWM. This feature will not likely fall under the jurisdiction of the USACE.

Conclusions

The June 2019 field review for the SR 252 Slide Correction Project identified four stream features, UNT 1, UNT 2, UNT 3, and UNT 4 to East Fork Whitewater River, within the investigation area. All identified stream features would be considered jurisdictional features due to their defined bed and bank, OHWM, and connectivity to the Whitewater River, a TNW in Franklin County. One RSD with no discernible OHWM was also observed and no wetlands were found within the investigation area.

Every effort should be taken to avoid and minimize the impacts to the water resources listed above. Disturbance of a stream could result in a mitigation requirement to secure the required permits for the slide correction project. If construction exceeds the limits of the survey review area illustrated in this document, further field investigation will be needed. This report is this office’s best judgment of water resources that are likely to be under federal jurisdiction, based on the guidelines set forth by the USACE. The final determination of jurisdictional waters is ultimately the responsibility of the USACE.

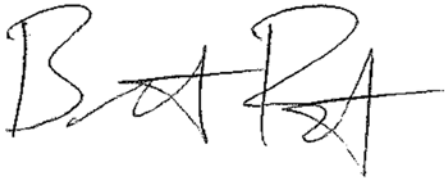


This waters determination has been prepared based on the best available information, interpreted in the light of the investigator’s training, experience and professional judgement in conformance with the 1987 *Corps of Engineers Wetlands Delineation Manual*, the appropriate regional supplement, the USACE *Jurisdictional Determination Form Instructional Guidebook*, and other appropriate agency guidelines.

Preparers

Lochmueller Group, Inc. Staff	Position	Contributing Effort
Chris Kunkel	Environmental Biologist	Field Data Collection Report Preparation
Brenten Reust	Environmental Biologist	Field Data Collection Report Preparation

Signature of Preparer:



Brenten Reust





ATTACHMENTS





Legend

-  Streams
-  Investigation Area

Source: Aerial Photography (Franklin County, 2017)

Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.




LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

Water Resources Map
 Waters of the U.S. Report
 Des. No. 1700194

0 120 240
 Feet



County: Franklin
 Township: Brookville
 State: Indiana



SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 7/17/2019, C. Kunkel

Page 1 of 3

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Legend

-  Streams
-  Investigation Area

Source: Aerial Photography (Franklin County, 2017)

Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.




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Water Resources Map
 Waters of the U.S. Report
 Des. No. 1700194

0 120 240
 Feet



County: Franklin
 Township: Brookville
 State: Indiana



SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 7/17/2019, C. Kunkel

Page 2 of 3

S:_2016\216-0534\Environ\Water\A\Attachments\media\Water_Resources\WaterResources.mxd



Legend

-  Streams
-  Investigation Area

Source: Aerial Photography (Franklin County, 2017)

Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.




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0 120 240
 Feet



County: Franklin
 Township: Brookville
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SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 7/17/2019, C. Kunkel

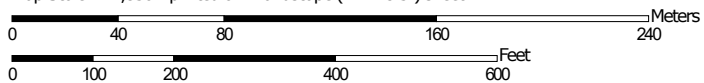
Page 3 of 3

S:_2016\216-0534\Environ\Water\Attachments\media\Water_Resources_Waters.mxd

Soil Map—Franklin County, Indiana
(Des. No. 1700194)



Map Scale: 1:2,850 if printed on A landscape (11" x 8.5") sheet.



Map projection: Web Mercator Corner coordinates: WGS84 Edge tics: UTM Zone 16N WGS84




MAP LEGEND

Area of Interest (AOI)

 Area of Interest (AOI)

Soils

 Soil Map Unit Polygons

 Soil Map Unit Lines

 Soil Map Unit Points

Special Point Features



Blowout



Borrow Pit



Clay Spot



Closed Depression



Gravel Pit



Gravelly Spot



Landfill



Lava Flow



Marsh or swamp



Mine or Quarry



Miscellaneous Water



Perennial Water



Rock Outcrop



Saline Spot



Sandy Spot



Severely Eroded Spot



Sinkhole



Slide or Slip



Sodic Spot



Spoil Area



Stony Spot



Very Stony Spot



Wet Spot



Other



Special Line Features

Water Features



Streams and Canals

Transportation



Rails



Interstate Highways



US Routes



Major Roads



Local Roads

Background



Aerial Photography

MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:15,800.

Warning: Soil Map may not be valid at this scale.

Enlargement of maps beyond the scale of mapping can cause misunderstanding of the detail of mapping and accuracy of soil line placement. The maps do not show the small areas of contrasting soils that could have been shown at a more detailed scale.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service

Web Soil Survey URL:

Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Franklin County, Indiana

Survey Area Data: Version 18, Sep 7, 2018

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Oct 2, 2011—Nov 8, 2017

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.

Map Unit Legend

Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
EbE2	Eden flaggy silty clay, 15 to 25 percent slopes, eroded	2.1	21.4%
EdG	Eden flaggy silty clay, 25 to 50 percent slopes	7.7	78.6%
Totals for Area of Interest		9.8	100.0%

Hydric Rating by Map Unit

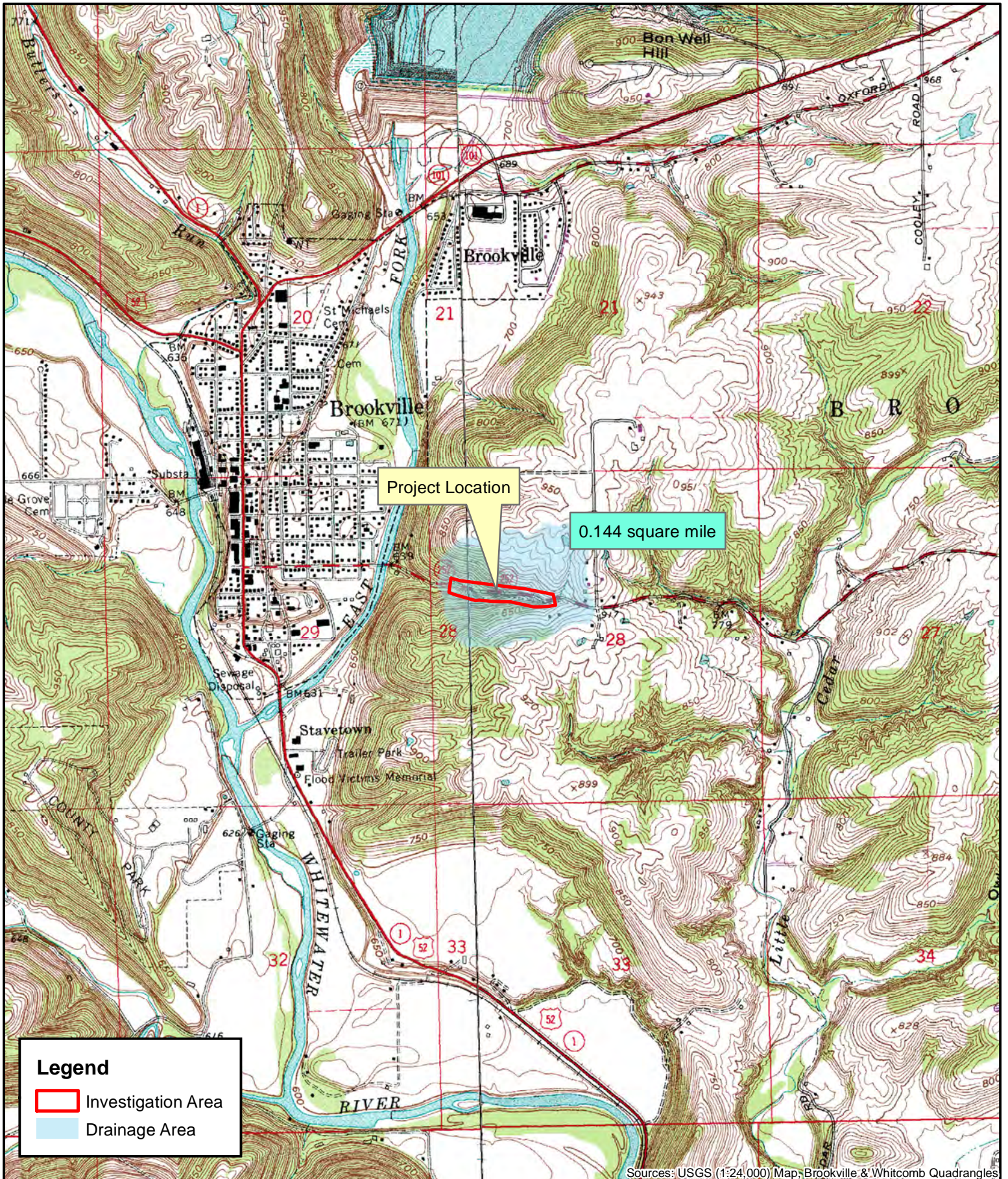
Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
EbE2	Eden flaggy silty clay, 15 to 25 percent slopes, eroded	0	2.1	21.4%
EdG	Eden flaggy silty clay, 25 to 50 percent slopes	0	7.7	78.6%
Totals for Area of Interest			9.8	100.0%

Report—Hydric Soil List - All Components

Hydric Soil List - All Components--IN047-Franklin County, Indiana					
Map symbol and map unit name	Component/Local Phase	Comp. pct.	Landform	Hydric status	Hydric criteria met (code)
EbE2: Eden flaggy silty clay, 15 to 25 percent slopes, eroded	Eden	100	Hills	No	—
EdG: Eden flaggy silty clay, 25 to 50 percent slopes	Eden	85	Hills	No	—
	Switzerland	5	Hills	No	—
	Pate	5	Hills	No	—
	Carmel	5	Hills	No	—

Data Source Information

Soil Survey Area: Franklin County, Indiana
 Survey Area Data: Version 18, Sep 7, 2018



Legend

- Investigation Area
- Drainage Area

Sources: USGS (1:24,000) Map: Brookville & Whitcomb Quadrangles

LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

USGS StreamStats Map
 Brookville & Whitcomb Quadrangles
 Des. No. 1700194

0 1,500 3,000 Feet








County: Franklin
 Township: Brookville
 State: Indiana

SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 8/16/2019, C. Kunkel



July 15, 2019

Wetlands

- | | | |
|--|---|--|
|  Estuarine and Marine Deepwater |  Freshwater Emergent Wetland |  Lake |
|  Estuarine and Marine Wetland |  Freshwater Forested/Shrub Wetland |  Other |
| |  Freshwater Pond |  Riverine |

This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or currentness of the base data shown on this map. All wetlands related data should be used in accordance with the layer metadata found on the Wetlands Mapper web site.

National Flood Hazard Layer FIRMette



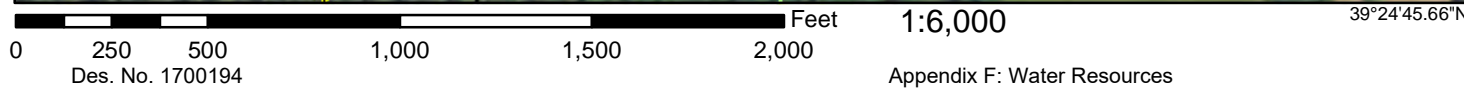
39°25'13.45"N



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

- | | | |
|-----------------------------|--|--|
| SPECIAL FLOOD HAZARD AREAS | | Without Base Flood Elevation (BFE)
<i>Zone A, V, A99</i> |
| | | With BFE or Depth <i>Zone AE, AO, AH, VE, AR</i> |
| | | Regulatory Floodway |
| OTHER AREAS OF FLOOD HAZARD | | 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile <i>Zone X</i> |
| | | Future Conditions 1% Annual Chance Flood Hazard <i>Zone X</i> |
| | | Area with Reduced Flood Risk due to Levee. See Notes. <i>Zone X</i> |
| | | Area with Flood Risk due to Levee <i>Zone D</i> |
| OTHER AREAS | | Area of Minimal Flood Hazard <i>Zone X</i> |
| | | Effective LOMRs |
| | | Area of Undetermined Flood Hazard <i>Zone D</i> |
| GENERAL STRUCTURES | | Channel, Culvert, or Storm Sewer |
| | | Levee, Dike, or Floodwall |
| OTHER FEATURES | | Cross Sections with 1% Annual Chance Water Surface Elevation
20.2
17.5 |
| | | Coastal Transect |
| | | Base Flood Elevation Line (BFE) |
| | | Limit of Study |
| | | Jurisdiction Boundary |
| | | Coastal Transect Baseline |
| MAP PANELS | | Digital Data Available |
| | | No Digital Data Available |
| | | Unmapped |
- The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.



This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards.

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on **6/19/2019 at 9:38:36 AM** and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

Appendix 2 - PRELIMINARY JURISDICTIONAL DETERMINATION (PJD) FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PJD: July 17, 2019

B. NAME AND ADDRESS OF PERSON REQUESTING PJD: Brenten Reust; 3502 Woodview Trace, Indianapolis, IN 46268

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:

D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:

The INDOT – Seymour District proposes to proceed with a slide correction project in central Franklin County, Indiana (Des. No. 1700194). The proposed project will involve the stabilization of the roadway side slopes along State Road (SR) 252, 0.80 mile east of US 52. The proposed project will reconstruct the roadway, construct a pier wall with pier shafts, and the replacement of the existing culverts conveying drainage under US 252. The maintenance of traffic will likely require a full closure of SR 252 and a detour will be established. The project investigation area is generally upland forested with a significant slope from the north side of SR 252 to the south side.

(USE THE TABLE BELOW TO DOCUMENT MULTIPLE AQUATIC RESOURCES AND/OR AQUATIC RESOURCES AT DIFFERENT SITES)

State: Indiana County/parish/borough: Franklin City: Brookville

Center coordinates of site (lat/long in degree decimal format):

Lat.: 39.417128 N Long.: -84.998242 W

Universal Transverse Mercator: 16 S 672264.70 m E 4364982.15 m N

Name of nearest waterbody: East Fork Whitewater River

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

Office (Desk) Determination. Date:

Field Determination. Date(s):

TABLE OF AQUATIC RESOURCES IN REVIEW AREA WHICH “MAY BE” SUBJECT TO REGULATORY JURISDICTION.

Site number	Latitude (decimal degrees)	Longitude (decimal degrees)	Estimated amount of aquatic resource in review area (acreage and linear feet, if applicable)	Type of aquatic resource (i.e., wetland vs. non-wetland waters)	Geographic authority to which the aquatic resource “may be” subject (i.e., Section 404 or Section 10/404)
UNT 1 to East Fork White +	39.417244° N	-85.00062° W	2319 feet (0.59 acre)	non-wetland	Section 404
UNT 2 to East Fork White +	39.417244° N	-84.996833° W	217 feet (0.03 acre)	non-wetland	Section 404
UNT 3 to East Fork White +	39.417252° N	-84.998364° W	393 feet (0.04 acre)	non-wetland	Section 404
UNT 4 to East Fork White +	39.417332° N	-84.998225° W	142 feet (0.01 acre)	non-wetland	Section 404

- 1) The Corps of Engineers believes that there may be jurisdictional aquatic resources in the review area, and the requestor of this PJD is hereby advised of his or her option to request and obtain an approved JD (AJD) for that review area based on an informed decision after having discussed the various types of JDs and their characteristics and circumstances when they may be appropriate.
- 2) In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring “pre-construction notification” (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an AJD for the activity, the permit applicant is hereby made aware that: (1) the permit applicant has elected to seek a permit authorization based on a PJD, which does not make an official determination of jurisdictional aquatic resources; (2) the applicant has the option to request an AJD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an AJD could possibly result in less compensatory mitigation being required or different special conditions; (3) the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) undertaking any activity in reliance upon the subject permit authorization without requesting an AJD constitutes the applicant’s acceptance of the use of the PJD; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a PJD constitutes agreement that all aquatic resources in the review area affected in any way by that activity will be treated as jurisdictional, and waives any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an AJD or a PJD, the JD will be processed as soon as practicable. Further, an AJD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331. If, during an administrative appeal, it becomes appropriate to make an official determination whether geographic jurisdiction exists over aquatic resources in the review area, or to provide an official delineation of jurisdictional aquatic resources in the review area, the Corps will provide an AJD to accomplish that result, as soon as is practicable. This PJD finds that there “*may be*” waters of the U.S. and/or that there “*may be*” navigable waters of the U.S. on the subject review area, and identifies all aquatic features in the review area that could be affected by the proposed activity, based on the following information:

SUPPORTING DATA. Data reviewed for PJD (check all that apply)

Checked items should be included in subject file. Appropriately reference sources below where indicated for all checked items:

- Maps, plans, plots or plat submitted by or on behalf of the PJD requestor:
Map: Location maps, topographic map, aerial map, floodplain map, NWI map
- Data sheets prepared/submitted by or on behalf of the PJD requestor.
 - Office concurs with data sheets/delineation report.
 - Office does not concur with data sheets/delineation report. Rationale: _____
- Data sheets prepared by the Corps: _____
- Corps navigable waters' study: _____
- U.S. Geological Survey Hydrologic Atlas: _____
 - USGS NHD data.
 - USGS 8 and 12 digit HUC maps.
- U.S. Geological Survey map(s). Cite scale & quad name: Brookville & Whitcomb 1:24,000
- Natural Resources Conservation Service Soil Survey. Citation: <https://websoilsurvey.sc.egov.usda.gov/App/HomePage.htm>
- National wetlands inventory map(s). Cite name: <https://ww.fws.gov/wetlands/Data/Mapper.html>
- State/local wetland inventory map(s): _____
- FEMA/FIRM maps: 18047C0192D (1/16/2014); 18047C0215D (1/16/2014)
- 100-year Floodplain Elevation is: 621.2 (National Geodetic Vertical Datum of 1929)
- Photographs: Aerial (Name & Date): Franklin County, Indiana 2017
or Other (Name & Date): Ground level photos: June 24, 2019
- Previous determination(s). File no. and date of response letter: _____
- Other information (please specify): _____

IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Signature and date of
Regulatory staff member
completing PJD

Brenten Reust 7/17/2019
Signature and date of
person requesting PJD
(REQUIRED, unless obtaining
the signature is impracticable)¹

¹ Districts may establish timeframes for requestor to return signed PJD forms. If the requestor does not respond within the established time frame, the district may presume concurrence and no additional follow up is necessary prior to finalizing an action.

Categorical Exclusion
Appendix G
Public Involvement



NOTICE OF SURVEY

February 7, 2019

Sample Notice of
Survey Letter

RE: PROJECT: State Road 252
Road Improvement Project
Brookville, Indiana

Dear Property Owner:

Our information indicates that you own or occupy property near this proposed Road Improvement construction project. Our employees will be doing a survey of the project area in the near future. It may be necessary for them to come onto your property to complete this work. This is allowed by Indiana Code IC 8-23-7-26. They will show you their identification, if you are available, before coming onto your property. If you have sold this property, or someone else occupies it, please let us know the name and address of the new owner or current occupant so we can contact them about the survey.

At this stage we generally do not know what effect, if any, our project may eventually have on your property. If we determine later your property is involved, we will contact you with additional information.

The survey work will include mapping the location of features such as buildings, trees, fences, and drives, and obtaining ground elevations. This work is necessary for the proper planning and design of the Road Improvement construction project. Please be assured of our sincere desire to cause you as little inconvenience as possible during the survey. If any problems do occur, please contact our field crew or contact me at the phone number or address shown below.

We do appreciate your input regarding any issues that this project may encounter during the design phase. Included with this notice is a short questionnaire that you can fill out and return to us in the enclosed self-addressed stamped envelope. Thank you, in advance, for your participation in this process.

Sincerely,

SJCA P.C.

A handwritten signature in blue ink that reads "Daniel G. Kovert".

Daniel G. Kovert, PE, PS
Director of Surveying
dkovert@sjca-pc.com



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204-2216

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

Indiana Department of Transportation Notice of Entry for Survey or Investigation Indiana Department of Transportation

If you have received a “Notice of Entry for Survey or Investigation” from INDOT or an INDOT representative, you may be wondering what it means. In the early stages of a project’s development, INDOT must collect as much information as possible to ensure that sound decisions are made in designing the proposed project. Before entering onto private property to collect that data, INDOT is required to notify landowners that personnel will be in the area and may need to enter onto their property. Indiana Code, Title 8, Article 23, Chapter 7, Section 26 deals with the department’s authority to enter onto any property within Indiana.

Receipt of a Notice of Entry for Survey or Investigation does not necessarily mean that INDOT will be buying property from you. It doesn’t even necessarily mean that the project will involve your property at all. Since the Notice of Entry for Survey or Investigation is sent out in the very early stages and since we want to collect data within AND surrounding the project’s limits more landowners are contacted than will actually fall within the eventual project limits. It may also be that your property falls within the project limits but we will not need to purchase property from you to make improvements to the roadway. Another thing to keep in mind is that when you receive a Notice of Entry for Survey or Investigation, very few specifics have been worked out and actual construction of the project may be several years in the future.

Before INDOT begins a project that requires them to purchase property from landowners, they must first offer the opportunity for a public hearing. If you were on the list of people who received a Notice of Entry for Survey or Investigation, you should also receive a notice informing you of your opportunity to request a public hearing. These notices will also be published in your local newspaper so interested individuals who are not adjacent to the project will also have the opportunity to request a public hearing. If a public hearing is to be held, INDOT will publicize the date, location, and time. INDOT will present detailed project information at the public hearing, comments will be taken from the public in spoken and written form, and question and answer sessions will be offered. Based on the feedback INDOT receives from the public, a project can be modified and improved to better serve the public.

So, if you have received a “Notice of Entry for Survey or Investigation”, remember:

1. You do not need to take any action at this time. It is merely letting you know that people in orange/lime vests are going to be in your neighborhood.
2. The project is still in its very early planning stages.
3. You will be notified of your opportunity to comment on the project at a later date.

www.in.gov/dot/
An Equal Opportunity Employer

Categorical Exclusion
Appendix H
Air Quality

Indiana Department of Transportation (INDOT)
 State Preservation and Local Initiated Projects FY 2020 - 2024

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
Franklin County																		
Franklin County	1800899	Init.	IR 1010	HMA Overlay, Preventive Maintenance	St. Mary's Road between Brookville Town limits to Levee Road- 1.5 miles	Seymour	1.5	STPBG		Local Bridge Program	CN	\$265,744.00	\$0.00			\$265,744.00		
										Local Funds	CN	\$0.00	\$241,032.00			\$241,032.00		
										Group IV Program	CN	\$788,536.00	\$0.00			\$788,536.00		
Indiana Department of Transportation	1900192	A 04	US 52	HMA Overlay Minor Structural	SR 244 to SR 229	Seymour	8.64	STBG	\$6,545,172.00	Road Consulting	PE	\$200,000.00	\$50,000.00	\$250,000.00				
										Road Construction	CN	\$5,036,137.60	\$1,259,034.40					\$6,295,172.00
Comments:Amend PE phase in 2020 and CN phase in 2024 to current STIP. No MPO.																		
Indiana Department of Transportation	35242 / 1296697	Init.	SR 46	Bridge Replacement, Concrete	1.22 miles W of SR-229 over Laughery Creek on SR-46	Seymour	0	STPBG		Bridge Construction	CN	\$1,709,506.40	\$427,376.60	\$2,136,883.00				
Franklin County	38175 / 1500205	Init.	VA VARI	Bridge Inspections	Countywide Bridge Inspection and Inventory Program for Cycle Years 2018-2021	Seymour	0	STPBG		Local Bridge Program	PE	\$91,501.77	\$0.00	\$9,363.14	\$70,742.85	\$11,395.78		
										Local Funds	PE	\$0.00	\$22,875.43	\$2,340.78	\$17,685.71	\$2,848.94		
Indiana Department of Transportation	38620 / 1500020	Init.	US 52	Small Structure Replacement	Over Unnamed Trib to Whitewater River 4.77 miles E of the E SR 1 junction	Seymour	0	NHPP		Bridge Construction	CN	\$475,746.40	\$118,936.60	\$594,683.00				
Indiana Department of Transportation	39400 / 1593049	Init.	SR 252	Bridge Deck Replacement	6.03 miles E of US 52, over Big Cedar Creek	Seymour	0	STPBG		Bridge ROW	RW	\$20,000.00	\$5,000.00	\$25,000.00				
										Bridge Construction	CN	\$1,372,499.20	\$343,124.80		\$1,715,624.00			
Indiana Department of Transportation	39426 / 1593017	Init.	US 52	HMA Overlay, Preventive Maintenance	SR 1 to I-74	Seymour	8.861	NHPP		Road Construction	CN	\$1,629,050.40	\$407,262.60	\$2,036,313.00				
Indiana Department of Transportation	40055 / 1601977	Init.	SR 1	Bridge Painting	0.31 mile S of US 52, over Whitewater River	Seymour	0	NHPP		Bridge Consulting	PE	\$64,000.00	\$16,000.00	\$80,000.00				
										Bridge Construction	CN	\$717,330.40	\$179,332.60	\$896,663.00				
* Indiana Department of Transportation	40431 / 1700195	Init.	US 52	Slide Correction	0.1 mile S. of SR 252	Seymour	.05	NHPP		Road ROW	RW	\$540,000.00	\$135,000.00	\$675,000.00				
										Road Construction	CN	\$5,094,703.20	\$1,273,675.80			\$6,368,379.00		
Indiana Department of Transportation	40432 / 1701378	Init.	SR 1	Bridge Replacement, Concrete	01.12 mile N of SR 101 at Butlers Run	Seymour	0	NHPP		Bridge ROW	RW	\$32,000.00	\$8,000.00		\$40,000.00			
										Bridge Consulting	PE	\$68,800.00	\$17,200.00			\$86,000.00		

*Please note: Des. No. 1700194 is part of a bundled contract of which Des. No. 1700195 is the lead Des. No. Funds for both projects are programmed above.

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Categorical Exclusion
Appendix I
Environmental Justice
Analysis



Des. No. 1700194: SR 252 Slide Correction Project
EJ Analysis

May 18, 2020

The Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT), Seymour District propose to proceed with a slide correction project in Brookville, Franklin County, Indiana.

Project Location

The project will involve stabilizing the embankment on the south side the roadway to prevent further damage from the land slide along SR 252 in Brookville, Franklin County, Indiana. Specifically, the project is located in Section 28, Township 9 North, Range 2 West as depicted on the Brookville and Whitcomb U.S. Geological Survey 1:24,000 quadrangles. Adjacent land use consists of residential and forested areas.

Purpose and Need

The need for this project is evidenced by the deteriorated condition of SR 252. This condition is a result of the gradual land slide occurring along the south side of SR 252. This has caused the pavement and roadside embankment to deteriorate and fail. The purpose of the project is to correct the embankment failure and improve conditions of SR 252 to allow for continued mobility and accessibility.

Project Description (Preferred Alternative)

The proposed project will construct a soldier pile wall with anchor tiebacks along SR 252 where the embankment is sliding to the south of the roadway. A soldier pile wall is a retaining wall that uses steel piles installed vertically and timber lagging installed horizontally between the piles to stabilize and spread the load behind it. Installation of this new retaining wall will require the removal of the existing stone retaining wall along the south side of the roadway. The roadway within the damaged area will be reconstructed to a typical section consisting of two 11-foot asphalt travel lanes (one in each direction) with a 4-foot wide asphalt shoulder along the eastbound lane and a 2-foot wide asphalt shoulder and a 3-foot wide gutter along the westbound lane. Riprap will be placed along the south side of SR 252 within the construction limits. New guardrail will be installed along the south side of SR 252. The pavement reconstruction will consist of mill/overlay, wedge and level, and widening. The culverts conveying stormwater and roadside drainage under SR 252 will be replaced with culverts that are hydrologically sufficient in size. No new permanent lighting will be installed and the use of temporary lighting during construction is not anticipated. The total length of the project along SR 252 will be approximately 1,072 feet (0.2 mile).

The Maintenance of Traffic (MOT) during construction will close SR 252 and a detour route will be established. The detour will utilize US 52, SR 1, Interstate 74, Ohio SR 128, and Ohio SR 126. The total length will be approximately 56 miles and will likely be in place for 6 months. Property owners will be provided access throughout the duration of the project to reduce impacts as much as possible.

Environmental Justice Analysis

An Environmental Justice (EJ) analysis is required for any project requiring two or more relocations or more than 0.5 acre of new permanent right-of-way. Because the project is expected to require more than 0.5 acre of new permanent right-of-way (approximately 2.1 acres), an EJ analysis was conducted.

Potential EJ impacts are detected by locating minority populations and low-income populations in and near the project area, calculating their percentage in the area relative to a reference population to determine if, in fact, populations of EJ concern do exist, and determining whether there will be disproportionate adverse impacts to them. The reference population may be a county, city, or town and is called the community of comparison (COC). For this project the COC is Brookville Township in Franklin County, Indiana. The community that overlaps the project limits is called the affected community (AC). For this project there is one AC. The AC is Census Tract 9697.

An AC has a population of concern for EJ if the population is more than 50% low-income or minority or if the low-income population or minority population is greater than 125% of the population in the COC.

	COC Brookville Township, Franklin County, Indiana	AC 1 Census Tract 9697
LOW-INCOME POPULATION		
Total Population for Whom Poverty Status is Determined	5,617	2,504
Total Population Below Poverty Level	456	347
Percent Low-Income	8.1%	13.9%
125 Percent of COC	10.1%	
AC Percent Low-Income Greater Than 125 Percent of COC?		Yes
AC Percent Low-Income Greater Than 50 Percent?		No
Population of EJ Concern?		Yes
MINORITY POPULATION		
Total Population	5,698	2,504
Minority Population	59	34
Percent Minority	1.0%	1.4%
125 Percent of COC	1.3%	
AC Percent Minority Greater Than 125 Percent of COC?		Yes
AC Percent Minority Greater Than 50 Percent?		No
Population of EJ Concern?		Yes

A review of American Community Survey five-year estimates data (2014-2018) was completed on May 17, 2020. The data was obtained from the U.S. Census Bureau’s webpage (<https://data.census.gov/cedsci/advanced?tid=GOVSTIMESERIES.CG00ORG01>).

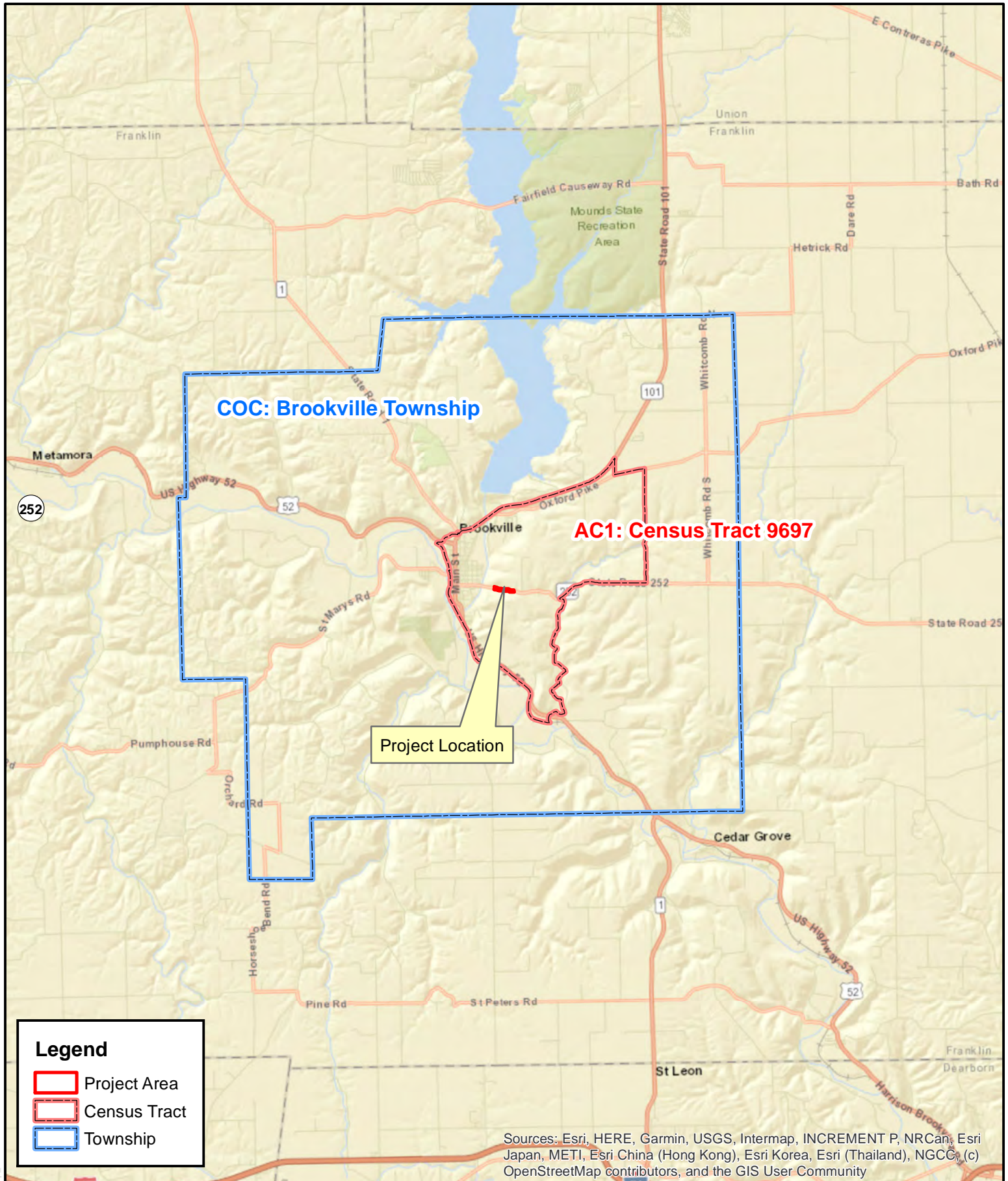
A review of the data revealed that the AC did contain a population greater than 125% of the COC minority population. Therefore, a minority population of EJ concern is present within the project area. The data for

low-income populations determined the AC was 125% of the low-income population of the COC. Therefore, a low-income population of EJ concern is present within the project area.

The proposed project is expected to require the acquisition of approximately 2.10 acres of permanent ROW and 0.02 acre of temporary ROW. Land use within the proposed permanent ROW consists of forested and residential land. No relocations are anticipated.

Overall, the negative impacts to property owners within the project area will be minimal and consist primarily of short-term construction impacts and the loss of a strip of ROW. No relocations are anticipated. The ROW to be acquired will not substantially diminish the existing use of the affected property owners. The MOT during construction will close SR 252 and a detour route will be established. Property owners will be provided access throughout the duration of the project to reduce impacts as much as possible. Thru access along SR 252 will be restored upon completion of the project. No permanent impacts to community cohesion are anticipated.

Impacts from the project to any EJ community in this area will be beneficial due to the improved safety of travel along SR 252 at this location. It is expected the project will not have a disproportionately high and adverse environmental or health impact to low-income or minority populations of EJ concern when compared to non EJ populations.



Legend

- Project Area
- Census Tract
- Township

Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, (c) OpenStreetMap contributors, and the GIS User Community

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EJ Analysis Map
 Des. No. 1700194

0 1.5 3
 Miles

County: Franklin
 Township: Brookville
 State: Indiana

SR 252 Slide Correction Project
 SR 252, 0.8 mile east of US 52
 Created: 7/19/2019, C. Kunkel

Note: This is a modified view of the original table produced by the U.S. Census Bureau.

Note: This download or printed version may have missing information from the original table.

HISPANIC OR LATINO ORIGIN BY RACE

Survey/Program:

American Community Survey

Universe:

Total population

Year:

2018

Estimates:

5-Year

Table ID:

B03002

Source: U.S. Census Bureau, 2018 American Community Survey 1-Year Estimates

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.

While the 2018 American Community Survey (ACS) data generally reflect the July 2015 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas, in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineations due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Explanation of Symbols:

An "*" entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.

An "-" entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution, or the margin of error associated with a median was larger than the median itself.

An "-" following a median estimate means the median falls in the lowest interval of an open-ended distribution.

An "+" following a median estimate means the median falls in the upper interval of an open-ended distribution.

An "****" entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.

An "*****" entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.

An "N" entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.

An "(X)" means that the estimate is not applicable or not available.

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

	Brookville township, Franklin County, Indiana	Census Tract 9697, Franklin County, Indiana
	Estimate	Estimate
∨ Total:	5,698	2,504
∨ Not Hispanic or Latino:	5,679	2,485
White alone	5,639	2,470
Black or African American alone	25	0
American Indian and Alaska Native alone	0	0
Asian alone	0	0
Native Hawaiian and Other Pacific Islander alone	0	0
Some other race alone	0	0
∨ Two or more races:	15	15
Two races including Some other race	0	0
Two races excluding Some other race, and three or more races	15	15
∨ Hispanic or Latino:	19	19
White alone	10	10
Black or African American alone	0	0
American Indian and Alaska Native alone	0	0
Asian alone	0	0
Native Hawaiian and Other Pacific Islander alone	0	0
Some other race alone	9	9
∨ Two or more races:	0	0
Two races including Some other race	0	0
Two races excluding Some other race, and three or more races	0	0

Note: This is a modified view of the original table produced by the U.S. Census Bureau.

Note: This download or printed version may have missing information from the original table.

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE

Survey/Program:

American Community Survey

Universe:

Population for whom poverty status is determined

Year:

2018

Estimates:

5-Year

Table ID:

B17001

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

Source: U.S. Census Bureau, 2014-2018 American Community Survey 5-Year Estimates

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.

While the 2014-2018 American Community Survey (ACS) data generally reflect the February 2013 Office of Management and Budget (OMB) definitions of metropolitan and micropolitan statistical areas; in certain instances the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB definitions due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Explanation of Symbols:

An "***" entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.

An "-" entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution, or the margin of error associated with a median was larger than the median itself.

An "-" following a median estimate means the median falls in the lowest interval of an open-ended distribution.

An "+" following a median estimate means the median falls in the upper interval of an open-ended distribution.

An "****" entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.

An "*****" entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.

An "N" entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.

An "(X)" means that the estimate is not applicable or not available.

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

	Brookville township, Franklin County, Indiana	Census Tract 9697, Franklin County, Indiana
	Estimate	Estimate
∨ Total:	5,617	2,504
∨ Income in the past 12 months below poverty level:	456	347
∨ Male:	168	148
Under 5 years	7	7
5 years	2	0
6 to 11 years	4	4
12 to 14 years	11	11
15 years	0	0
16 and 17 years	16	16
18 to 24 years	14	9
25 to 34 years	2	0
35 to 44 years	11	5
45 to 54 years	4	4
55 to 64 years	43	38
65 to 74 years	18	18
75 years and over	36	36
∨ Female:	288	199
Under 5 years	23	4
5 years	7	7
6 to 11 years	9	4
12 to 14 years	0	0
15 years	13	13
16 and 17 years	4	4
18 to 24 years	43	29
25 to 34 years	26	4
35 to 44 years	37	37
45 to 54 years	32	28
55 to 64 years	48	23
65 to 74 years	11	11
75 years and over	35	35
∨ Income in the past 12 months at or above poverty level:	5,161	2,157
∨ Male:	2,452	968

Under 5 years	128	69
5 years	39	3
6 to 11 years	173	44
12 to 14 years	81	36
15 years	54	8
16 and 17 years	52	20
18 to 24 years	224	102
25 to 34 years	287	116
35 to 44 years	267	109
45 to 54 years	348	93
55 to 64 years	301	179

Chris Kunkel

From: Bales, Ronald <rbales@indot.IN.gov>
Sent: Monday, May 18, 2020 7:45 PM
To: Chris Kunkel
Cc: Chad Costa; Miller, Brandon
Subject: RE: SR 252 Slide Correction - Des. No. 1700194
Attachments: Draft EJ Analysis_SR 252 Slide Correction combined.pdf

INDOT-Environmental Services Division (ESD) has reviewed the project information along with the Environmental Justice (EJ) Analysis for the above referenced project. The project would require strip right-of-way, no relocations, would not disrupt community cohesion or create a physical barrier. With the information provided, INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and/or low incomes populations of EJ concern relative to non EJ populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a. Should changes occur to the project scope and/or right-of-way, coordination with INDOT ESD should occur to determine if a reassessment of the EJ analysis is needed.

Ron Bales

INDOT-Environmental Services Division
Office: (317) 234-4916
Email: rbales@indot.in.gov

From: Chris Kunkel <CKunkel@lochgroup.com>
Sent: Monday, May 18, 2020 5:30 PM
To: Bales, Ronald <rbales@indot.IN.gov>
Cc: Chad Costa <CCosta@lochgroup.com>
Subject: FW: SR 252 Slide Correction - Des. No. 1700194
Importance: High

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Hi Ron,

I'm emailing to request a quick turnaround review of the EJ Analysis for this project in Franklin County. We originally sent this to you in August of last year, as you can see from the email below. We never received a response from you at that time. Since then, however, the project limits have been refined and the ROW amounts have changed. I've updated the write up and used the more recent data from the census website.

Please review this at your earliest convenience and let us know if you have any questions.

Thank you and have a great evening!

Chris Kunkel

Environmental Biologist

Lochmueller Group

317.334.6818 (direct) | 317.677.5132 (mobile)

Categorical Exclusion
Appendix J
Additional Information

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated December 2019)

ProjectNumber	SubProjectCode	County	Property
1800031	1800031	Franklin	Franklin County Park
1800176	1800176	Franklin	Whitewater Canal State Historic Site
1800225	1800225	Franklin	Fairfield Marina, Brookville Lake
1800324	1800324	Franklin	Mounds State Recreation Area
1800331	1800331	Franklin	Batesville Community Park
1800363	1800363B	Franklin	Brookville Lake State Park

Please note, some of the property names are cut off on the ends due to character limits

Also, park names may have changed and is not reflected on the list.

*Various - this may include multiple sites in multiple counties and should always be included in your searches by county.