

**FHWA-Indiana Environmental Documenter**  
**CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM**  
**GENERAL PROJECT INFORMATION**

<b>Road No./County:</b>	Governors Parkway / Lake County
<b>Designation Number(s):</b>	1801907 (bridge Des. No. 1900833)
<b>Project Description/Termini:</b>	New grade separation of new Governors Parkway over Norfolk Southern Railroad corridor beginning from 173 <sup>rd</sup> Street to approximately 200 feet south of 169 <sup>th</sup> Street along Parrish Avenue.

	<b>Categorical Exclusion, Level 2</b> – Required Signatories: INDOT DE and/or INDOT ESD
	<b>Categorical Exclusion, Level 3</b> – Required Signatories: INDOT ESD
<b>X</b>	<b>Categorical Exclusion, Level 4</b> – Required Signatories: INDOT ESD and FHWA
	<b>Environmental Assessment (EA)</b> – Required Signatories: INDOT ESD and FHWA
	<b>Additional Investigation (AI)</b> – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

**Approval**

_____	_____
INDOT DE Signature and Date	INDOT ESD Signature and Date
_____	
FHWA Signature and Date	

**Release for Public Involvement**

_____	_____
INDOT DE Initials and Date	INDOT ESD Initials and Date

ADWP  
February 14, 2023

**Certification of Public Involvement**

\_\_\_\_\_

INDOT Consultant Services Signature and Date

**INDOT DE/ESD Reviewer Signature and Date:** \_\_\_\_\_

**Name and Organization of CE/EA Preparer:** \_\_\_\_\_

Marion Wells; Crawford, Murphy & Tilly, Inc.

# Indiana Department of Transportation

County Lake County

Route Governors Parkway

Des. No. 1801907

Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

## Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

\*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners near the project area on April 4, 2019 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, page G-1. Additional Notice of Entry letters were mailed to potentially affected property owners near the project area on November 24, 2021 notifying them that individuals responsible for geotechnical studies and field activities may be seen in on their property and in the area. A sample copy of the Notice of Entry letter is included in Appendix G, page G-2 to G-3

The City of Hammond held a Mayor's Night Out public meeting in the Hessville neighborhood on December 8, 2021; these public engagement events are scheduled in advance and allow the mayor and staff to hear and respond to questions and concerns from the public (<https://www.gohammond.com/departments/mayors-office/mayors-night-out/>). The December 8, 2021 Mayor's Night Out was not specifically held to discuss the proposed project, but the project became the sole topic of discussion due to the recent survey that was circulated to residents near the project site; approximately 200 people were in attendance. Surveys were sent to residents living within the boundary of 165<sup>th</sup> Street, Kennedy Avenue, Grand Avenue, and Interstate 80/94 and over 600 responses were received. Questions in the survey included:

1. How often are you impacted by a stopped train in Hessville?
2. How strongly do you believe that stopped trains in Hessville are an issue that you would like your city government to address and solve?
3. Are you in favor of the city partnering with the State of Indiana and the railroad companies to build a bridge to help eliminate on-the-street crossings and to help solve trains blocking intersections in Hessville?
4. How strongly are you in favor of the bridge being built?
5. The proposed pathway to the bridge is through the undisturbed, wooded area sometimes referred to as Briar East Woods that will result in hundreds of trees being cut down and habitat disturbed. Knowing this, how strongly are you in favor of the bridge being built?
6. Do you believe that building the bridge is a good investment by the city and that it will positively impact the quality of life in Hessville?

A sample of the survey that was sent to residents and the results of the survey are included in Appendix G (pages G-2 to G-14). Approximately 60% of respondents indicated that they are impacted by stopped trains in the Hessville area at least once a day. Approximately 75% are somewhat or very strongly in favor of the bridge being built and approximately 63% are somewhat or very strongly in favor of the bridge in the proposed location knowing there would be impacts to a wooded area. Approximately 32% are not that much or not at all in favor of the bridge in the proposed location through the wooded area.

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of No Adverse Effect was published in *Northwest Indiana Times* on January 24, 2022 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on February 24, 2022. The text of the public notice and the affidavit of publication appear in Appendix D, pages D-57 to D-59. No comments were received.

The project will go beyond the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public*

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*Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comments and/or request a public hearing. A public hearing will be held and a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

## Public Controversy on Environmental Grounds

*Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.*

As documented in the public survey conducted by the City of Hammond, some residents in the Hessville neighborhood of Hammond have expressed disapproval of the proposed project since the alignment of the new roadway and bridge over the Norfolk Southern Railroad would require impacts to a wooded area in the community and several acres of tree removal. The proposed alignment avoids bisecting the existing wooded area as much that is feasible to meet critical design standards such as the vertical clearance over the railroad and crossing the tracks as close to a right angle as practical. Please refer to the *Other Alternatives Considered* section of this document for further details about the project alternatives, including alternative sites that were considered.

## Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation INDOT District: La Porte

Local Name of the Facility: Governors Parkway

Funding Source (mark all that apply): Federal  State  Local  Other\*

\*If other is selected, please identify the funding source: \_\_\_\_\_

### **PURPOSE AND NEED:**

*The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.*

**Need:** The Indiana Department of Transportation (INDOT) has identified the need to address delays from and exposure to frequent train crossings that vehicles and pedestrians experience at the at-grade road crossings of the Norfolk Southern Railroad tracks in the Hessville neighborhood of Hammond, Indiana.

The need is supported by the Federal Railroad Administration (FRA) - US DOT Crossing Inventory Forms (478690B) which over the past five years of inventories has reported that 10 to 40 scheduled trains utilize the Norfolk Southern Railroad tracks per day at the Parrish Avenue crossing (Appendix K, pages K-2 to K-13). According to the City of Hammond's Local TRAX application, in addition to trains actively crossing Parrish Avenue and the other at-grade crossings in Hessville, including Kennedy Avenue, 169<sup>th</sup> Street, Arizona Avenue, 173<sup>rd</sup> Street and Grand Avenue, standing trains routinely block these crossings for 20 minutes or longer, sometimes hours. These frequent train crossings can cause delays for pedestrians, including those going to and from the nearby schools, and vehicles, including fire department and emergency medical services (EMS) vehicles. The delays to emergency vehicles and pedestrian exposure at the railroad crossing present an additional safety issue. Witnesses have seen parents tell their children to climb through the standing trains in order for their child to make it to school on time.

Hammond Fire Station 6's service area is located south of the Norfolk Southern Railroad tracks and Hammond Fire Station 8's service area is located north of the Norfolk Southern Railroad tracks. Station 6 only has one fire engine and Station 8 has one fire engine, one fire truck, and one EMS unit. Due to multiple apparatuses stationed at Station 8, the EMS unit at Station 8 frequently services the service area 6 south of the Norfolk Southern Railroad tracks. Of the service area 6 calls in 2020, over half of them (54%) were responded to by Station 8 (Appendix K, pages K-23 to K-49). If a responder from Station 8 needs to access service area 6 and completely avoid the railroad crossings due to train blockages, travel can take upwards of 15 to 20 minutes to travel from Station 8 to the furthest house in service area 6. This delay of emergency response can lead to increased property damage and potentially loss of life.

According to the Engineering Assessment (HDR, 2020), eight crashes have been reported at the Parrish Avenue crossing since the FRA began record keeping, with the most recent crash occurring in 1992 at the Parrish Avenue at-grade crossing. Two of the

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incidents involved pedestrians and two vehicular incidents involved fatalities. In addition to the incidents reported at the Parrish Avenue crossing, a recent fatal incident was reported in the project vicinity at the Norfolk Southern and Grand Avenue crossing in July 2018 when a vehicle did not stop at the railroad gates and flashing lights.

**Purpose:** The purpose of the project is to provide reliable transportation access across the Norfolk Southern Railroad tracks for vehicles and pedestrians, improve safety for vehicles and pedestrians at the railroad crossings, and reduce vehicle, pedestrian, and emergency services delays as a result of the existing at-grade rail crossings in the Hessville neighborhood of Hammond, Indiana.

### PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County:   Lake County   Municipality:   Hammond  

Limits of Proposed Work:   A new alignment to be called Governors Parkway from 173<sup>rd</sup> Street to approximately 200 feet south of 169<sup>th</sup> Street along Parrish Avenue.  

Total Work Length:   0.63   Mile Total Work Area:   13.64   Acres

Is an Interstate Access Document (IAD)<sup>1</sup> required?  
 If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes <sup>1</sup>	No
	X
Date: _____	

<sup>1</sup>If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

*Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.*

INDOT and the Federal Highway Administration (FHWA) intend to proceed with a new grade separated roadway to be called Governors Parkway over the Norfolk Southern Railroad corridor.

**Location:** The project is located in Hammond, Lake County, Indiana (Appendix B, page B-1). The project is located along Parrish Avenue and the wooded, undeveloped area east of Parrish Avenue between 169<sup>th</sup> Street and 173<sup>rd</sup> Street (Appendix B, page B-3). The project is located in Section 10, Township 36 North, Range 9 West of the U.S. Geological Survey (USGS) Highland, Indiana Quadrangle (Appendix B, page B-2).

**Existing Conditions:** Parrish Avenue is classified by INDOT as an Urban Minor Collector. Within the project area, Parrish Avenue travels north-south and the posted speed limit is 25 mph. Two tracks of Norfolk Southern Railroad cross Parrish Avenue approximately 0.3 mile south of 169<sup>th</sup> Street and approximately 0.2 mile north 173<sup>rd</sup> Street. North of the Norfolk Southern Railroad crossing, Parrish Avenue consists of one 12-foot lane and unmarked 4-foot paved shoulders in each direction, with concrete curb and gutter and concrete sidewalk on one or both sides of the roadway. South of the Norfolk Southern Railroad crossing, the paved width widens to a total of 38 feet; the southbound lane widens an additional 10 feet to accommodate on-street parking for the 250 feet immediately north 173<sup>rd</sup> Street.

Within the project area, 173<sup>rd</sup> Street travels east-west and the posted speed limit is 25 mph. The roadway consists of one 18.5-foot lane in each direction with concrete curb and gutter and concrete sidewalk on both sides of the roadway. The roadway width accommodates on-street parking and share-the-road bicycle lanes within the project area.

The Norfolk Southern Railroad crosses Parrish Avenue at approximately 35 degrees and the paved width of the road reduces to 26 feet. The rail crossing consists of signalized crossing gates with stop arms. The FRA - US DOT Crossing Inventory Form (478690B) indicates that 23 scheduled trains utilize the crossing per day. The maximum timetable speed is 60 mph with the typical speed over the crossing ranging from 5 to 50 mph.

Land use in the vicinity of the project is urban and the project area is surrounded by commercial and residential properties and undeveloped forested areas. No water resources were identified within the project area.

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**Preferred Alternative:** The project will provide a grade-separated overpass (Lake County Bridge #45-00401) on a new roadway alignment to be called Governors Parkway; the new alignment will be built in undeveloped, forested land east of the existing Parrish Avenue between 169<sup>th</sup> Street and 173<sup>rd</sup> Street. Governors Parkway will have one 11-foot travel lane in each direction with a 10-foot shared-use path along the east side of the roadway. Starting at the south end of the project at 173<sup>rd</sup> Street, the alignment curves to the northeast in order to cross the railroad close to a right angle, which will limit the overall bridge length. The proposed bridge will be 123 feet long, have a minimum vertical clearance (height) of 23 feet, 2 inches, and a 10-degree skew. Once north of the railroad, the alignment curves to the north to run parallel and adjacent to the residential properties along the east side of Kansas Avenue. Once north of the homes along Kansas Avenue and 170<sup>th</sup> Place, the alignment curves to the northwest towards Parrish Avenue at the minimum acceptable radius for 30 mph. Embankment slopes will be constructed on both sides of the new roadway on the north and south side of the railroad to retain the roadway fill as the roadway approaches the new bridge crossing and provide the acceptable vertical clearance over the railroad.

173<sup>rd</sup> Street will be widened to the north to add a westbound to northbound Governors Parkway right-turn lane. The intersection of the new Governors Parkway and 173<sup>rd</sup> Street will be stop-controlled on Governors Parkway. There will not be any changes to the traffic control at the 173<sup>rd</sup> Street and Parrish Avenue intersection.

Where Governors Parkway will meet Parrish Avenue near the north end of the project, certain improvements will be included to facilitate local access to the businesses and residences that would still need to use Parrish Avenue. A new extension of 170<sup>th</sup> Street from Parrish Avenue eastward to the new Governors Parkway is planned and will have one 11-foot travel lane in each direction. This new intersection will be stop-controlled for 170<sup>th</sup> Street traffic and illuminated with streetlights. For traffic traveling southbound on Parrish Avenue, a right-turn lane on Parrish Avenue will connect the new roadway with the existing Parrish Avenue/169<sup>th</sup> Place intersection. This will provide more direct access to 169<sup>th</sup> Place compared to using the new 170<sup>th</sup> Street.

The new roadway alignment may require up to two residential relocations and the reconstruction of three residential driveway aprons along 173<sup>rd</sup> Street. The project will require approximately 12.2 acres of tree removal. The proposed alignment through the undeveloped area minimizes forest fragmentation by routing the new roadway as far to the west as practicable, utilizes curvature that meets a relatively low 30 mph design speed, and a railroad crossing as close to a right angle as practical (taller bridge structures that are constructed at sharp angles are more expensive and prone to deterioration at the abutment corners). The proposed bridge and roadway design are shown on the plan sheets included in Appendix B, pages B-23 to B-36.

The maintenance of traffic (MOT) for the project will include phased construction with temporary lane shifts and restrictions along 173<sup>rd</sup> Street and a posted detour for Parrish Avenue traffic during the final phase of construction. Refer to the MOT section of this document and plan sheets (Appendix B, pages B-37 to B-43) for details.

The preferred alternative will meet the purpose and need for the project by providing reliable and safe transportation access across the Norfolk Southern Railroad tracks for vehicles, pedestrians, and emergency services. The Hessville neighborhood in the city of Hammond will have a grade-separated crossing over the Norfolk Southern Railroad tracks which will reduce vehicle, pedestrian, and emergency services delays caused by frequent train crossings and stoppings at the existing at-grade rail crossings. This alternative also minimized the number of required residential and commercial relocations; therefore, this is the preferred alternative. Construction is anticipated to begin in Fall 2023 and be completed in Summer 2025.

**Logical Termini/Independent Utility:** The project is located along Parrish Avenue from approximately 200 feet south of 169<sup>th</sup> Street to approximately 50 feet south of 170<sup>th</sup> Street, the undeveloped and residential areas east of Parrish Avenue, south of 169<sup>th</sup> Street, west of Kentucky Avenue, and north of 173<sup>rd</sup> Street, and along 173<sup>rd</sup> Street from approximately 240 feet west to approximately 530 feet east of Parrish Avenue. These logical termini were established based on the limits of the necessary work to accommodate the construction of the new grade-separated railroad crossing. The project is not dependent on any other projects to meet the project purpose and need and therefore exhibits independent utility.

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## OTHER ALTERNATIVES CONSIDERED:

*Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.*

### **No Build Alternative**

The No Build alternative has a negligible cost to maintain the existing at-grade crossing and leaves the existing Parrish Avenue at-grade crossing at the Norfolk Southern Railroad as-is. The No Build alternative would not address the purpose and need to provide reliable transportation access across the Norfolk Southern Railroad tracks for vehicles and pedestrians or remove the lengthy pedestrian and vehicle delays experienced by local drivers and emergency services when trains transit through or stop at the existing at-grade crossing. Safety for vehicles and pedestrians would also not be improved since collisions between trains and pedestrians/vehicles could still occur. Since this alternative would not fulfill the purpose and need, it was eliminated from further consideration.

### **Grade Separation on Existing Alignment Alternative**

This alternative would provide a grade-separated overpass along the existing Parrish Avenue roadway alignment. The grade-separated overpass would be a single-span bridge to carry Parrish Avenue over the Norfolk Southern Railroad. A grade separation on the existing Parrish Avenue alignment would require extensive retaining walls on both sides of Parrish Avenue, extending approximately 600 feet both north and south of the existing railroad crossing, in order to achieve the required clearance over the railroad and to minimize the direct impact to the properties immediately adjacent to the existing roadway alignment. Even with retaining walls, this alternative would still result in greater impacts to residents and ROW acquisition. The new retaining wall fill would impact access to existing residential roads and private driveways. Residential roads 172<sup>nd</sup> Street, 171<sup>st</sup> Street (north and south of the Norfolk Southern Railroad) would need to be closed off from Parrish Avenue. 170<sup>th</sup> Street would require approximately 200 feet of reconstruction to meet the elevation of the reconstructed Parrish Avenue in order to maintain access to 170<sup>th</sup> Street and the residential neighborhood on the west side of Parrish Avenue. On the east side of Parrish Avenue, Idaho Avenue would need to be extended north to connect to Parrish Avenue at the 169<sup>th</sup> Place intersection, in order to replace the sole neighborhood access at 171<sup>st</sup> Street, which would be closed. Additionally, at least 13 residences, with driveways connecting directly to Parrish Avenue where the proposed walls would be constructed, would need to be acquired and relocated. This alternative would meet the purpose and need by providing reliable and safe transportation access across the Norfolk Southern Railroad tracks for vehicles, pedestrians, and emergency services; however, due to the required multiple residential displacements and other visual and physical impacts to residences along Parrish Avenue, this alternative was eliminated from further consideration.

### **Grade Separation on New Alignment with MSE Walls Throughout Alternative**

This alternative is the same as the preferred alternative, except instead of constructing embankment slopes to retain the roadway fill as the roadway approaches the new bridge crossing, mechanically stabilized earth (MSE) retaining walls would be constructed instead along the proposed alignment. MSE walls generally have a narrower footprint which would reduce the acreage of tree removal; however, MSE walls for this project will be very expensive, driven not only by the cost of the MSE walls but also due to the depth of soil removal required according to the geotechnical evaluation. The project's geotechnical investigations shows this area to contain a high amount of organic soils that may be unsuitable for wall construction. Adding more wall length would increase concerns for unacceptable wall settlement during construction or require outright removal of the organic soils prior to wall erection. While this alternative would meet the project purpose and need by providing reliable and safe transportation access across the Norfolk Southern Railroad tracks for vehicles, pedestrians, and emergency services, it would add substantial cost to the project and was eliminated from further consideration.

### **Alternate Sites**

A preliminary screening was completed for five alternate crossing sites of the Norfolk Southern Railroad in the Hessville neighborhood of Hammond, Indiana, and are documented in a Preliminary Screening of Alternatives memo (CMT, 2019) (Appendix K, pages K-14 to K-22) as an appendix of the Engineering Assessment (HDR, 2020). The alternate Norfolk Southern Railroad crossing sites were Kennedy Avenue, 169<sup>th</sup> Street, Kennedy Avenue/169<sup>th</sup> Street Roundabouts, 173<sup>rd</sup> Street, and Grand Avenue. All alternatives involve the construction of a new bridge and closures of at-grade crossing for Norfolk Southern Railroad. The estimated number of relocations for each alternative were: 21 relocations for Kennedy Avenue, 12 relocations for 169<sup>th</sup> Street, 26 relocations for Kennedy Avenue/169<sup>th</sup> Street Roundabouts, 6 relocations for 173<sup>rd</sup> Street, and 9 relocations for Grand Avenue. While all of these alternative sites would meet the project purpose and need by providing reliable and safe transportation access across the Norfolk Southern Railroad tracks for vehicles, pedestrians, and emergency services and reduce the amount of required tree clearing, they were eliminated from a further detailed assessment in the Engineering Assessment due to higher construction costs, higher anticipated residential and commercial relocations, change in access for multiple residential and commercial properties, and/or additional visual and physical impacts to remaining properties around the project site.

No other alternatives were considered.

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**The No Build Alternative is not feasible, prudent or practicable because** (Mark all that apply):

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe): It would not provide reliable transportation access and not reduce vehicle, pedestrian, and emergency service delays as a result of the existing at-grade rail crossings.

X
X

## ROADWAY CHARACTER:

*If the proposed action includes multiple roadways, complete and duplicate for each roadway.*

Name of Roadway   Governors Parkway (new roadway alignment)    
 Functional Classification:   Minor Collector    
 Current ADT:   N/A   Design Year ADT:   7,353   VPD (2042)  
 Design Hour Volume (DHV):   535   Truck Percentage (%):   2    
 Designed Speed (mph):   30   Legal Speed (mph):   30  

	Existing		Proposed
Number of Lanes:	N/A		2
Type of Lanes:	N/A		1 northbound (NB), 1 southbound (SB)
Pavement Width:	N/A	ft.	22-32
Shoulder Width:	N/A	ft.	2
Median Width:	N/A	ft.	N/A
Sidewalk Width:	N/A	ft.	10-11.5

Setting:  Urban       Suburban       Rural  
 Topography:  Level       Rolling       Hilly

Name of Roadway   Parrish Avenue    
 Functional Classification:   Minor Collector    
 Current ADT:   3,503   VPD (2019)    Design Year ADT:   N/A    
 Design Hour Volume (DHV):   N/A   Truck Percentage (%):   N/A    
 Designed Speed (mph):   N/A   Legal Speed (mph):   30  

	Existing		Proposed (where the new Governor's Parkway ties into Parrish Avenue)
Number of Lanes:	2		2
Type of Lanes:	1 NB, 1 SB		1 NB, 1 SB
Pavement Width:	34	ft.	34
Shoulder Width:	6-7	ft.	6-7
Median Width:	N/A	ft.	N/A
Sidewalk Width:	0-5	ft.	0-5

Setting:  Urban       Suburban       Rural  
 Topography:  Level       Rolling       Hilly

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Name of Roadway 173<sup>rd</sup> Street  
 Functional Classification: Minor Collector  
 Current ADT: 1,870 VPD (2019) Design Year ADT: 2,100 VPD (2042)  
 Design Hour Volume (DHV): 187 Truck Percentage (%) 10  
 Designed Speed (mph): 25 Legal Speed (mph): 25

	Existing		Proposed
Number of Lanes:	2		3
Type of Lanes:	1 Eastbound (EB), 1 Westbound (WB)		1 EB, 1 WB, 1 Right Turn Lane (RTL)
Pavement Width:	36-37	ft.	36-43 ft.
Shoulder Width:	0	ft.	0 ft.
Median Width:	N/A	ft.	N/A ft.
Sidewalk Width:	5	ft.	5 ft.

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

Name of Roadway 170<sup>th</sup> Street (new extension)  
 Functional Classification: Urban Local Road  
 Current ADT: 200 VPD (2019) Design Year ADT: 220 VPD (2042)  
 Design Hour Volume (DHV): 20 Truck Percentage (%) 10  
 Designed Speed (mph): 25 Legal Speed (mph): 25

	Existing		Proposed
Number of Lanes:	N/A		3
Type of Lanes:	N/A		1 WB, 1 Left Turn Lane (LTL), 1 RTL
Pavement Width:	N/A	ft.	33 ft.
Shoulder Width:	N/A	ft.	2 ft.
Median Width:	N/A	ft.	N/A ft.
Sidewalk Width:	N/A	ft.	N/A ft.

Setting:  Urban  Suburban  Rural  
 Topography:  Level  Rolling  Hilly

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**BRIDGES AND/OR SMALL STRUCTURE(S):**

*If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.*

Structure/NBI Number:   Lake County Bridge 45-00401   Sufficiency Rating:   N/A    
(Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:	N/A		Composite prestressed concrete bulb-tee beam bridge	
Number of Spans:	N/A		1	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	N/A	ft.	27.67	ft.
Outside to Outside Width:	N/A	ft.	42.17	ft.
Shoulder Width:	N/A	ft.	2-4	ft.

*Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.*

A new one span (123-foot) composite prestressed concrete bulb-tee beam bridge (Lake County Bridge #45-00401) will be constructed to carry the new Governors Parkway over the Norfolk Southern Railroad. The proposed bridge will have a minimum vertical clearance (height) of 23 feet, 2 inches, and a 10-degrees skew over the railroad. The bridge will have one 11-foot travel lane in each direction with a 10-foot shared-use path along the east side of the bridge. Leading up to the bridge, there will be guardrails along the west side and a pedestrian fence along the east side, adjacent to the shared-use path, and concrete bridge railings with pedestrian fences will be constructed on the bridge. The bridge will not have weight or height restrictions. The proposed bridge is shown on the plan sheets included in Appendix B, pages B-23 to B-27. No other bridges or small structures are located within the project area.

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### MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe below)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by pedestrians and/or bicyclist and so posted (describe below).	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.*

The MOT for the project will require phased construction along 173<sup>rd</sup> Street and temporary closure of Parrish Avenue with a posted detour. During the first phase of construction, the construction of Governors Parkway and the bridge will occur, which will have minimal traffic disruptions along Parrish Avenue and 173<sup>rd</sup> Street. Phase 2 of construction will require temporary lane closures and traffic shifts along 173<sup>rd</sup> Street for the construction of the new access to Governors Parkway. Phase 2a will maintain one lane of traffic in each direction along 173<sup>rd</sup> Street during construction and Phase 2b will require a posted flagger for alternating eastbound and westbound traffic along 173<sup>rd</sup> Street. During Phase 3, Parrish Avenue between 169<sup>th</sup> Street and 169<sup>th</sup> Place will be closed to through traffic for the construction of the new access to Governors Parkway. Motorists will use 169<sup>th</sup> Street, Grand Avenue, and 173<sup>rd</sup> Street for the detour. The added travel distance as a result of the detour will be approximately 1.0 mile. Residences and businesses along the detoured road will have maintained access to their property. The detour is expected to be in place for approximately 3 months. Pedestrian access will be maintained during construction though phased construction of sidewalks, temporary pavement, and posted detours. Minor pedestrian detours will occur during Phase 2a and 2B around the new 173<sup>rd</sup> Street at Governors Parkway intersection and during Phase 3 around the new Parrish Avenue and 170<sup>th</sup> Street intersection and Parrish Avenue and Governors Parkway junction. Please refer to Appendix B (pages B-37 to B-43) for MOT details.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

### ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 1,363,834 (2021) Right-of-Way: \$ 569,160 (2022) Construction: \$ 9,932,931 (2023)

Anticipated Start Date of Construction: Fall 2023

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### RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	1.59	0.03
Commercial	0	0
Agricultural	0	0
Forest	13.25	0
Wetlands	0	0
<b>TOTAL</b>	<b>14.84</b>	<b>0.03</b>

*Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.*

The existing ROW along Parrish Avenue is consistently 60 feet wide and existing ROW along 173<sup>rd</sup> Steet is 80 feet wide. The proposed permanent ROW along the new Governors Parkway will typically be approximately 185 feet wide, with a maximum of approximately 460 feet wide. The proposed permanent ROW along 173<sup>rd</sup> Street will typically be approximately 97 feet wide, with a maximum of approximately 99 feet wide. The proposed permanent ROW along the 170<sup>th</sup> Street extension will typically be approximately 90 feet wide, with a maximum of approximately 334 feet wide. The reported ROW width is measured as the entire width of the roadway ROW.

The project requires approximately 14.84 acres of permanent ROW. This includes approximately 1.59 acres from five residential properties located along 173<sup>rd</sup> Street, and approximately 13.25 acres from forested properties located immediately south of the Norfolk Southern Railroad, north of the Norfolk Southern Railroad between Kansas Avenue and Kentucky Avenue, and east of Parrish Avenue between 170<sup>th</sup> Place and the multi-family residential property to the north. The project also requires approximately 0.03 acre of temporary ROW from three residential properties located along 173<sup>rd</sup> Street. Permanent ROW from residential properties along 173<sup>rd</sup> Street and the forested properties are needed for the new road alignment. Temporary ROW from residential properties along 173<sup>rd</sup> Street is needed for construction access.

If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

## Part III – Identification and Evaluation of Impacts of the Proposed Action

### SECTION A - EARLY COORDINATION:

*List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.*

Early coordination letters were sent on July 9, 2019 (Appendix C, pages C-1 to C-2).

<u>Agency</u>	<u>Date Sent</u>	<u>Date Response Received</u>	<u>Appendix</u>
INDOT – Office of Public Hearings	7/9/2019	7/9/2019	Appendix C, page C-6
INDOT - Office of Communication	7/9/2019	No response received	N/A
INDOT – Office of Aviation	7/9/2019	7/10/2019	Appendix C, pages C-7 to C-8
INDOT – La Porte Environmental District Office	7/9/2019	No response received	N/A
Housing and Urban Development (HUD) - Chicago Regional Environmental Officer	7/9/2019	No response received	N/A
Indiana Geological Survey (IGS)	7/9/2019	7/9/2019	Appendix C, pages C-9 to C-11

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Indiana Department of Environmental Management (IDEM)	7/9/2019	7/9/2019	Appendix C, pages C-12 to C-18
US Fish and Wildlife Service (USFWS) - Northern Indiana Suboffice	7/9/2019; 2/1/2022	8/1/2019; 3/1/2022; 4/12/2022	Appendix C, pages C-19 to 21; C-49 to C-53
Indiana Department of Natural Resources - Division of Fish and Wildlife (IDNR-DFW)	7/9/2019	8/8/2019	Appendix C, pages C-22 to C-23
IDEM - Groundwater Section, Drinking Water Branch	7/9/2019	7/9/2019	N/A
US Department of Agriculture – Natural Resource Conservation Service (NRCS)	7/9/2019	7/16/2019	Appendix C, page C-24
US Army Corp of Engineers (USACE) – Chicago District	7/9/2019	No response received	N/A
Northwestern Indiana Regional Planning Commission	7/9/2019	No response received	N/A
City of Hammond MS4 Coordinator	7/9/2019	No response received	N/A
City of Hammond Mayor	7/9/2019	No response received	N/A
City of Hammond Council	7/9/2019	No response received	N/A
City of Hammond Engineer	7/9/2019	No response received	N/A
City of Hammond Fire Department	7/9/2019	No response received	N/A
City of Hammond Police Chief	7/9/2019	No response received	N/A
Lake County Commissioners	7/9/2019	No response received	N/A
Lake County Sheriff	7/9/2019	No response received	N/A
Lake County Surveyor	7/9/2019	No response received	N/A
Lake County Emergency Management Agency	7/9/2019	No response received	N/A
School City of Hammond Transportation Director	7/9/2019	No response received	N/A
School City of Hammond Superintendent	7/9/2019	No response received	N/A

All applicable recommendations are included in the Environmental Commitments section of this CE document.



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**SECTION B – ECOLOGICAL RESOURCES:**

**Streams, Rivers, Watercourses & Other Jurisdictional Features**

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

Presence


Impacts

Yes	No

Total stream(s) in project area: 0 Linear feet      Total impacted stream(s): 0 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
N/A				

*Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages E-1 to E-22) there are no streams, rivers, watercourse, or other jurisdictional features within the 0.5-mile search radius. There are no streams, rivers, watercourse, or other jurisdictional features within or adjacent to the project area, which was confirmed by the site visit on October 2, 2019 by Crawford, Murphy & Tilly, Inc. (CMT). Therefore, no impacts are expected.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on December 6, 2019. Please refer to Appendix F, pages F-1 to F-26 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no Waters of the United States, including streams, rivers, watercourse or jurisdictional ditches, were identified within the project area.

**Open Water Feature(s)**

- Reservoirs
- Lakes
- Farm Ponds
- Retention/Detention Basin
- Storm Water Management Facilities
- Other: \_\_\_\_\_

Presence


Impacts

Yes	No

*Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages E-1 to E-22) there are three open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on October 2, 2019 by CMT. Therefore, no impacts are expected.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on December 6, 2019. Please refer to Appendix F, pages F-1 to F-26 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no open water features were identified within the project area.

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<u>Presence</u>	<u>Impacts</u>	
<input type="checkbox"/>	<u>Yes</u>	<u>No</u>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

**Wetlands**

Total wetland area: 0 Acre(s) Total wetland area impacted: 0 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
N/A				

**Wetlands (Mark all that apply)**

- Wetland Determination
- Wetland Delineation
- USACE Isolated Waters Determination

Documentation

X
X

ESD Approval Dates

December 6, 2019
December 6, 2019

**Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in** (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.


*Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages E-1 to E-22) there are three wetlands within the 0.5-mile search radius. There are no wetlands within or adjacent to the project area, which was confirmed by the site visit on October 2, 2019 by CMT. Therefore, no impacts are expected.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by INDOT Ecology and Waterway Permitting Office on December 6, 2019. Please refer to Appendix F, pages F-1 to F-26 for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no wetlands were identified within the project area.

**Terrestrial Habitat**

<u>Presence</u>	<u>Impacts</u>	
<input checked="" type="checkbox"/>	<u>Yes</u>	<u>NO</u>
<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total terrestrial habitat in project area: 13.0 Acres Total tree clearing: 12.2 Acres

*Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.*

Based on a desktop review, a site visit on October 2, 2019 by CMT, and the aerial map of the project area (Appendix B, page B-3), there is deciduous forest and mowed lawn/ROW within the project area. Approximately 12.2 acres of deciduous forest, dominated by eastern cottonwood (*Populus deltoides*), silver maple (*Acer saccharinum*), black cherry (*Prunus serotina*), pin oak (*Quercus palustris*), box elder (*Acer negundo*), and amur honeysuckle (*Lonicera maackii*), and approximately 0.8 acre of mowed grass/lawn, dominated by common turf grasses, including fowl bluegrass (*Poa palustris*) and narrowleaf plantain (*Plantago lanceolata*) will be directly impacted for construction of the new roadway and bridge. A total of 13.0 acres of terrestrial habitat will be directly impacted

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for the project improvements. A total of 12.2 acres of tree removal is expected to be required for this project. Efforts were made to avoid and minimize terrestrial habitat impacts to the greatest extent possible. Avoidance alternatives are not practicable because they would require constructing the overpass at a different location, which, while this would address the project's purpose and need, would require additional residential and commercial relocations and impacts. See the *Other Alternatives Considered* section of this document for further details. Tree mitigation is required for this project and has been determined through consultation with USFWS (Appendix C, pages C-51 to C-53); please refer to the *Protected Species* section of this document for additional details.

IDNR-DFW responded on August 8, 2019 with recommendations to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources, including mitigating for tree removal and revegetating all bare and disturbed areas. The response also recommended placing the roadway and bridge alignment in a location that would require the least possible amount of tree removal and closest to the edge of the forested area as possible to minimize forest fragmentation (Appendix C, pages C-22 to C-23). USFWS responded on August 1, 2019 indicating that the forested area that would be impacted by the proposed project is partially a remnant of the native sand dune and wetland swale ecosystem that existed in this portion of Hammond at the time of the city's founding and is partially regrowth of lands disturbed by development of adjacent properties. USFWS also requested that the loss of trees be mitigated through the planting of replacement trees within Hammond at the recommended 2:1 ratio for non-wetland urban forest with more than 1 acre of impact in addition to any possible mitigation requirement for potential impact to the Indiana bat or northern long-eared bat under the Rangewide Programmatic Informal Consultation process (Appendix C, pages C-19 to C-21). IDEM's general comments in their automated response on July 9, 2019 require permitting if there is more than one acre of total land area disturbance (Appendix C, pages C-12 to C-18). All applicable recommendations are included in the Environmental Commitments section of this CE document.

**Protected Species**

**Federally Listed Bats**

Information for Planning and Consultation (IPaC) determination key completed  
 Section 7 informal consultation completed (IPaC cannot be completed)  
 Section 7 formal consultation Biological Assessment (BA) required

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS:            NE             NLAA             LAA

**Other Species not included in IPaC**

Additional federal species found in project area (based on IPaC species list)  
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Migratory Birds**

Known usage or presence of birds (i.e. nests)  
 State bird species based upon coordination with IDNR

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

*Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.*

Based on a desktop review and the RFI report (Appendix E, pages E-1 to E-22), completed by CMT on July 2, 2019, the IDNR Lake County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated August 8, 2019 (Appendix C, pages C-22 to C-23), the Natural Heritage Program's Database has been checked and no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity. An INDOT 0.5-mile bat review occurred on May 29, 2019 and did not indicate the presence of endangered bat species in or within 0.5 mile of the project area.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages C-25 to C-39). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally endangered northern long-eared bat (NLEB) (*Myotis septentrionalis*).

The official species list generated from IPaC indicated one (1) other species present within the project area. The project is also within range of the candidate species monarch butterfly (*Danaus plexippus*), which is not yet listed or proposed for listing. There are no Section 7 requirements for candidate species. No further coordination is needed with USFWS.

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The official species list generated from IPaC lists migratory birds of concern; this includes American golden-plover (*Pluvialis dominica*), bald eagle (*Haliaeetus leucocephalus*), black-billed cuckoo (*Coccyzus erythrophthalmus*), bobolink (*Dolichonyx oryzivorus*), king rail (*Rallus elegans*), lesser yellowlegs (*Tringa flavipes*), prothonotary warbler (*Protonotaria citrea*), red-headed woodpecker (*Melanerpes erythrocephalus*), rusty blackbird (*Euphagus carolinus*), short-billed dowitcher (*Limnodromus griseus*), and wood thrush (*Hylocichla mustelina*). The wooded area within the project area can serve as habitat for migratory birds. Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Tree clearing time of year restrictions will already be implemented from April 1<sup>st</sup>– October 31<sup>st</sup>, which coincide with the nesting season. Details of the required procedures are outlined in the “Potential Migratory Bird on Structure” Unique Special Provision (USP). This firm commitment is included in the Environmental Commitments of this document.

Although not listed on the IPaC species list, the USFWS early coordination letter response on August 1, 2019 indicated that proposed project is within the range of the Federally endangered piping plover (*Charadrius melodus*), and karner blue butterfly (*Lycaeides melissa samuelis*), and the threatened, rufa red knot (*Calidris canutus rufa*), Pitcher's thistle (*Cirsium pitcheri*), and Mead's milkweed (*Asclepias meadii*). USFWS determined that there is no habitat for these listed species within the proposed project area, and concluded that the proposed project is not likely to adversely affect the piping plover, karner blue butterfly, rufa red knot, Pitcher's thistle, and Mead's milkweed.

Based on the removal of habitat outside of 300 feet from existing road/rail surfaces without a valid Presence/Probable Absence survey indicating that bats are not present, this project does not qualify for the *Rangewide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB)*. A standard coordination letter was prepared and submitted for INDOT review (Appendix C, pages C-40 to C-48). INDOT reviewed the standard coordination letter and submitted to USFWS for review on February 1, 2022. On March 1, 2022, USFWS issued a concurrence letter with the “May Affect, Not Likely to Adversely Affect” finding for the Indiana bat and/or the NLEB (Appendix C, pages C-49 to C-50). The following Avoidance and Minimization Measures (AMMs) are proposed for the project:

- General AMM 1. Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.
- Lighting AMM 1. Direct temporary lighting away from suitable habitat during the active season.
- Lighting AMM 2. When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, the goal is to be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.
- Tree Removal AMM 1. Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.
- Tree Removal AMM 2. Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/ rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (October 31 to April 1).
- Tree Removal AMM 3. Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).
- Tree Removal AMM 4. Do not remove:
  - documented Indiana bat or NLEB roosts that are still suitable for roosting; or
  - trees within 0.25 miles of roosts; or
  - documented foraging habitat any time of year
- Mitigation will be required for the proposed tree removals within the construction limits. Mitigation for the tree removals will follow the Rangewide Indiana Bat In-Lieu Fee Program mitigation ratios and/or ratios requested by USFWS. The City of Hammond will fulfill the mitigation requirements through potential tree planting within the project limits, purchase of in-lieu fee credits, and/or planting at off-site locations. If mitigation will occur off-site, a separate environmental document for the mitigation site will be required.

USFWS had responded to the July 9, 2019 early coordination letter on August 1, 2019 indicating that the forested area that would be impacted by the proposed project is partially a remnant of the native sand dune and wetland swale ecosystem that existed in this portion of Hammond at the time of the city's founding and is partially regrowth of lands disturbed by development of adjacent

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properties. USFWS also requested that the loss of trees be mitigated through the planting of replacement trees within Hammond at the recommended 2:1 ratio for non-wetland urban forest with more than 1 acre of impact in addition to any possible mitigation requirement for potential impact to the Indiana bat or northern long-eared bat under the Rangewide Programmatic Informal Consultation process (Appendix C, pages C-19 to C-21). AMMs and/or commitments are included as firm commitments in the *Environmental Commitments* section of this document.

INDOT and the City of Hammond shall satisfy the compensatory mitigation requirements of the standard coordination with USFWS. In a letter dated April 12, 2022, USFWS has determined that the 12.2 acres of tree removal will require a mitigation in-lieu fee cost ratio of 2:1, resulting in the need for tree mitigation equivalent to the value of 24.4 acres. Although the in-lieu fee acre cost for mitigation in Indiana can be used (\$9,354.00 per acre), the Range-wide Indiana Bat In-Lieu Fee Mitigation Program cannot be used for this project. The impacts will occur within the heavily urbanized City of Hammond and the mitigation for the loss must also occur within Hammond through the planting and protection of new trees within the City, valued at \$228,238.00. The in-lieu fee funds required for this project need to be provided to the Student Conservation Association/CommuniTree Program for use only within the City of Hammond. The USFWS believes that it is appropriate to base the mitigation on the cost/value of the trees to be planted rather than acreage, with the \$228,238.00 to be used to purchase either larger trees or more trees instead of specifically requiring the planting of 24.4 acres. This will allow the CommuniTree Program the flexibility to plant suitable trees within suitable locations to benefit the affected neighborhood and other sites within Hammond (Appendix C, pages C-51 to C-53).

One residential structure and one detached garage will be removed as a result of the project. Prior to any demolition, the structures will be inspected for bats or evidence of bats. If bats, or evidence of bats, are found coordination will occur with INDOT ESD and USFWS before demolition may occur. If further coordination is needed no demolition shall occur until coordination is concluded with INDOT ESD and USFWS. This firm commitment is included in the *Environmental Commitments* of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

**Geological and Mineral Resources**

- Project located within the Indiana Karst Region
- Karst features identified within or adjacent to the project area
- Oil/gas or exploration/abandoned wells identified in the project area

	Yes	No
Project located within the Indiana Karst Region	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Karst features identified within or adjacent to the project area	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Oil/gas or exploration/abandoned wells identified in the project area	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Date Karst Evaluation reviewed by INDOT EWPO (if applicable):   N/A  

*Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)*

Based on a desktop review and the Indiana Karst Region map, the project is located outside the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page B-2) and the RFI report (Appendix E, pages E-1 to E-22), there are no karst features identified within or adjacent to the project area. In the early coordination response on July 9, 2019, the IGWS did not indicate that karst features exist in the project area (Appendix C, pages C-9 to C-11). IGWS noted a high liquefaction potential and the presence of a 1% annual chance flood hazard, as well as a high potential to encounter bedrock resources and a low potential to encounter sand and gravel resources. No active or abandoned mineral resources extraction sites are documented in the area. Response from IGWS has been communicated with the designer on July 9, 2019. No impacts are expected.

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**SECTION C – OTHER RESOURCES**

**Drinking Water Resources**

- Wellhead Protection Area(s)
- Source Water Protection Area(s)
- Water Well(s)
- Urbanized Area Boundary
- Public Water System(s)

Presence

X
X
X

Impacts

Yes	No
	X
X	
X	

- Is the project located in the St. Joseph Sole Source Aquifer (SSA):
- If Yes, is the FHWA/EPA SSA MOU Applicable?
- If Yes, is a Groundwater Assessment Required?

Yes	No

*Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.*

**Sole Source Aquifer**

The project is located in Lake County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

**Wellhead Protection Area and Source Water**

The IDEM's Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on July 9, 2019 and July 29, 2021 by CMT. This project is located within a Source Water Area. The City of Hammond Water Works Department was contacted regarding the Source Water Area on March 14, 2022. City of Hammond Water Works Department responded on April 4, 2022 indicating that only water mains and an above-ground water tank are located near the project area and no intakes or water sources are located within the project vicinity (Appendix C, pages C-54 to C-55). Therefore, no impacts are expected.

**Water Wells**

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on July 29, 2021 by CMT. No wells are located near this project. Therefore, no impacts are expected.

**Urban Area Boundary**

Based on a desktop review of the INDOT MS4 website (<https://entapps.indot.in.gov/MS4/>) by CMT on July 29, 2021 this project is located in an Urban Area Boundary (UAB). An early coordination letter was sent on July 8, 2019 to the City of Hammond MS4 Coordinator. The MS4 coordinator did not respond within the 30-day time frame. Post construction Storm Water Quality Best Management Practices (BMPs) may be required. Avoidance alternatives are not practicable because they would not allow the project to address the existing access, delays, and safety concerns, and therefore would not fulfill the project's purpose and need.

**Public Water System**

Based on a desktop review, a site visit on October 2, 2019 by CMT and the aerial map of the project area (Appendix B, page B-3), this project is located where there is a public water system. The public water system will be affected because of the new intersection of Governors Parkway at 173<sup>rd</sup> Street; relocation of the existing watermain will be required. An early coordination letter was sent on May 14, 2019 to the Hammond Water Department. The Hammond Water Department responded on October 16, 2019 (Appendix B, page B-3) indicating water mains are located along 169<sup>th</sup> Street, Parrish Avenue, and 173<sup>rd</sup> Street. Coordination with Hammond Water Department is ongoing to minimize impacts to the public water system. Although the public water system may be temporarily impacted, all services are expected to be restored by project completion. Avoidance alternatives are not practicable because they would not allow the project to address the existing access, delays, and safety concerns, and therefore would not fulfill the project's purpose and need.

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<b>Floodplains</b>	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Project located within a regulated floodplain	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Longitudinal encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If applicable, indicate the Floodplain Level?

Level 1  Level 2  Level 3  Level 4  Level 5

*Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.*

The IDNR Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on February 24, 2022 by CMT. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain map (Appendix F, page F-28). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

<b>Farmland</b>	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006\*) \_\_\_\_\_  
*\*If 160 or greater, see CE Manual for guidance.*

*Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.*

Based on a desktop review, a site visit on October 2, 2019 by CMT and the aerial map of the project area (Appendix B, page B-3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on July 9, 2019, to NRCS. NRCS responded on July 16, 2019, indicating that the project will not cause a conversion of prime farmland (Appendix C, page C-24).

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**SECTION D – CULTURAL RESOURCES**

**Minor Projects PA** Category(ies) and Type(s)  INDOT Approval Date(s)  N/A

**Full 106 Effect Finding**  
 No Historic Properties Affected     No Adverse Effect     Adverse Effect

**Eligible and/or Listed Resources Present**  
 NRHP Building/Site/District(s)     Archaeology     NRHP Bridge(s)

Documentation Prepared (mark all that apply)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
APE, Eligibility and Effect Determination	<input checked="" type="checkbox"/>				<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
800.11 Documentation	<input checked="" type="checkbox"/>				<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Historic Properties Report or Short Report	<input checked="" type="checkbox"/>				<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Records Check and Assessment					<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>				<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ic Survey Report					<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other:					<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Memorandum of Agreement (MOA)  **MOA Signature Dates** (List all signatories)

*If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.*

**Area of Potential Effect (APE):** The APE for this project generally extends one-quarter mile (1,320 feet) in all directions to account for visual and atmospheric impacts from the project. The APE for archaeology includes all existing and proposed ROW; it is encompassed by the survey area which includes the archaeology APE and any areas investigated beyond it (Appendix D, pages D-12 to D-13).

**Coordination with Consulting Parties:** On August 7, 2019, an early coordination letter was distributed to the organizations listed below inviting them to participate in the Section 106 process (Appendix D, pages D-21 to D-23). Those identified in bold print are participating consulting parties.

Organization	Response Date
<b>Indiana State Historic Preservation Officer (SHPO)</b> (automatic consulting party)	August 27, 2019; Appendix D, pages D-26 to D-27
Eastern Shawnee Tribe of Oklahoma	N/A
Miami Tribe of Oklahoma	N/A
Peoria Tribe of Indians of Oklahoma	N/A
<b>Pokagon Band of Potawatomi Indians</b>	April 22, 2021; Appendix D, page D-40
<b>Forest County Potawatomi Community</b>	September 4, 2019; Appendix D, page D-28
Hammond, Indiana Office of the Mayor	N/A
Hammond City Engineer	N/A



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Hammond Streets Department	N/A
Hammond Planning Commission	N/A
Hammond Historic Preservation Commission	N/A
Hammond Public Library	N/A
Hammond Historical Society	N/A
Lake County Board of Commissioners	N/A
Lake County Historical Society	N/A
Lake County Highway Department	N/A
Decay Devils, Inc.	N/A
Northwestern Indiana Regional Planning Commission	N/A
Indiana Landmarks Northwest Field Office	N/A

The Indiana SHPO responded on August 27, 2019 and recommended that the Norfolk Southern Railroad be invited to join consultation, if that entity was not already “being kept closely informed of developments in this project.” SHPO had no other suggestions for consulting parties and stated its review would continue when additional information was provided (Appendix D, pages D-26 to D-27). Coordination has occurred with Norfolk Southern Railroad as part of the INDOT railroad coordination process.

**Archaeology:** Archaeologists completed an Archaeological Records Check and Phase Ia Reconnaissance Report (Archaeology Report) in March 2021. The archaeologists identified one new site, 12LA0729, which is a precontact isolated find consisting of a chipped stone debitage flake. The site lacked future research potential and did not appear to meet eligibility requirements; therefore, it was not recommended as eligible for listing in the National Register. The archaeologist recommended project clearance (Appendix D, pages D-52 to D-54).

Consulting parties were notified of availability of the Archaeology Report on March 22, 2021 (Appendix D, pages D-35 to D-37).

The Tribal Historic Preservation Officer (THPO) for the Pokagon Band of Potawatomi Indians responded to the archaeology report in a letter dated April 22, 2021 and made the determination “that there will be No Historic Properties” in the APE that are “significant to the Pokagon Band of Potawatomi Indians.” The THPO requested to be contacted and that work stop immediately in the event that “any archaeological resources are uncovered during this undertaking” (Appendix D, page D-40).

The Indiana SHPO responded to the Archaeology Report in a letter dated April 22, 2021. SHPO commented that “we have not identified any currently known archaeological resources listed in or eligible for inclusion in the [National Register] within the proposed project area.” SHPO concurred “with the opinions of the archaeologist, as expressed in the archaeological records check and Phase Ia field reconnaissance survey report (Giedd, 03/2021), that archaeological site 12LA0729 . . . does not appear eligible for inclusion in the [National Register], and that no further archaeological investigations appear necessary at the proposed project area” (Appendix D, pages D-38 to D-39).

**Historic Properties:** Historians completed a Historic Property Report (HPR) on February 26, 2021, that recommended two resources eligible for listing in the National Register: the E.H. Lewis Grand Park Addition Historic District and the Manufacturer’s Addition Historic District. The E.H. Lewis Grand Park Addition is a subdivision of approximately 140 lots on nine sections encompassing about 40 acres bound by Kentucky Avenue, Grand Avenue, 171<sup>st</sup> Street, and 173<sup>rd</sup> Street. The neighborhood was developed by Chicago real estate agent A.A. Lewis and his son, E.H. Lewis, between 1940 and 1941. Homes in the neighborhood include one- and one-and-one-half story, side-gabled American Small Houses, some with gable dormers. A few Ranch and Modern-style homes are present within the neighborhood. The E.H. Lewis Grand Park Addition Historic District is eligible for listing in the National Register under Criteria A and C with significance in the areas of Community Planning and Development and Architecture. The period of significance is circa 1940 to circa 1950 and includes the dates of platting and construction. The Manufacturer’s Addition Historic District is located in the southern part of Hammond in a traditionally working-class area. The district has around 500 parcels and is roughly bound by New Hampshire Avenue, 165<sup>th</sup> Street, Kentucky Avenue, 167<sup>th</sup> Street, Parrish Avenue, Idaho Avenue, 169<sup>th</sup> Street, and Grand Avenue. Originally platted around 1907, significant development occurred after World War II and the majority of homes date to the 1950s and 1960s. Homes in the district have a common scale with repeating patterns of architectural styles and massing, especially Ranch homes with rectilinear and L-shaped footprints. Manufacturer’s Addition Historic District is eligible for listing in the National Register under Criteria A and C with significance in the areas of Community Planning and Development and Architecture. The period of significance is 1954 to circa 1960 and includes the dates of construction of resources

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within the district (Appendix D, pages D-50 to D-51).

Consulting parties were notified of availability of the HPR on March 22, 2021 (Appendix D, pages D-31 to D-34).

The Indiana SHPO responded to the HPR in a letter dated April 22, 2021. SHPO concurred with the APE defined in the HPR and concurred that for “the purposes of the Section 106 review of this federal undertaking, we agree with the conclusions . . . that the E.H. Lewis Grand Park Addition and the Manufacturer’s Addition are both mid-century residential subdivisions that are eligible for inclusion in the [National Register]” (Appendix D, pages D-38 to D-39).

**Documentation Findings:** The project will avoid impacts to historic properties since project activities will occur outside the boundaries of historic resources. INDOT, acting on FHWA’s behalf, has determined a “No Adverse Effect” finding is appropriate for this undertaking. INDOT, acting on behalf of FHWA, requested the Indiana SHPO provide written concurrence with the Section 106 determination of “No Adverse Effect” for the E. H. Lewis Grand Park Addition Historic District and the Manufacturer’s Addition Historic District on October 15, 2021 and consulting parties were invited to review the determination and provide comments within 30 days (Appendix D, pages D-41 to D-44).

On October 27, 2021, SHPO concurred with the Section 106 finding of “No Adverse Effect” for this federal undertaking (Appendix D, pages D-45 to D-46).

**Public Involvement:** To meet the public involvement requirements of Section 106, a legal notice of FHWA’s finding of “No Adverse Effect” was published in *Northwest Indiana Times* on January 24, 2022 offering the public an opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on February 24, 2022. The text of the public notice and the affidavit of publication appear in Appendix D, pages D-57 to D-59. No comments were received.

This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

### SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<b>Presence</b>	<b>Use</b>	
		<b>Yes</b>	<b>No</b>
<b>Parks and Other Recreational Land</b>			
Publicly owned park			
Publicly owned recreation area			
Other (school, state/national forest, bikeway, etc.)			
<b>Wildlife and Waterfowl Refuges</b>			
National Wildlife Refuge			
National Natural Landmark			
State Wildlife Area			
State Nature Preserve			
<b>Historic Properties</b>			
Site eligible and/or listed on the NRHP			
<b><u>Evaluations Prepared</u></b>			
Programmatic Section 4(f)			
“De minimis” Impact			
Individual Section 4(f)			
Any exception included in 23 CFR 774.13			

*Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.*

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Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, pages E-1 to E-22) there are seven (7) potential 4(f) resources located within the 0.5-mile search radius. According to additional research and by the site visit on October 2, 2019 by CMT, there are no Section 4(f) resources within or adjacent to the project area. Therefore, no use is expected.

One playground was identified approximately 120 feet north of the project area. The playground is located along Patricia Lane near the Kennedy Crossing Apartment Complex south of 169<sup>th</sup> Steet and east of Parrish Avenue. County parcel records indicate that the playground is on property belonging to the City of Hammond, which extends into the project area. The City of Hammond confirmed that the playground is not open to the public, and therefore is not considered a Section 4(f) resource (Appendix C, page C-56).

A small community park with a gazebo was identified approximately 250 feet west of the project area. This park is not located within or adjacent to the project area. There will be no impacts to the park; therefore, no use is expected.

As discussed in the Cultural Resources section, historians completed a HPR on February 26, 2021, that recommended two resources eligible for listing in the National Register: the E.H. Lewis Grand Park Addition Historic District and the Manufacturer's Addition Historic District. The project will avoid impacts to historic properties since project activities will occur outside the boundaries of historic resources. INDOT, acting on FHWA's behalf, has determined a "No Adverse Effect" finding is appropriate for this undertaking. INDOT, acting on behalf of FHWA, requested the Indiana SHPO provide written concurrence with the Section 106 determination of "No Adverse Effect" for the E. H. Lewis Grand Park Addition Historic District and the Manufacturer's Addition Historic District on October 15, 2021 and consulting parties were invited to review the determination and provide comments within 30 days (Appendix D, pages D-41 to D-44). On October 27, 2021, SHPO concurred with the Section 106 finding of "No Adverse Effect" for this federal undertaking (Appendix D, pages D-45 to D-46). No ROW will be taken from the E.H. Lewis Grand Park Addition Historic District or the Manufacturer's Addition Historic District; therefore, no 4(f) use is expected.

**Section 6(f) Involvement**

**Presence**

**Use**

**Section 6(f) Property**

**Yes**

**No**




*Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.*

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of 57 properties in Lake County (Appendix K, page K-1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

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**SECTION F – Air Quality**

**STIP/TIP and Conformity Status of the Project**

- Is the project in the most current STIP/TIP?
- Is the project located in an MPO Area?
- Is the project in an air quality non-attainment or maintenance area?
- If Yes, then:
  - Is the project in the most current MPO TIP?
  - Is the project exempt from conformity?
- If No, then:
  - Is the project in the Transportation Plan (TP)?
  - Is a hot spot analysis required (CO/PM)?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Location in STIP: FY 2022-2026  
Northwestern Indiana Regional Planning Commission

Name of MPO (if applicable): Northwestern Indiana Regional Planning Commission

Location in TIP (if applicable): FY 2022-2026, page 79

Level of MSAT Analysis required?

Level 1a  Level 1b  Level 2  Level 3  Level 4  Level 5

*Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.*

**STIP/TIP**

This project is included in the Fiscal Year (FY) 2022-2026 Northwestern Indiana Regional Planning Commission Transportation Improvement Program (NIRPC TIP) (Appendix H, page H-1), which has been directly incorporated by reference into the FY 2022-2026 Statewide Transportation Improvement Program (STIP) (Appendix H, pages H-2 to H-5).

**Attainment Status**

This project is located in Lake County, which is currently a nonattainment area for Ozone under the 2015 8-hour standard, 2008 8-hour standard, and 1979 1-hour standard (revoked), and a maintenance area for PM2.5 under the 1997 standard, Ozone under the 1997 8-hour standard (revoked), Carbon monoxide under the 1971 standard, and Sulfur dioxide under the 1971 standard according to the US Environmental Protection Agency Nonattainment Areas for Criteria Pollutants Green Book (<https://www.epa.gov/green-book>). This project has been identified as being exempt from air quality analysis in accordance with 40 CFR Part 93.126 and this project is not a project of air quality concern (40 CFR Part 93.123). Therefore, the project will have no significant impact on air quality.

**MSAT**

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a Mobile Source Air Toxics analysis is not required.

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### SECTION G - NOISE

**Noise**

**Yes      No**

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?       

Date Noise Analysis was approved/technically sufficient by INDOT ESD:   September 7, 2021  

*Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.*

The project is a Type 1 project which involves the construction of a road on a new alignment. CMT prepared a noise analysis according to the INDOT Traffic Noise Policy and FHWA regulations (Appendix I, pages I-1 to I-14). The noise analysis identified 114 residential receptors (93 single-family and 21 multi-family receptors), a small community park with a gazebo, and a playground on a multi-family residential property, for a total of 116 receptors in ten Common Noise Environments (CNE). The small community park is 250 feet west of the project area and is not adjacent. Existing noise levels ranged from 43.6 to 60.4 dB(A) and projected 2042 Build Scenario noise levels ranged from 45.0 to 60.4 dB(A), with increases no greater than 11.5 dB(A) in any CNE. A receptor was determined to be impacted if the predicted traffic noise level with the project in the design year (2042) was equal to or greater than 66 dB(A) or if levels with the project increase 15 dB(A) or more when compared to existing levels. Therefore, no receptors will be impacted by the project. The noise analysis was determined to be technically sufficient by INDOT ESD on September 7, 2021 (Appendix I, pages I-104).

Based on the studies completed to date, CMT has identified no impacted receptors. As a result, noise abatement was not evaluated. This noise analysis was based on preliminary design criteria. A reevaluation of the noise analysis will occur during final design. If during final design it has been determined that conditions have changed and noise impacts are identified, noise abatement will be evaluated at that time as to whether it is feasible and reasonable.

### SECTION H – COMMUNITY IMPACTS

**Regional, Community & Neighborhood Factors**

**Yes      No**

Will the proposed action comply with the local/regional development patterns for the area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed action result in substantial impacts to community cohesion?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed action result in substantial impacts to local tax base or property values?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will construction activities impact community events (festivals, fairs, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the community have an approved transition plan?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, are steps being made to advance the community's transition plan?	<input type="checkbox"/>	<input type="checkbox"/>
Does the project comply with the transition plan? (explain in the discussion below)	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.*

The surrounding land use is mixed residential and commercial and the current land use of the majority of the project area is undeveloped and forested.

The NIRPC Northwestern Indiana (NWI) 2050 Plan (available at <https://nirpc.org/2050-plan/>) indicates that for motor vehicle transportation, there is a need to address safety and connectivity concerns stemming from frequent railroad crossings since NWI has one of the highest concentrations of railroads in the country. Although the project is not specifically mentioned in the plan, the plan includes an initiative to develop a regional railroad crossing improvement plan that will focus on roadway-rail grade separations and identifies the need to fund projects that address safety issues at railroad crossings. This project conforms to the plan as the purpose of the project is to provide reliable transportation access across the Norfolk Southern Railroad tracks for vehicles, pedestrians, and emergency services and improve safety for vehicles and pedestrians.

The 2012 City of Hammond, Indiana American with Disabilities Act (ADA) Transition and Implementation Plan (available at <https://www.gohammond.com/departments/personnel/ada-compliance/>) identified physical barriers to accessibility that may inhibit

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those with disabilities from accessing and using City of Hammond amenities, and makes recommendations for either program or physical improvements to improve accessibility. Overall, a high percentage of City of Hammond public ROW is very accommodating to all pedestrians. Very few high priority compliance issues were identified during the assessments associated with the report. The most common compliance issue by far was the lack of detectable warnings on curb ramps. The portion of Parrish Avenue within the project area was included in the assessment area but no specific issues were identified. The plan states that as major improvements of roads are undertaken, accessibility should be incorporated into the design and construction process so that accessibility barriers are addressed when construction occurs. The project will comply with the City of Hammond ADA Plan.

The project is expected to positively impact community cohesion and safety by reducing delays and exposure to stopped trains that pedestrians and vehicles, including school buses and fire department and EMS vehicles, experience at the existing Parrish Avenue at-grade crossing. Despite additional travel time (about 30 seconds) and inconvenience for both motorized and pedestrian travel in the proposed design due to at-grade crossing closures, improvements to safety and traffic during the dozens of daily train crossings are anticipated to positively impact the community.

The MOT for the project will include phased construction with temporary lane shifts and restrictions along 173<sup>rd</sup> Street and a posted detour for Parrish Avenue traffic during the final phase of construction. Refer to the MOT section of this document and plan sheets (Appendix B, pages B-37 to B-43) for details. The detour will pose a temporary inconvenience to travelling motorists, including school buses and emergency services; however, no significant delays are anticipated, and all inconveniences will cease upon project completion.

The Festival of the Lakes occurs annually in the summer at Wolf Lake, located approximately 7 miles northwest from the project. No impacts to community events are expected after construction is complete. Based on the city website, other various community events occur in the surrounding area throughout the year. Any impacts to these events will likely be temporary minor traffic delays during the construction period. No impacts to community events are expected after construction is completed.

### Public Facilities and Services

*Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.*

Based on a desktop review, the aerial map of the project area (Appendix B, page B-3), and the RFI report (Appendix E, pages E-1 to E-22) there are 11 religious facilities, one cemetery, six schools, eight recreational facilities, one railroad, and two pipelines within the 0.5-mile search radius; one public-use airport is located within 3.8 miles (20,000 feet) of the project area. There is one railroad within the project area. That number was confirmed by the site visit on October 2, 2019 by CMT. The Norfolk Southern Railroad and pedestrian sidewalks are within the project area and the Gary/Chicago International Airport, is located approximately 2.4 miles northeast of the project area.

A coordination meeting occurred with the Norfolk Southern Railroad on December 15, 2020 (Appendix C, pages C-57 to C-60); coordination will continue with Norfolk Southern Railroad as part of the INDOT railroad coordination process. Special provisions will be implemented into the project plans to ensure safety for the railroad, construction works, and public during construction. Standard coordination will occur with INDOT Utilities and Railroads by the Project Management Team or their consultant no later than the Ready for Contracts (RFC) date.

Existing pedestrian sidewalks are located within and adjacent to the project area along Parrish Avenue and 173<sup>rd</sup> Street. Pedestrian detour signage will be posted on the sidewalks during construction; please refer to the MOT section of this document and Appendix B (pages B-41 and B-43) for pedestrian detour details. Access to all properties will be maintained during construction.

Several utilities are present within the project area (electric, water, telephone, gas, etc.), some of which, including water and gas, are expected to be impacted by the project and may need to be relocated. Utility coordination has been initiated for the project and coordination is ongoing.

INDOT Office of Aviation responded on July 10, 2019 concerning the projects proximity to Gary/Chicago International Airport (Appendix C, pages C-7 to C-8). Since the project will not involve any equipment or obstructions over 100 feet in height, no impact is expected.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

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# Indiana Department of Transportation

County Lake County Route Governors Parkway Des. No. 1801907

**Environmental Justice (EJ)** (Presidential EO 12898)

During the development of the project were EJ issues identified?  
Does the project require an EJ analysis?

<b>Yes</b>	<b>No</b>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

If YES, then:

Are any EJ populations located within the project area?

<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Will the project result in adversely high and disproportionate impacts to EJ populations?

*Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.*

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent ROW. The project will require approximately 14.84 acres of new permanent ROW and may require up to two residential relocations. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is North Township in Lake County, Indiana. The community that overlaps the project area is called the affected community (AC). In this project, the ACs are Census Tract 211 and 220 in Lake County, Indiana. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2015-2019 American Community Survey 5-Year Estimates was obtained from the US Census Bureau website (<https://data.census.gov/cedsci/>) on November 11, 2021 by CMT. The data collected for minority and low-income populations within the ACs are summarized in the below table.

Table: Minority and Low-Income Data (2015-2019 American Community Survey 5-Year Estimates)			
	COC – North Township, Lake County, IN	AC-1 – Census Tract 211, Lake County, IN	AC-2 – Census Tract 220, Lake County, IN
Percent Minority	56.1%	52.6%	57.4%
125% of COC	70.1%	AC < 125% COC	AC < 125% COC
EJ Population of Concern	-	Yes	Yes
Percent Low-Income	19.1%	20.2%	17.2%
125% of COC	23.9%	AC < 125% COC	AC < 125% COC
EJ Population of Concern	-	No	No

AC-1, Census Tract 211 has a percent minority of 52.6% which is above 50%. AC2, Census Tract 220 has a percent minority of 57.4% which is above 50%. Therefore, AC-1 and AC-2 are minority populations of EJ concern.

AC-1, Census Tract 211 has a percent low-income of 20.2% which is below 50% and is below the 125% COC threshold. AC-2, Census Tract 220 has a percent low-income of 17.2% which is below 50% and is below the 125% COC threshold. Therefore, AC-1 and AC-2 are not low-income populations of EJ concern.

**Conclusion**

The project will require 14.84 acres of new permanent ROW and may require up to two residential relocations. The two relocations are located within an EJ area, minority populations of EJ concern. The MOT will include phased construction with temporary lane shifts and restrictions along 173<sup>rd</sup> Street and a detour for Parrish Avenue traffic during the final phase of construction, but all properties will remain accessible during construction. All restrictions will cease upon project completion when the new railroad overpass, Governors Parkway, will be opened and the existing at-grade railroad crossing at Parrish Avenue will be closed. The project is expected to positively impact community cohesion and safety by reducing delays and exposure to stopped trains that pedestrians and vehicles, including school buses and fire department and EMS vehicles, experience at the Parrish Avenue at-grade crossing. The noise analysis for this project also identified no impacted receptors and no EJ concerns were raised during the early coordination phase or public meeting.

## Indiana Department of Transportation

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A public hearing is planned to occur for the project. Efforts will be made to engage the EJ populations during the public hearing and comment period. Discussions with the EJ populations will include, but are not limited to, whether the wooded area is open and used by an EJ population for recreational activities and the impact of tree removal on property owners adjacent to the new road. The property owners of the two residential relocations will be contacted specifically regarding the project and relocations. The EJ section of the environmental document will be revised, based on the comments received from the public hearing. Therefore, pending comments from the public hearing, it will be determined whether the identified population will experience a disproportionately high and adverse impact from the project. EJ Analysis documentation is provided in Appendix J, pages J-1 to J-3. On September 13, 2022, INDOT-ESD requested CE re-submittal (Appendix J, page J-4).

**Relocation of People, Businesses or Farms**

Will the proposed action result in the relocation of people, businesses or farms?  
Is a BIS or CSRS required?

Yes	No
X	
	X

Number of relocations:      Residences:   2        Businesses:   0        Farms:   0        Other:   0  

*Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.*

The project may require up to two residential relocations along 173<sup>rd</sup> Street. The property owners of the two residential relocations will be contacted specifically regarding the project and relocations. As discussed in the Other Alternatives Considered section of this document, the other build alternative would have required at least 13 residential relocations along Parrish Avenue. Efforts were made to minimize the number of residential relocations along 173<sup>rd</sup> Street. The acquisition and relocation program will be conducted in accordance with 49 CFR 24 of the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 as amended. Relocation resources are available to all residential and business relocatees without discrimination. No person displaced by this project will be required to move from a displaced dwelling unless comparable replacement housing is available to that person. The location of the residential relocations are noted on the aerial map in Appendix B, page B-3.

**SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES**

**Hazardous Materials & Regulated Substances** (Mark all that apply)

Documentation

- Red Flag Investigation (RFI)
- Phase I Environmental Site Assessment (Phase I ESA)
- Phase II Environmental Site Assessment (Phase II ESA)
- Design/Specifications for Remediation required?

X

Date RFI concurrence by INDOT SAM (if applicable): August 22, 2022

*Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.*

Based on a review of GIS and available public records, the RFI was completed on July 2, 2019 by CMT and INDOT Site Assessment & Management (SAM) provided their concurrence on September 17, 2019 (Appendix E, Pages E-7 to E-28). An RFI Addendum was completed on August 22, 2022 by CMT and INDOT SAM provided their concurrence on August 22, 2022 that there were no new concerns (Appendix E, pages E-1 to E-6). Two RCRA Generator/TSD sites, one State Cleanup site, six Underground Storage Tank (UST) sites, five Leaking Underground Storage Tank (LUST) sites, two Institutional Control sites, and one National Pollutant Discharge Elimination System (NPDES) facility are located within 0.5 mile of the project area. One LUST site could affect the project area. The United Parcel Service (3147 W. 169<sup>th</sup> Street, Hammond, IN, AI ID 12349) is located adjacent to the project area. IDEM issued a No Further Action (NFA) letter on February 2, 2004; however, there is no closure report or documentation of closure activities on the IDEM Virtual File Cabinet. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary.



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**Part IV – Permits and Commitments**

<b>PERMITS CHECKLIST</b>
--------------------------

**Permits** (mark all that apply)

**Likely Required**

**Army Corps of Engineers (404/Section10 Permit)**

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Other

**IN Department of Environmental Management (401/Rule 5)**

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Isolated Wetlands
- Rule 5
- Other

**IN Department of Natural Resources**

- Construction in a Floodway
- Navigable Waterway Permit
- Other

**Mitigation Required**

**US Coast Guard Section 9 Bridge Permit**

**Others (Please discuss in the discussion below)**

*List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."*

The project lies within the Northwest Indiana-Chicago UAB. Post construction Storm Water Quality BMPs may be required.

A Construction Stormwater General Permit (CSGP), formerly known as the Rule 5 Permit, is expected to be required since greater than one acre of land will be disturbed. The Hammond MS4 is a Certified Construction Program and will have jurisdiction over the CSGP review. It will be the responsibility of the designer to submit plans to the MS4 to process any necessary permits. Once any necessary permits are obtained, they will be submitted to the INDOT Contracts Division prior to the construction of the project. The conditions of the permit will be requirements of the project.

In the early coordination letter response, IDNR-DFW noted that the project is within the Lake Michigan Coastal Program's boundary and may be subject to Federal Consistency review. Since the project is expected to require a CSGP, it is exempt from submitting for Federal Consistency review.

Applicable recommendations provided by resource agencies are included in the Environmental Commitments section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

# Indiana Department of Transportation

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## ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

### Firm:

- 1) If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT La Porte District)
- 2) It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
- 3) General AMM 1. Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
- 4) Lighting AMM 1. Direct temporary lighting away from suitable habitat during the active season. (USFWS)
- 5) Lighting AMM 2. When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, the goal is to be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable. (USFWS)
- 6) Tree Removal AMM 1. Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
- 7) Tree Removal AMM 2. Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/ rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (October 31 to April 1). (USFWS)
- 8) Tree Removal AMM 3. Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
- 9) Tree Removal AMM 4. Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting; or trees within 0.25 miles of roosts; or documented foraging habitat any time of year. (USFWS)
- 10) Prior to any construction or demolition, the structures, single family houses at 3225 173<sup>rd</sup> St and 3233 173<sup>rd</sup> St, will be investigated for bats or evidence of bats. If bats, or evidence of bats, are found coordination will occur with INDOT ESD and USFWS before construction or demolition starts. If further coordination is needed no construction or demolition can occur until coordination is concluded with INDOT ESD and USFWS. (USFWS)
- 11) Mitigation will be required for the proposed tree removals within the construction limits. Mitigation for the tree removals will follow the Range-wide Indiana Bat In-Lieu Fee Program mitigation ratios and/or ratios requested by USFWS. The City of Hammond will fulfill the mitigation requirements through potential tree planting within the project limits, purchase of in-lieu fee credits, and/or planting at off-site locations. (USFWS) If mitigation will occur off-site, a separate environmental document for the mitigation site will be required.
  - The 12.2 acres of tree removal will require a mitigation in-lieu fee cost ratio of 2:1, resulting in the need for tree mitigation equivalent to the value of 24.4 acres. Although the in-lieu fee acre cost for mitigation in Indiana can be used (\$9,354.00 per acre), the Range-wide Indiana Bat In-Lieu Fee Mitigation Program cannot be used for this project. The in-lieu fee funds required for this project need to be provided to the Student Conservation Association/CommuniTree Program for use only within the City of Hammond.
  - The impacts will occur within the heavily urbanized City of Hammond and the mitigation for the loss must also occur within Hammond through the planting and protection of new trees within the City, valued at \$228,238.00. Tree planting will occur both within the project limits and outside the project limits within the City of Hammond.
- 12) Standard coordination will occur with INDOT Utilities and Railroads by the Project Management Team or their consultant no later than the Ready for Contracts (RFC) date. (INDOT SAM)
- 13) The United Parcel Service (3147 W. 169<sup>th</sup> Street, Hammond, IN, AI ID 12349) is located adjacent to the project area. IDEM

This is page 30 of 31 Project name:

Hammond Local TRAX Project Governors Parkway

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issued a NFA letter on February 2, 2004; however, there is no closure report or documentation of closure activities on the IDEM Virtual File Cabinet. If excavation occurs in this area, proper handling, removal, and disposal of soil and/or groundwater may be necessary. (INDOT SAM)

- 14) The wooded area is likely in use (i.e. nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the “Potential Migratory Bird on Structure Unique Special Provision.” (USFWS)

**For Further Consideration:**

- 15) Place the bridge and road alignment in a location that requires the least possible amount of tree removal and closest to the edge of the forested area as possible to minimize forest fragmentation. (IDNR-DFW)
- 16) Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting tree canopy, woody understory, and herbaceous layer). (IDNR-DFW)

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# Hammond Local TRAX Project Governors Parkway CE Level 4

## APPENDIX A: INDOT SUPPORTING DOCUMENTATION



## Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 <sup>1</sup>
<b>Section 106</b>	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement <sup>2</sup>
<b>Stream Impacts<sup>3</sup></b>	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit <sup>4</sup>
<b>Wetland Impacts<sup>3</sup></b>	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
<b>Right-of-way<sup>5</sup></b>	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
<b>Relocations</b>	None	-	-	< 5	≥ 5
<b>Threatened/Endangered Species (Species Specific Programmatic for Indiana bat &amp; northern long eared bat)*</b>	“No Effect”, “Not likely to Adversely Affect” (With select AMMs <sup>6</sup> )	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic <sup>7</sup>
<b>Threatened/Endangered Species (Any other species)*</b>	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
<b>Environmental Justice</b>	No disproportionately high and adverse impacts	-	-	-	Potential <sup>8</sup>
<b>Sole Source Aquifer</b>	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
<b>Floodplain</b>	No Substantial Impacts	-	-	-	Substantial Impacts
<b>Section 4(f) Impacts</b>	None	-	-	-	Any <sup>9</sup>
<b>Section 6(f) Impacts</b>	None	-	-	-	Any
<b>Permanent Traffic Alteration</b>	None	-	-	-	Any
<b>Noise Analysis Required</b>	No	-	-	-	Yes
<b>Air Quality Analysis Required</b>	No	-	-	-	Yes <sup>10</sup>
<b>Approval Level</b>	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA
<ul style="list-style-type: none"> <li>• District Env. (DE)</li> <li>• Env. Serv. Div. (ESD)</li> <li>• FHWA</li> </ul>					

<sup>1</sup> Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

<sup>2</sup> Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

<sup>3</sup> Total permanent impacts to streams (linear feet) and wetlands (acres).

<sup>4</sup> US Army Corps of Engineers Individual 404 Permit

<sup>5</sup> Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

<sup>6</sup> Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

<sup>7</sup> Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

<sup>8</sup> Potential for causing a disproportionately high and adverse impact.

<sup>9</sup> Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

<sup>10</sup> Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

\* Includes the threatened/endangered species critical habitat

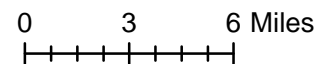
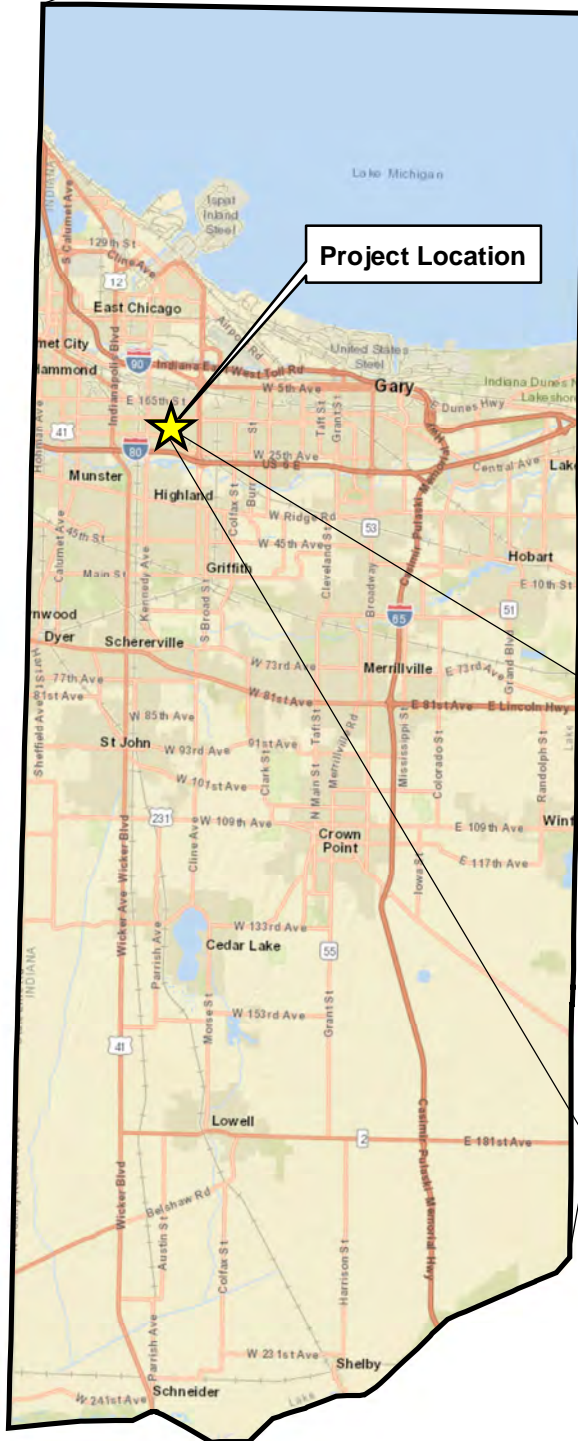
Note: Substantial public or agency controversy may require a higher-level NEPA document.

# Hammond Local TRAX Project Governors Parkway CE Level 4

## APPENDIX B: GRAPHICS AND PLANS







Service Layer Credits: Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, © OpenStreetMap contributors, and the GIS User Community

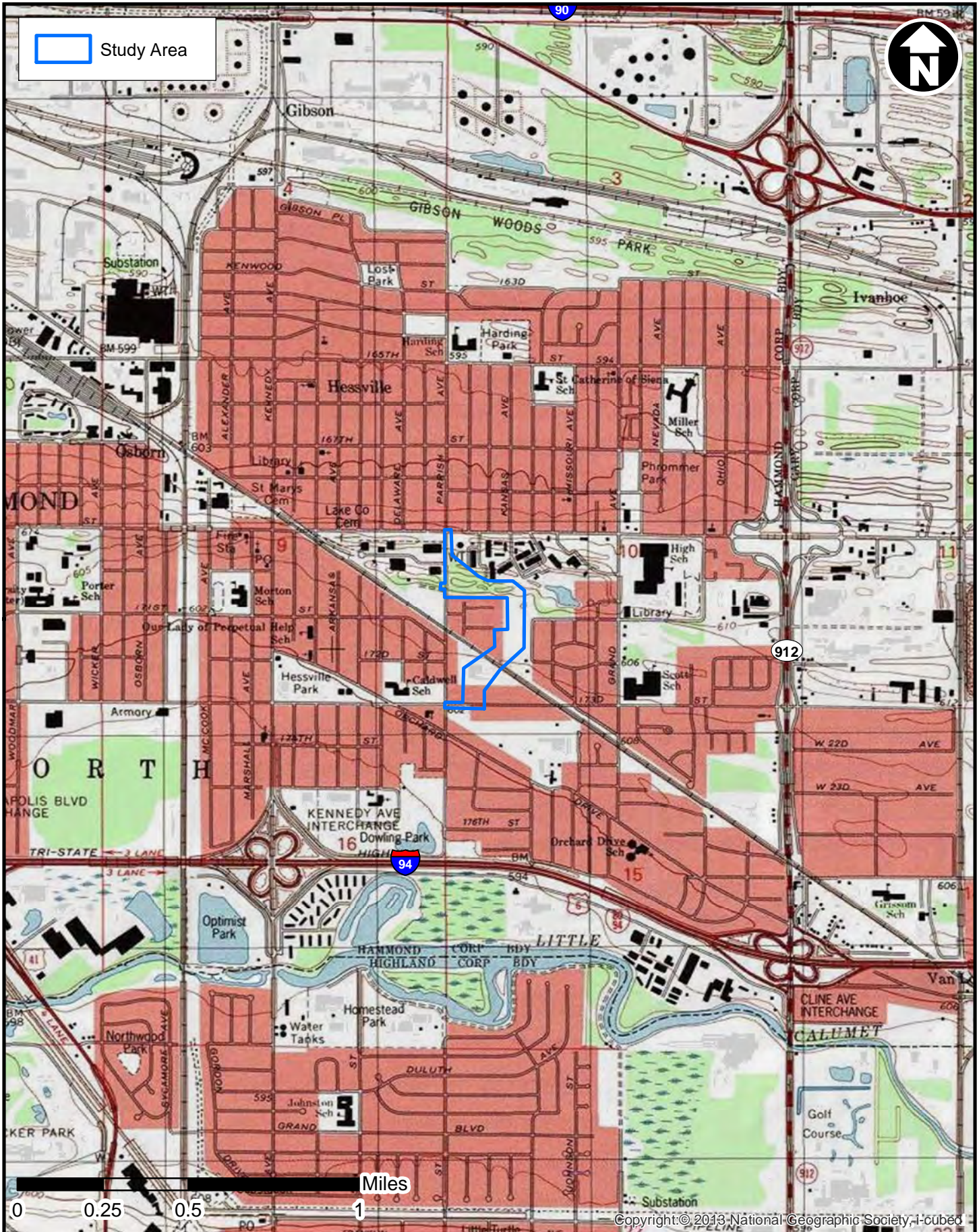
Hammond Local TRAX Project Governors Parkway  
(Des No 1801907) - Hammond, IN

Ellen Hogrebe;  
10/4/2019

**Location Map - Lake County, IN**







Hammond Local TRAX Project Governors Parkway  
 (Des No 1801907) - Hammond, IN

Ellen Hogrebe;  
 10/4/2019

**USGS Topographic Map - Highland, IN Quadrangle** Crawford, Murphy & Tilly





Parrish Ave. Bridge over Norfolk Southern Railroad  
 (Des No 1801907) - Hammond, IN

Claudia McAllister-Peterson;  
 07/01/2022



# Aerial Map





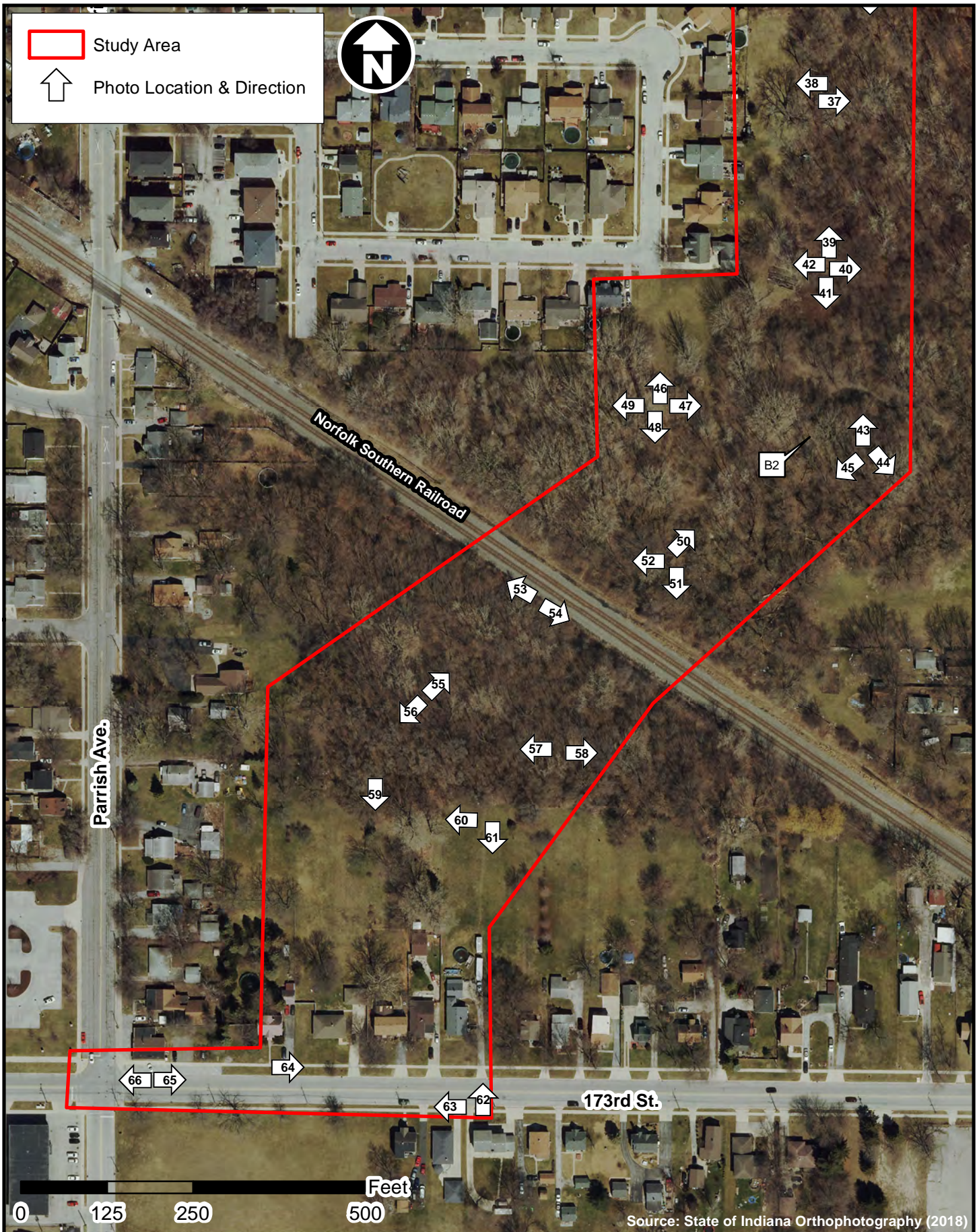
Hammond Local TRAX Project Governors Parkway  
 (Des No 1801907) - Hammond, IN

Ellen Hogrebe;  
 10/8/2019



**Photo Key - North (Page 1 of 2)**  
 B - 4





Hammond Local TRAX Project Governors Parkway  
 (Des No 1801907) - Hammond, IN

Ellen Hogrebe;  
 10/8/2019



**Photo Key - North (Page 1 of 2)**  
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1. View south towards Parrish Avenue and 169<sup>th</sup> Street intersection. 10/2/2019



3. View east from Parrish Avenue. 10/2/2019



2. View north along Parrish Avenue. 10/2/2019



4. View south along Parrish Avenue. 10/2/2019





5. View west from Parrish Avenue.  
10/2/2019



7. View south along Parrish Avenue.  
10/2/2019



6. View north along Parrish Avenue.  
10/2/2019



8. View west along 170<sup>th</sup> Street from Parrish Avenue.  
10/2/2019





9. View north.  
10/2/2019



11. View south.  
10/2/2019



10. View east.  
10/2/2019



12. View west.  
10/2/2019





13. View north.  
10/2/2019



15. View southwest.  
10/2/2019



14. View southeast.  
10/2/2019



16. View north.  
10/2/2019





17. View southeast.  
10/2/2019



19. View northwest.  
10/2/2019



18. View southwest.  
10/2/2019



20. View east.  
10/2/2019





21. View southeast.  
10/2/2019



23. View south.  
10/2/2019



22. View north.  
10/2/2019



24. View north.  
10/2/2019





25. View east.  
10/2/2019



27. View west.  
10/2/2019



26. View south.  
10/2/2019



28. View west.  
10/2/2019





29. View east.  
10/2/2019



31. View southeast.  
10/2/2019



30. View north.  
10/2/2019



32. View southwest.  
10/2/2019





33. View west.  
10/2/2019



35. View east.  
10/2/2019



34. View north.  
10/2/2019



36. View south.  
10/2/2019





37. View east.  
10/2/2019



39. View north.  
10/2/2019



38. View west.  
10/2/2019



40. View east.  
10/2/2019





41. View south.  
10/2/2019



43. View north.  
10/2/2018



42. View west.  
10/2/2019



44. View southeast.  
10/2/2019





45. View southwest.  
10/2/2019



47. View east.  
10/2/2019



46. View north.  
10/2/2019



48. View south.  
10/2/2019





49. View west.  
10/2/2019



51. View south.  
10/2/2019



50. View northeast.  
10/2/2019



52. View west.  
10/2/2019





53. View northwest along Norfolk Southern Railroad.  
10/2/2019



55. View northeast.  
10/2/2019



54. View southeast along Norfolk Southern Railroad.  
10/2/2019



56. View southwest.  
10/2/2019





57. View west.  
10/2/2019



59. View south.  
10/2/2019



58. View east.  
10/2/2019



60. View west.  
10/2/2019





61. View south.  
10/2/2019



63. View west along 173<sup>rd</sup> Street.  
10/2/2019



62. View north from 173<sup>rd</sup> Street.  
10/2/2019



64. View east along 173<sup>rd</sup> Street.  
10/2/2019





65. View east along 173<sup>rd</sup> Street.  
10/2/2019



66. View west towards 173<sup>rd</sup> Street and Parrish Avenue  
intersection. 10/2/2019

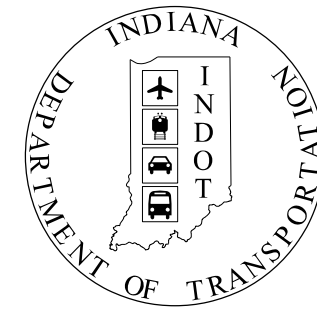
PROJECT	DESIGNATION
1801907	1900833
CONTRACT	BRIDGE FILE
B-41851	45-00401

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
45-00401	Composite Prestressed Concrete Bulb-Tee Beam, BT 60x49	1 Span: 123'-0" Skew: 10° Lt.	Norfolk Southern Railroad	18+40.10 "RD_Prop GovernorsPkwy"

KIN PROJECT INFORMATION	
KIN DES. NUMBER	PROJECT DESCRIPTION
1801907	PROPOSED GOVERNORS PARKWAY, FROM 173RD STREET TO 200 FEET SOUTH OF 169TH STREET (NEW CONNECTING ROAD) (LEAD DES. NO.)

**NOTE TO REVIEWER:**  
Maintenance of Traffic (MOT) plans & details for proposed approach roadway are included in roadway plans under Lead Des. No. 1801907.

# INDIANA DEPARTMENT OF TRANSPORTATION



## BRIDGE PLANS

FOR SPANS OVER 20 FEET

ROUTE: GOVERNORS PARKWAY N/A

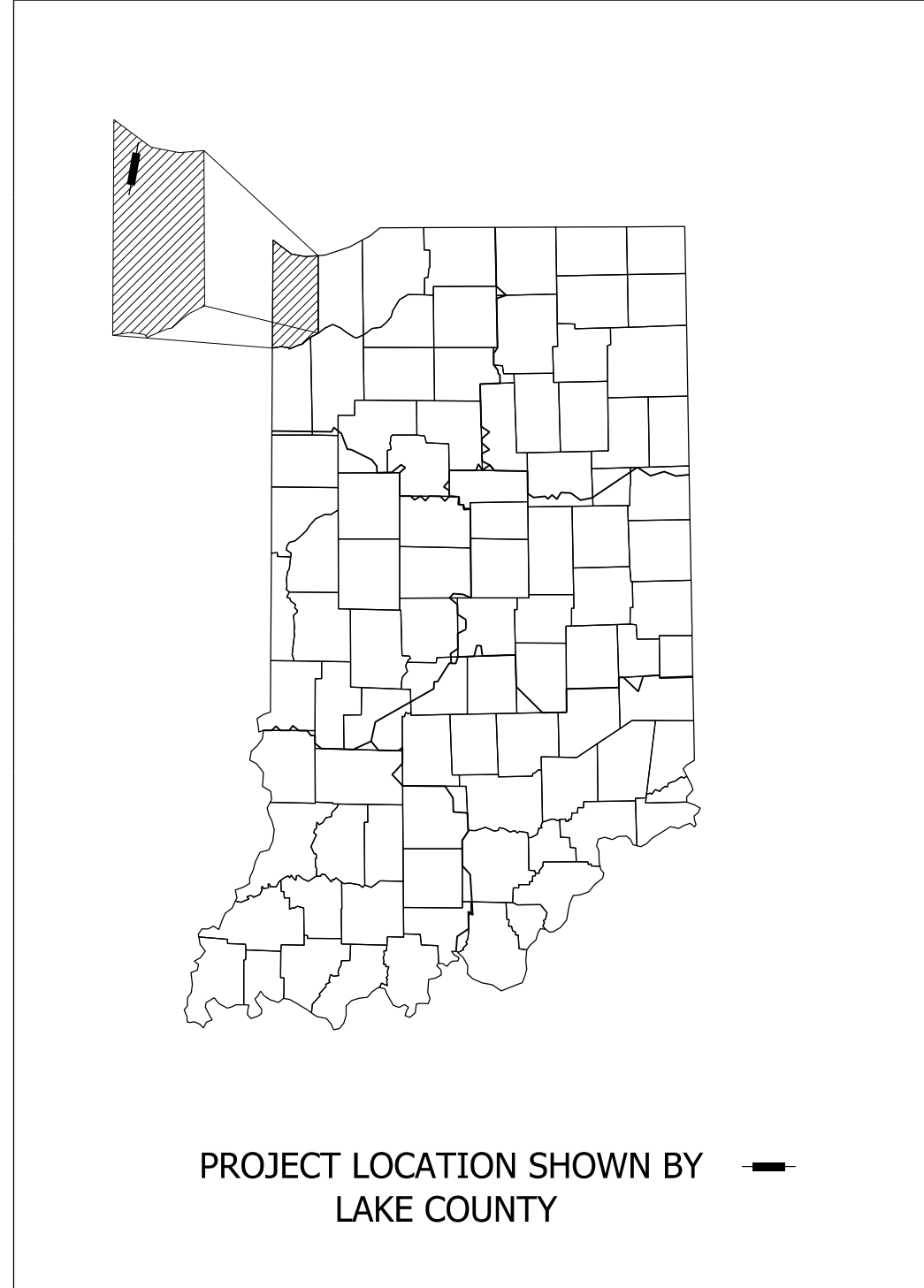
PROJECT NO. 1801907 P.E.  
1801907 R/W  
1900833 CONST.

New Bridge Construction on Proposed Governors Parkway over Norfolk Southern Railroad,  
Located 0.16 Miles North of 173rd Street, Section 10, T-36-N, R-9-W,  
Hammond Township, Lake County, Indiana

TRAFFIC DATA		
A.A.D.T.	(2019)	3,503 V.P.D.
A.A.D.T.	(2042)	7,353 V.P.D.
D.H.V	(2042)	535 V.P.H.
DIRECTIONAL DISTRIBUTION		50 %
TRUCKS		2 % A.A.D.T. 2 % D.H.V.

DESIGN DATA	
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	New (4R)
FUNCTIONAL CLASSIFICATION	Urban Major Collector
RURAL/URBAN	Urban (Intermediate)
TERRAIN	Level
ACCESS CONTROL	None

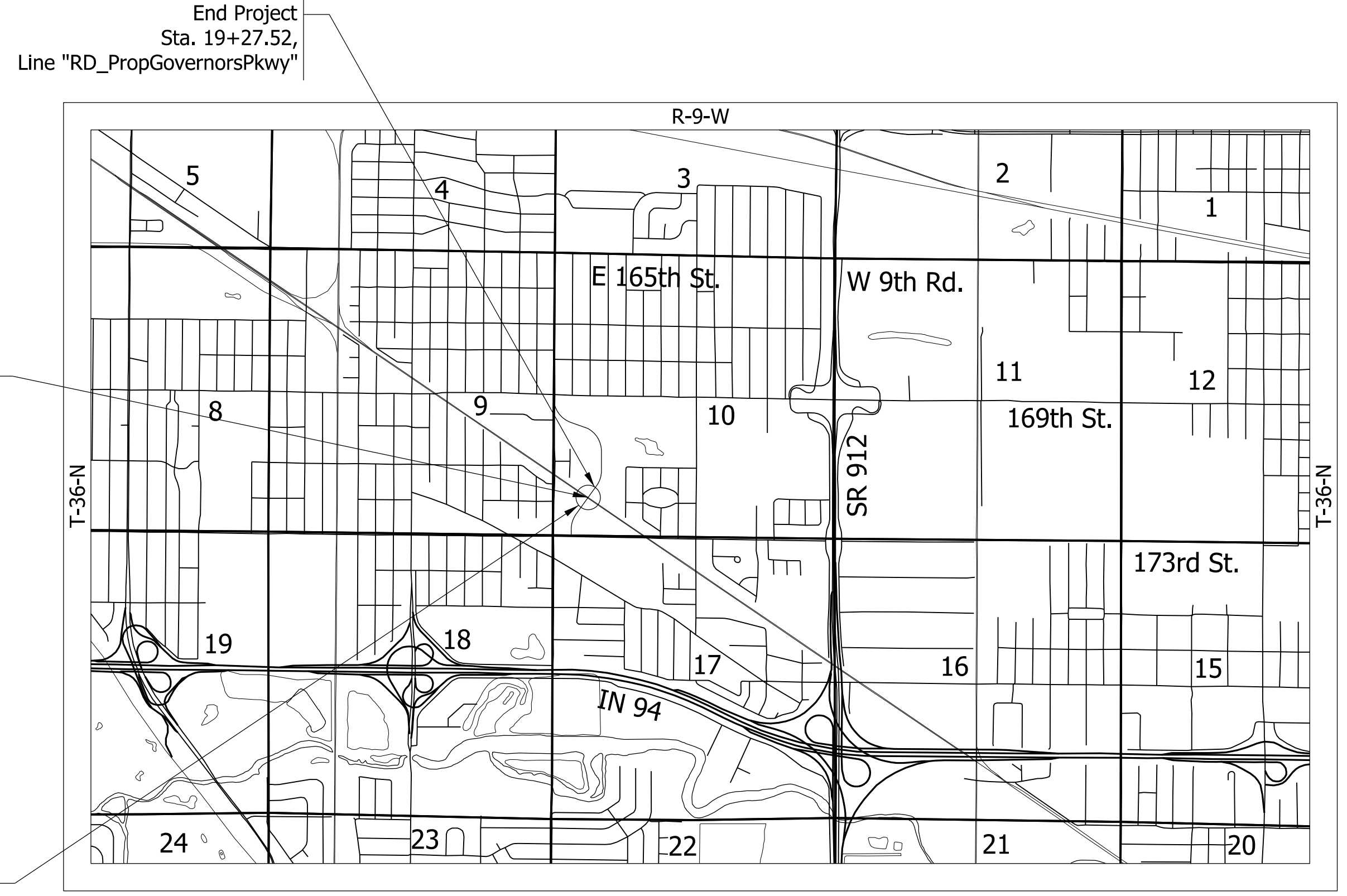


LATITUDE: 41°34'59" LONGITUDE: -87°26'58"

BRIDGE LENGTH:	0.024	MI.
ROADWAY LENGTH:	0.009	MI.
TOTAL LENGTH:	0.033	MI.
MAX. GRADE:	5.0	%

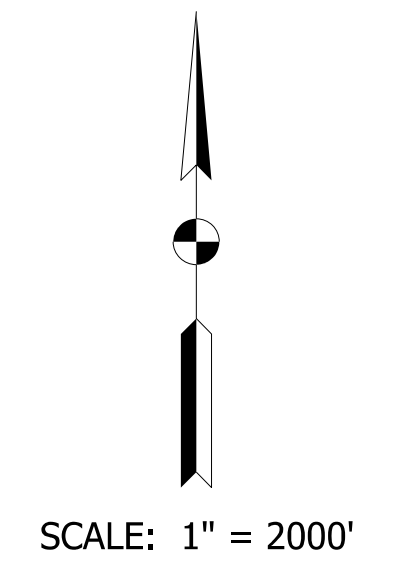
HUC: 071200030305

PFC PLANS  
November 1, 2021



Structure 45-00401  
Over Norfolk Southern Railroad  
Sta. 18+40.10,  
Line "RD\_PropGovernorsPkwy"

Begin Project  
Sta. 17+54.50,  
Line "RD\_PropGovernorsPkwy"



INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2022  
TO BE USED WITH THESE PLANS.

**CMT**  
Engineers and Consultants  
8790 PURDUE ROAD  
INDIANAPOLIS, IN 46268-6128  
PHONE: (317) 298-4500

Copyright CMT, Inc.

NOT FOR  
CONSTRUCTION

PLANS PREPARED BY: CRAWFORD, MURPHY & TILLY INC. (317) 298-4500 PHONE NUMBER

CERTIFIED BY: [Signature] 10/31/2021 DATE

APPROVED FOR LETTING: \_\_\_\_\_ INDIANA DEPARTMENT OF TRANSPORTATION DATE

BRIDGE FILE	
45-00401	
DESIGNATION	
1900833	
SHEETS	
Electronic	1 of 11
CONTRACT PROJECT	
B-41851	1801907

Plot: 10/31/2021 1:25:03 PM

- △△△ 1.5%  
From Sta. 15+86.97 to Sta. 17+04.50
- Varies 1.5% to -1.5%  
From Sta. 17+04.50 to Sta. 17+54.50
- Varies -1.5% to 1.5%  
From Sta. 19+27.52 to Sta. 19+77.52
- 1.5%  
From Sta. 19+77.52 to Sta. 20+91.06
- 10'-0"  
From Sta. 10+00.00 to Sta. 15+06.97
- Varies 10'-0" to 11'-5"  
From Sta. 15+06.97 to Sta. 15+56.97
- 11'-5"  
From Sta. 15+56.97 to Sta. 21+41.06
- Varies 11'-5" to 10'-0"  
From Sta. 21+41.06 to Sta. 21+91.06
- 10'-0"  
From Sta. 21+91.06 to Sta. 41+75.00

- K From Sta. 10+00.00 to Sta. 11+50.00  
From Sta. 25+50.00 to Sta. 43+11.72
- K1 From Sta. 11+50.00 to Sta. 25+50.00

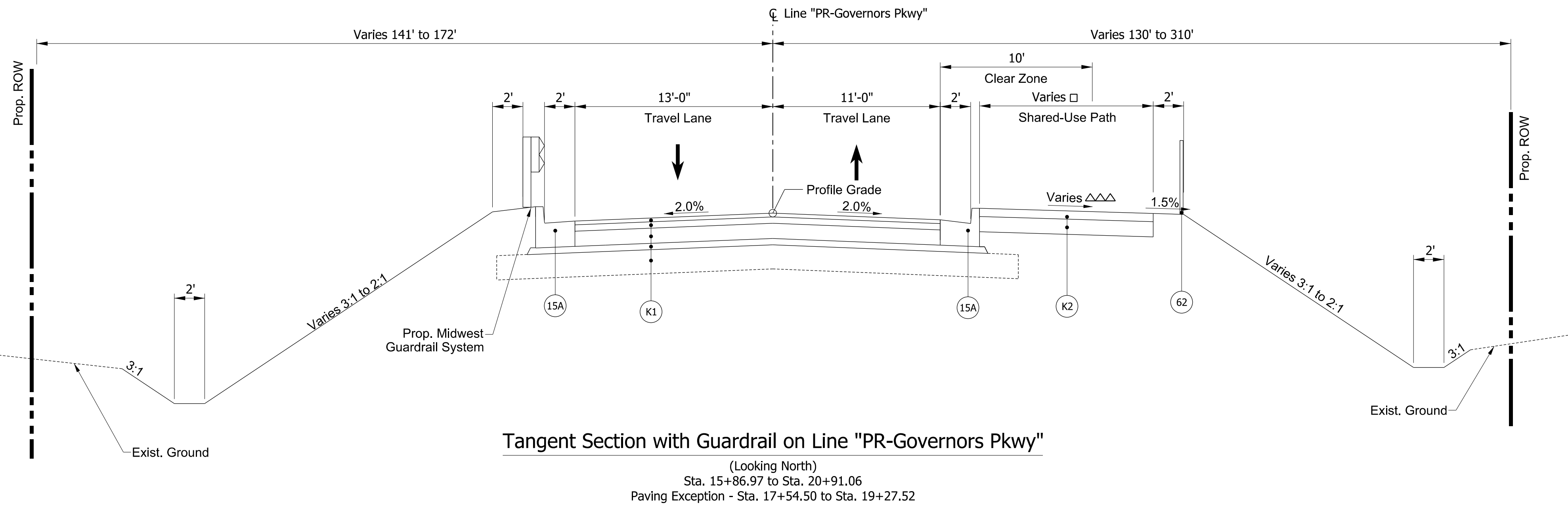
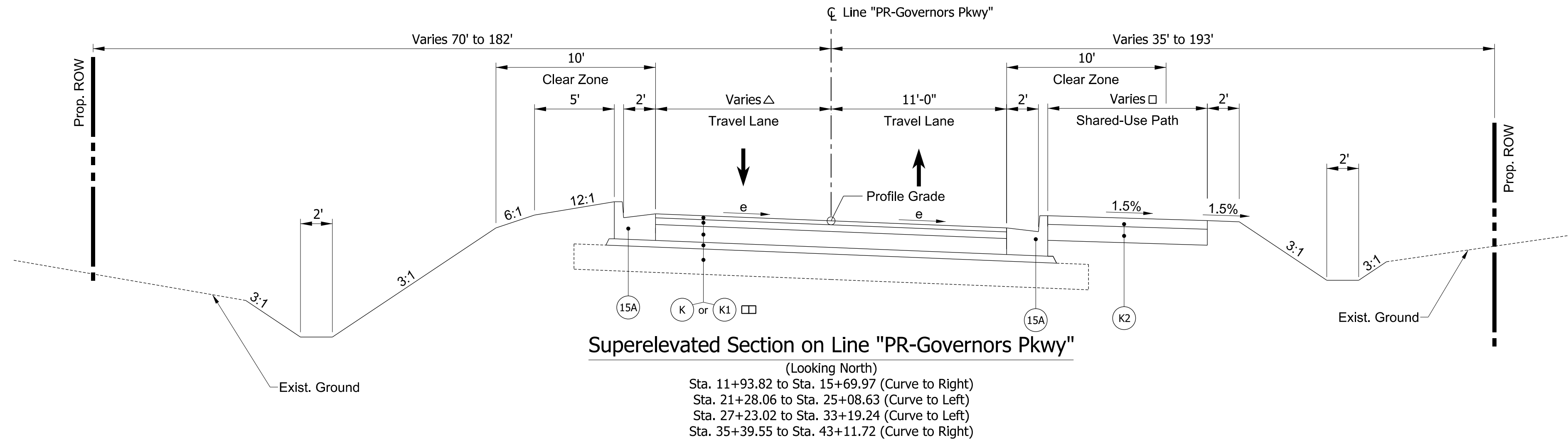
- 11'-0"  
From Sta. 10+00.00 to Sta. 11+88.80
- Varies 11'-0" to 0'-0"  
From Sta. 11+88.80 to Sta. 12+88.80

- △ 11'-0"  
From Sta. 10+00.00 to Sta. 15+06.97  
From Sta. 21+91.06 to Sta. 43+11.72
- Varies 11'-0" to 13'-0"  
From Sta. 15+06.97 to Sta. 15+86.97
- Varies 13'-0" to 11'-0"  
From Sta. 20+91.06 to Sta. 21+91.06

**Proposed Legend**

- ⊖ Concrete Sidewalk
- ⊖ Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type IBC
- ⊖ K1 Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type I
- ⊖ K2 HMA for Sidewalk:  
140 lbs/sy HMA Surface, Type B  
220 lbs/sy HMA Intermediate, Type B  
6" CA53 Stone  
Subgrade Treatment, Type III
- ⊖ 15A Curb and Gutter, Concrete, Modified
- ⊖ 62 Pedestrian Fence

NOTE: HMA PAVEMENT DESIGN TO BE DETERMINED DURING FINAL DESIGN.



**NOTE**

1. For information only, refer to Roadway Plans Des. No. 1801907 for details.

DATE	REVISION

**NOT FOR CONSTRUCTION**

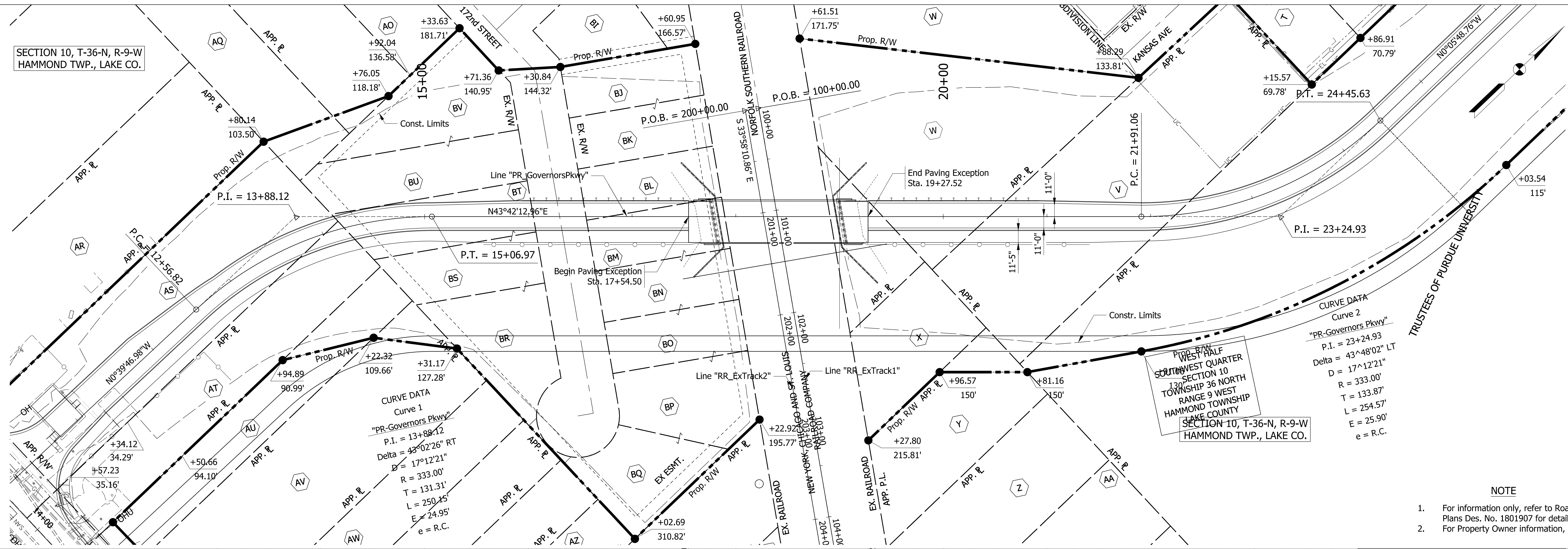
RECOMMENDED FOR APPROVAL	<i>BENG SIGNATURES</i>	10/31/2021
DESIGNED:	BJZ	10/31/2021
CHECKED:	JLP	10/31/2021

INDIANA DEPARTMENT OF TRANSPORTATION
<b>TYPICAL CROSS SECTIONS PROPOSED GOVERNORS PARKWAY</b>

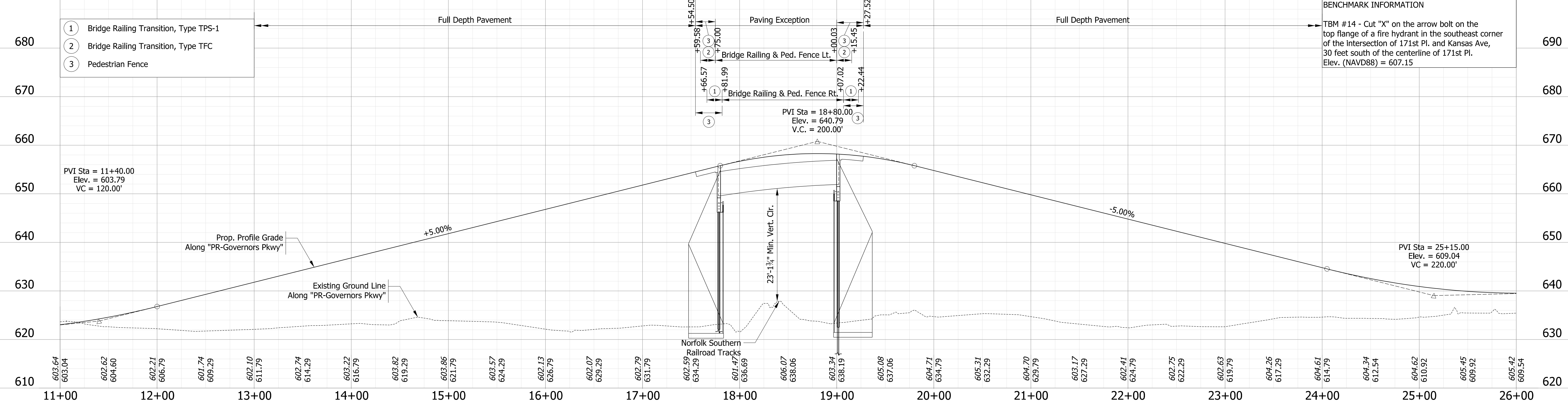
SCALE 1" = 5'	BRIDGE FILE 45-00401
	DESIGNATION 1900833
SURVEY BOOK Electronic	SHEETS 3 of 11
CONTRACT B-41851	PROJECT 1801907



SECTION 10, T-36-N, R-9-W  
HAMMOND TWP., LAKE CO.



- NOTE**
- For information only, refer to Roadway Plans Des. No. 1801907 for details.
  - For Property Owner information, see sheet 7.



**BENCHMARK INFORMATION**

TBM #14 - Cut "X" on the arrow bolt on the top flange of a fire hydrant in the southeast corner of the intersection of 171st Pl. and Kansas Ave, 30 feet south of the centerline of 171st Pl. Elev. (NAVD88) = 607.15

- Bridge Railing Transition, Type TPS-1
- Bridge Railing Transition, Type TFC
- Pedestrian Fence

Plot: 7/28/2022 9:50:57 AM

DATE	REVISION

Curve No.	Point	Station	Northing	Easting
Curve 1	P.C.	12+56.82	439376.27	773556.42
	P.I.	13+88.12	439507.57	773557.94
	P.T.	15+06.97	439602.49	773648.67
Curve 2	P.C.	21+91.06	440097.05	774121.33
	P.I.	23+24.93	440193.82	774213.82
	P.T.	24+45.63	440327.69	774213.60

**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL

*BENG SIGNATURES*  
DESIGN ENGINEER

7/28/2022 DATE

DESIGNED: EDLN 7/28/2022 DRAWN: EDLN 7/28/2022

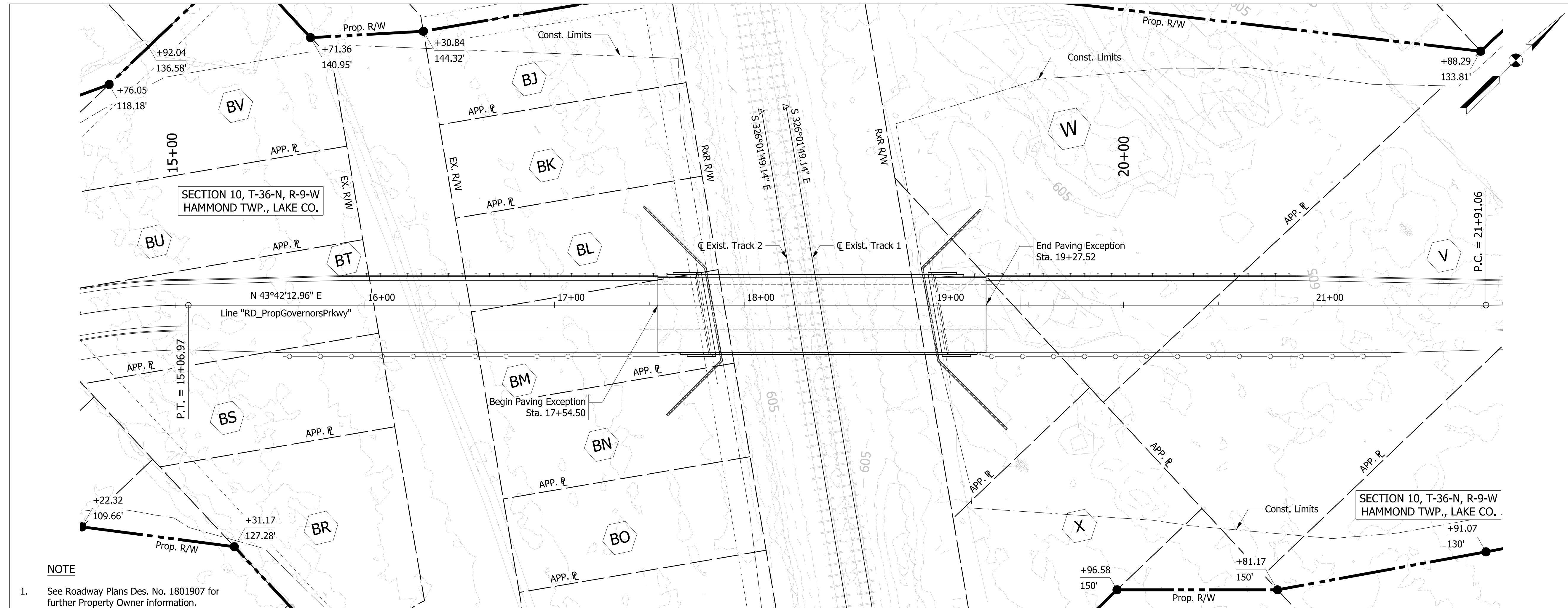
CHECKED: RCM 7/28/2022 CHECKED: RCM 7/28/2022

INDIANA  
DEPARTMENT OF TRANSPORTATION

**PLAN AND PROFILE**

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE 45-00401
VERTICAL SCALE 1" = 10'	DESIGNATION 1900833
SURVEY BOOK Electronic	SHEETS 4 of 17
CONTRACT B-41851	PROJECT 1801907

This Sheet for Information Only



**EXISTING STRUCTURE**

No Existing Structure

**HYDRAULIC DATA**

N/A

**HYDRAULIC SCOUR DATA**

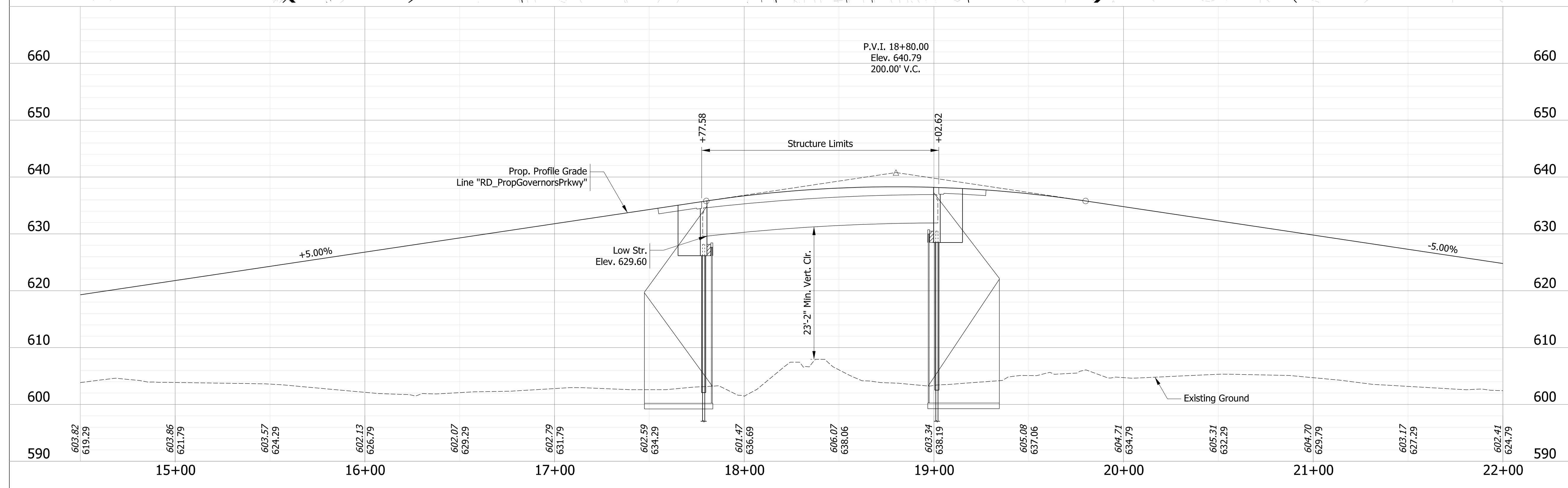
N/A

**EARTHWORK TABULATION**

Fill + 20%	709 cys
Excavation, Common	209 cys
B Borrow	3,818 cys
Structure Backfill, Type 3	5,501 cys

No direct payment for benching. Benching will not be paid for as common excavation.

See Road Plans Des. No. 1801907 for all other earthwork Quantities & Summary.



**COMPOSITE PRESTRESSED  
CONCRETE BULB-TEE BEAM BRIDGE**  
1 SPAN: 123'-0"; SKEW 10° LT.  
27'-8" CLEAR ROADWAY WIDTH  
GOVERNORS PARKWAY OVER NS RR  
LAKE COUNTY

DATE	REVISION

**NOT FOR  
CONSTRUCTION**

RECOMMENDED FOR APPROVAL	<i>ENG SIGNATURES</i>	DESIGN ENGINEER	10/31/2021	DATE
DESIGNED: DWH	10/31/2021	DRAWN: DWH	10/31/2021	DATE
CHECKED: RCM	10/31/2021	CHECKED: RCM	10/31/2021	DATE

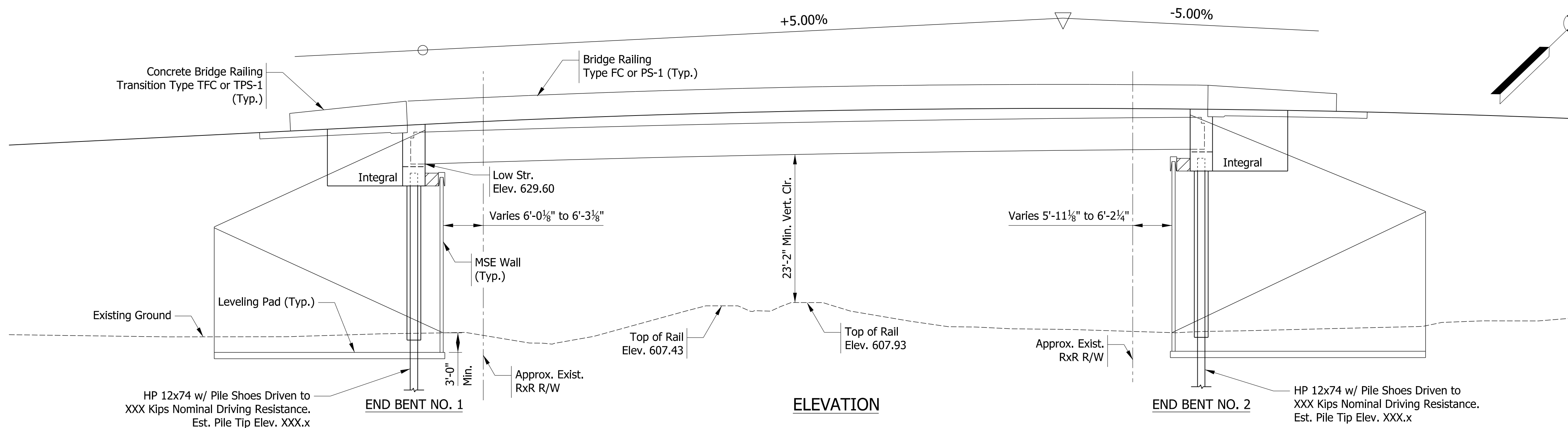
INDIANA  
DEPARTMENT OF TRANSPORTATION

**LAYOUT**

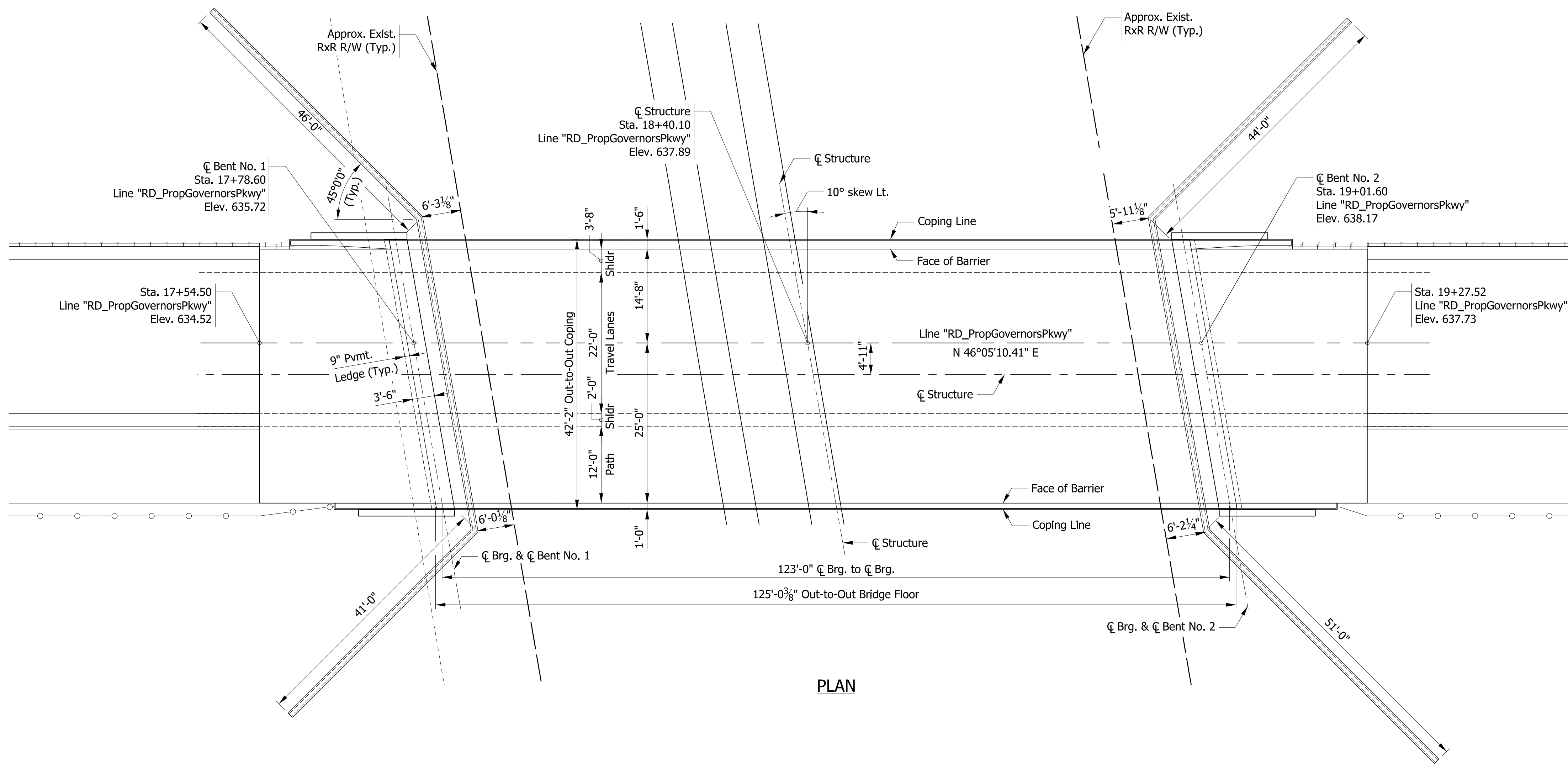
HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	45-00401
VERTICAL SCALE	DESIGNATION
1" = 10'	1900833
SURVEY BOOK	SHEETS
Electronic	5 of 11
CONTRACT	PROJECT
B-41851	1801907

Plot: 10/31/2021 1:25:17 PM

STRUCTURE TO BE BUILT ON A 200' VERTICAL CURVE AND A +5.00% TANGENT



ELEVATION



PLAN

GENERAL NOTES

Reinforcing steel cover shall be 2 1/2" in top and 1" minimum in bottom of floor slab, 3" in footings, except bottom steel which shall be 4", and 2" in all other parts, unless noted.

All reinforcement shall be epoxy coated.

Surface seal shall be applied to all exposed surfaces of concrete bridge railings and concrete bridge railing transitions, and all exposed faces of the end bents from outside face of exterior beam to end of wingwalls. Est. Quantity = 2,261 sq. ft.

DESIGN DATA

Designed for HL-93 loading, in accordance with AASHTO LRFD Bridge Design Specifications, 9th Edition, 2020.

DEAD LOAD

Actual weight plus 35 lb/ft<sup>2</sup> for future wearing surface and 15 lb/ft<sup>2</sup> for permanent metal deck forms.

FLOOR SLAB

Designed with a 7 1/2" structural depth plus 1/2" sacrificial wearing surface.

DESIGN STRESSES

CONCRETE

Class "A" Concrete f<sub>c</sub> = 3,500 p.s.i.  
Class "B" Concrete f<sub>c</sub> = 3,000 p.s.i.  
Class "C" Concrete f<sub>c</sub> = 4,000 p.s.i.

REINFORCING STEEL

Grade 60 f<sub>y</sub> = 60,000 p.s.i.

CONSTRUCTION LOADING

The exterior beam has been checked for strength, deflection, and overturning using the construction loads shown below. Cantilever overhang brackets were assumed for support of the deck overhang past the edge of the exterior beam. The finishing machine was assumed to be supported 6" outside the vertical coping form. The top overhang brackets were assumed to be located 6" past the edge of the vertical coping form. The bottom overhang brackets were assumed to be braced against the intersection of the beam bottom flange and web.

DECK FALSEWORK LOADS

Designed for 15 lb/ft<sup>2</sup> for permanent metal stay-in-place deck forms, removable deck forms, and 2-ft exterior walkway.

CONSTRUCTION LIVE LOAD

Designed for 20 lb/ft<sup>2</sup> extending 2-ft past the edge of coping and 75 lb/ft vertical force applied at a distance of 6 in. outside the face of coping over a 30-ft length of the deck centered with the finishing machine.

FINISHING-MACHINE LOAD

4500 lb distributed over 10-ft along the coping.

WIND LOAD

Designed for 70 mph horizontal wind loading in accordance with LRFD 3.8.1.

SEISMIC DESIGN DATA

Seismic Performance Zone TBD  
Peak Seismic Ground Acceleration, A<sub>s</sub> TBD  
Acceleration Coefficient, S<sub>D1</sub> TBD  
Seismic Soil Profile Type TBD

COMPOSITE PRESTRESSED  
CONCRETE BULB-TEE BEAM BRIDGE  
1 SPAN: 123'-0"; SKEW 10° LT.  
27'-8" CLEAR ROADWAY WIDTH  
GOVERNORS PARKWAY OVER NS RR  
LAKE COUNTY

DATE	REVISION

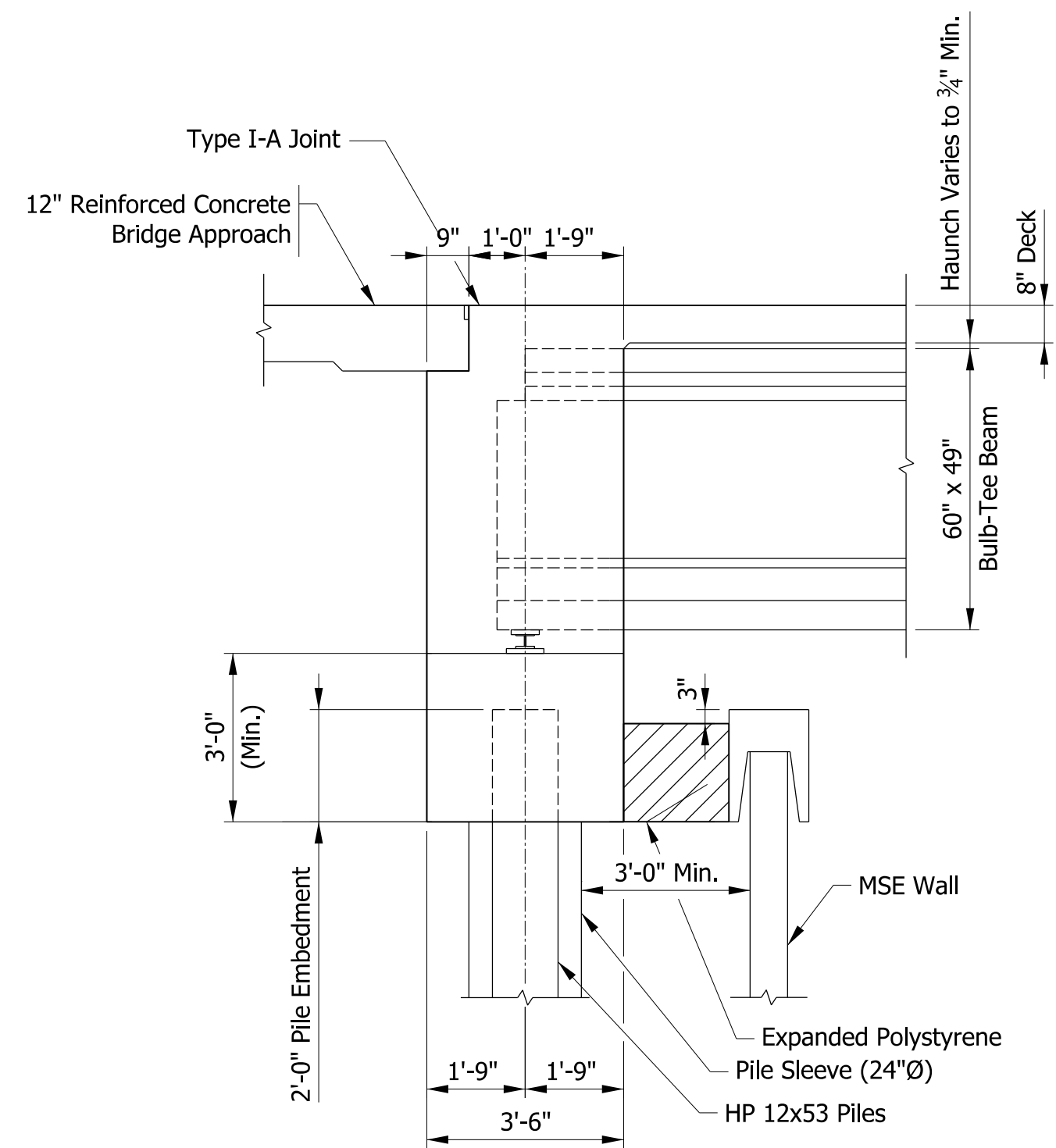
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	<i>BENG SIGNATURES</i>	DESIGN ENGINEER	10/31/2021
DESIGNED: DWH	10/31/2021	DRAWN: DWH	10/31/2021
CHECKED: RCM	10/31/2021	CHECKED: RCM	10/31/2021

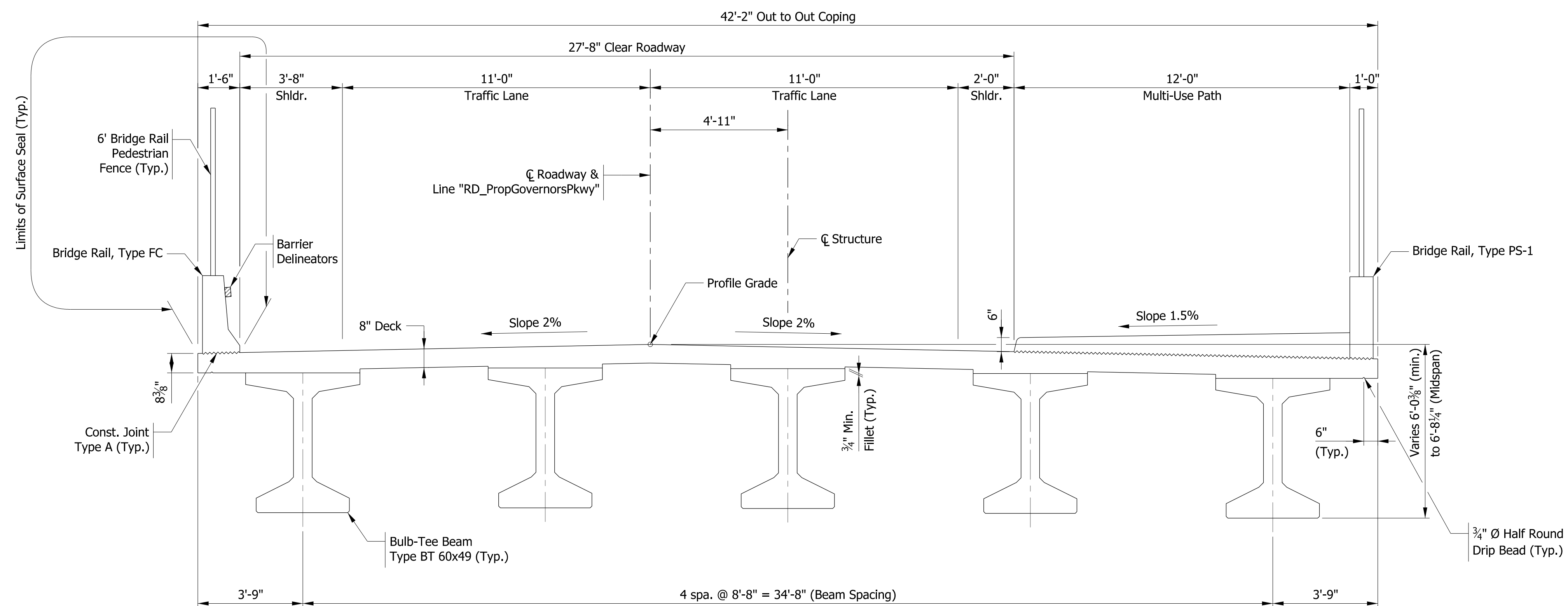
INDIANA DEPARTMENT OF TRANSPORTATION	
GENERAL PLAN	

SCALE	BRIDGE FILE
3/32" = 1'-0"	45-00401
	DESIGNATION
	1900833
SURVEY BOOK	SHEETS
Electronic	6 of 11
CONTRACT	PROJECT
B-41851	1801907

Plot: 10/31/2021 1:25:20 PM



TYPICAL INTEGRAL END BENT SECTION  
(SHOWING PROPOSED MSE WALL)



TYPICAL SECTION  
GOVERNORS PARKWAY OVER NS RAILROAD

COMPOSITE PRESTRESSED  
CONCRETE BULB-TEE BEAM BRIDGE  
1 SPAN: 123'-0"; SKEW 10° LT.  
27'-8" CLEAR ROADWAY WIDTH  
GOVERNORS PARKWAY OVER NS RR  
LAKE COUNTY

DATE	REVISION

NOT FOR  
CONSTRUCTION

RECOMMENDED FOR APPROVAL	<i>ENG SIGNATURES</i>	10/31/2021
DESIGNED: DWH	10/31/2021	DRAWN: DWH 10/31/2021
CHECKED: RCM	10/31/2021	CHECKED: RCM 10/31/2021

INDIANA DEPARTMENT OF TRANSPORTATION	
GENERAL PLAN	

SCALE	BRIDGE FILE
3/8" = 1'-0"	45-00401
	DESIGNATION
	1900833
SURVEY BOOK	SHEETS
Electronic	7 of 11
CONTRACT	PROJECT
B-41851	1801907

Plot: 10/31/2021 1:25:23 PM



PROJECT	DESIGNATION
1801907	1801907
CONTRACT	BRIDGE FILE
B-41851	N/A

KIN PROJECT INFORMATION	
DESIGNATION	PROJECT DESCRIPTION
1900833	GOVERNORS PARKWAY OVER RAILROAD NEW BRIDGE CONSTRUCTION

Note to Reviewer
PFC Plans - October 29, 2021

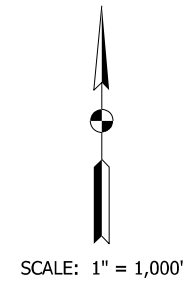
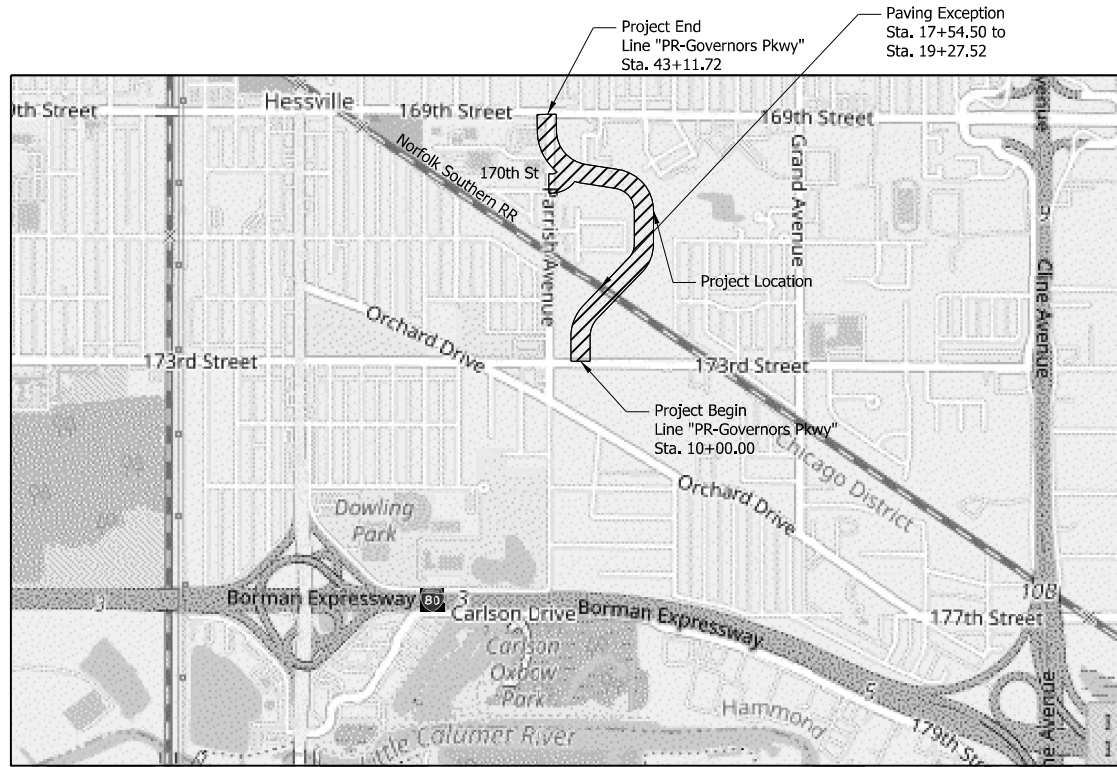
# INDIANA DEPARTMENT OF TRANSPORTATION



## ROAD PLANS

ROUTE: GOVERNORS PARKWAY  
PROJECT NO. 1801907 P.E.  
1801907 R/W  
1801907 CONST.

Project Description: New grade separation of Governors Parkway over Norfolk Southern Railroad corridor beginning from 173rd Street to 200 ft south of 169th Street  
Located 0.47 Miles North of I-80/I-94 and 0.92 Miles West of IN 912  
Section 10, T-36-N, R-9-W, North Township, Lake County, Indiana



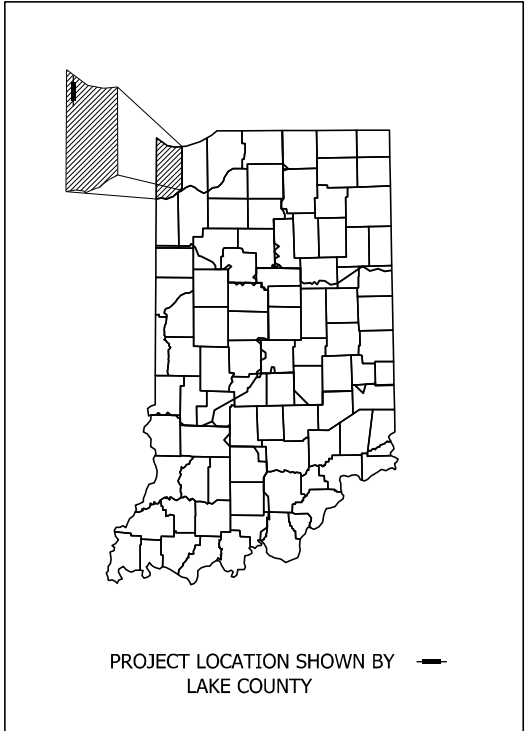
SCALE: 1" = 1,000'

Project Area

Location Map  
Lake County

TRAFFIC DATA		
A.A.D.T.	(2019)	3,503 V.P.D.
A.A.D.T.	(2042)	7,353 V.P.D.
D.H.V	(2042)	535 V.P.H.
DIRECTIONAL DISTRIBUTION		50 %
TRUCKS		2 % A.A.D.T. 2 % D.H.V.

DESIGN DATA	
DESIGN SPEED	30 M.P.H.
PROJECT DESIGN CRITERIA	NEW CONSTRUCTION (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MAJOR COLLECTOR
RURAL/URBAN	URBAN (INTERMEDIATE)
TERRAIN	LEVEL
ACCESS CONTROL	NONE



LATITUDE: 41°34'59" LONGITUDE: 87°26'58"

BRIDGE LENGTH:	0.033	MI.
ROADWAY LENGTH:	0.594	MI.
TOTAL LENGTH:	0.627	MI.
MAX. GRADE:	5.00	%

HUC: 071200030406

INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2022  
TO BE USED WITH THESE PLANS.

P:\PLOT\_DRAWING\_TIMES\_SAVE\_PMS

FILES



HDR ENGINEERING, INC.  
10401 NORTH MERIDIAN STREET  
INDIANAPOLIS, IN 46290-0900  
PHONE: (317) 558-4920

NOT FOR  
CONSTRUCTION

PLANS PREPARED BY:	HDR ENGINEERING INC.	(317) 558-4920 PHONE NUMBER
CERTIFIED BY:		\$SIG DATE\$ DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

B - 29

BRIDGE FILE	N/A
DESIGNATION	1801907
SURVEY BOOK	SHEETS
Electronic	1 of 83
CONTRACT	PROJECT
B-41851	1801907

- 10'-0"  
From Sta. 10+00.00 to Sta. 15+06.97
- Varies 10'-0" to 11'-5"  
From Sta. 15+06.97 to Sta. 15+56.97
- 11'-5"  
From Sta. 15+56.97 to Sta. 21+41.06
- Varies 11'-5" to 10'-0"  
From Sta. 21+41.06 to Sta. 21+91.06
- 10'-0"  
From Sta. 21+91.06 to Sta. 41+75.00

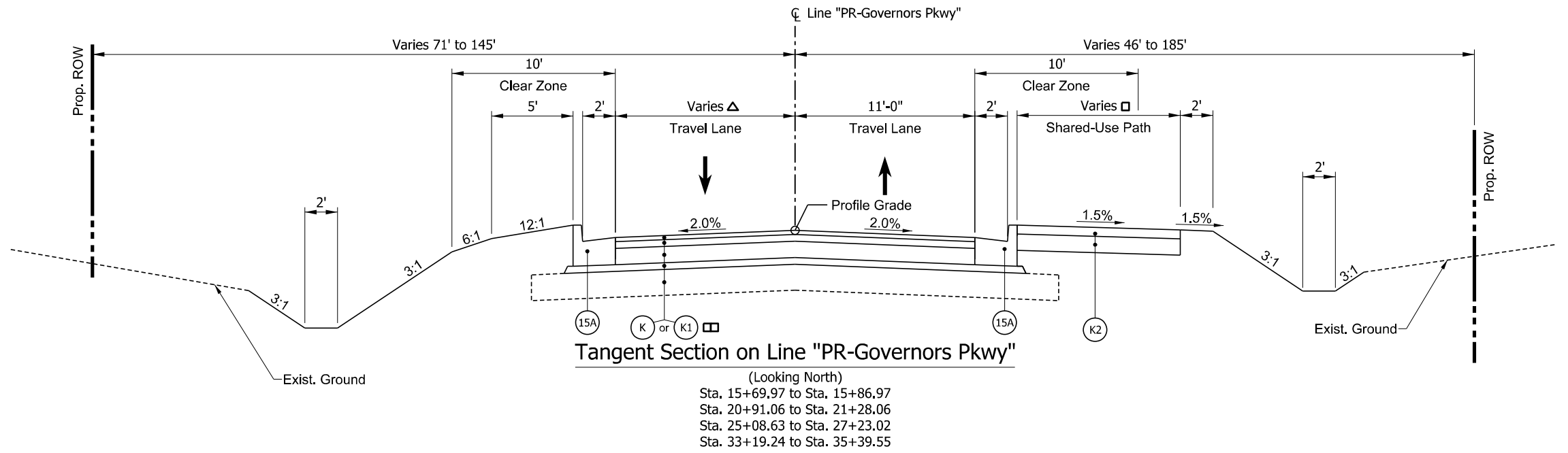
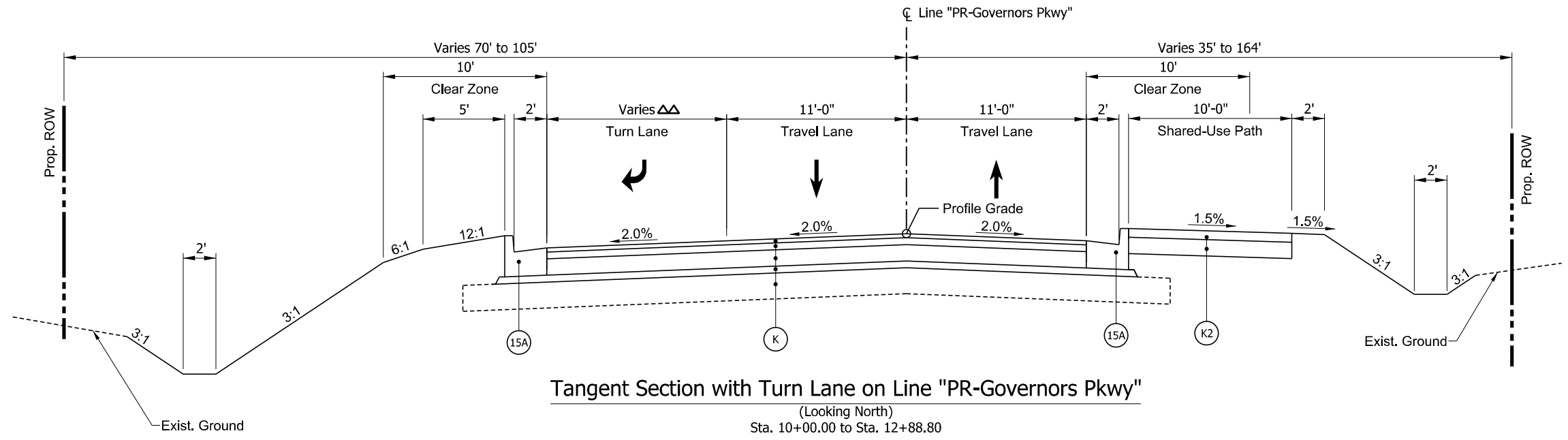
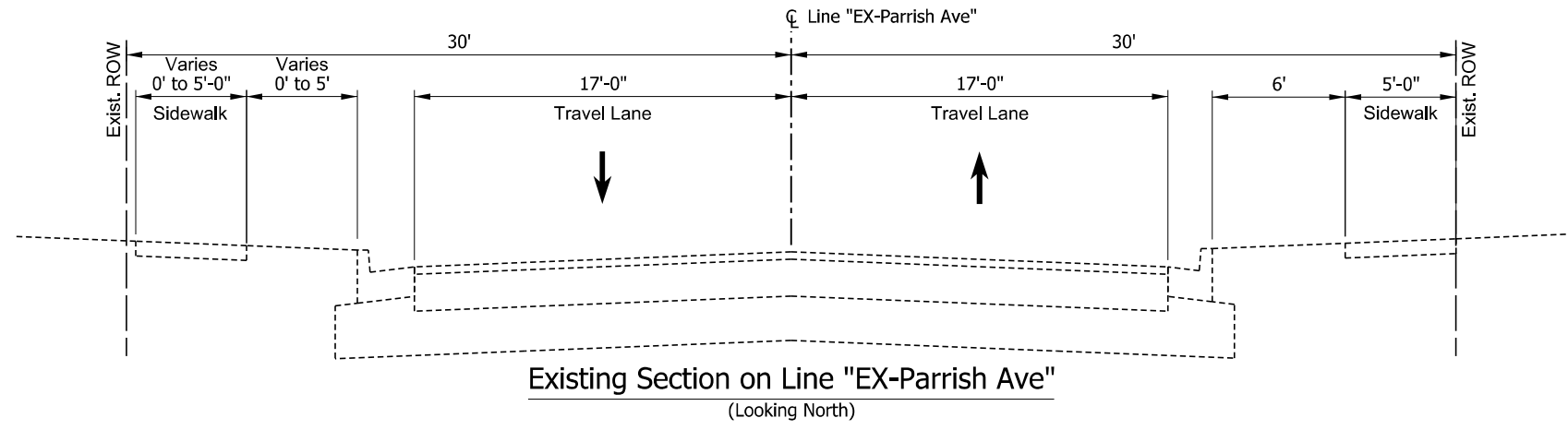
- (K) From Sta. 10+00.00 to Sta. 11+50.00
- From Sta. 25+50.00 to Sta. 43+11.72
- (K1) From Sta. 11+50.00 to Sta. 25+50.00

- 11'-0"  
From Sta. 10+00.00 to Sta. 11+88.80
- Varies 11'-0" to 0'-0"  
From Sta. 11+88.80 to Sta. 12+88.80

- △ 11'-0"  
From Sta. 10+00.00 to Sta. 15+06.97
- From Sta. 21+91.06 to Sta. 43+11.72

- Varies 11'-0" to 13'-0"  
From Sta. 15+06.97 to Sta. 15+86.97
- Varies 13'-0" to 11'-0"  
From Sta. 20+91.06 to Sta. 21+91.06

- △ 11'-0"  
From Sta. 10+00.00 to Sta. 11+88.80
- Varies 11'-0" to 0'-0"  
From Sta. 11+88.80 to Sta. 12+88.80



**Proposed Legend**

- (F) Concrete Sidewalk
- (K) Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type IBC
- (K1) Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type I
- (K2) HMA for Sidewalk:  
140 lbs/sy HMA Surface, Type B  
220 lbs/sy HMA Intermediate, Type B  
6" CA53 Stone  
Subgrade Treatment, Type III
- (15A) Curb and Gutter, Concrete, Modified
- (62) Pedestrian Fence

NOTE: HMA PAVEMENT DESIGN TO BE DETERMINED DURING FINAL DESIGN.

**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ BIZ _____	DRAWN: _____ NAH _____	
CHECKED: _____ JLP _____	CHECKED: _____ JLP _____	

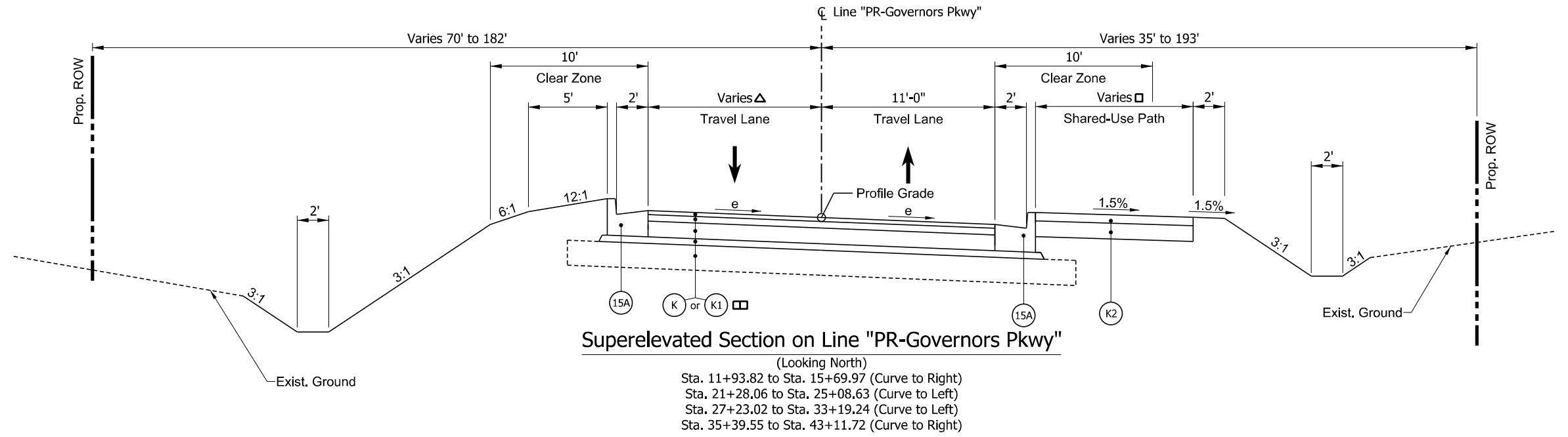
**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
LINE "PR-GOVERNORS PKWY"**

SCALE 1/4" = 1'-0"	BRIDGE FILE N/A
	DESIGNATION 1801907
SURVEY BOOK ELECTRONIC	SHEETS 3 of 83
CONTRACT B-41851	PROJECT 1801907

\$FILES\$

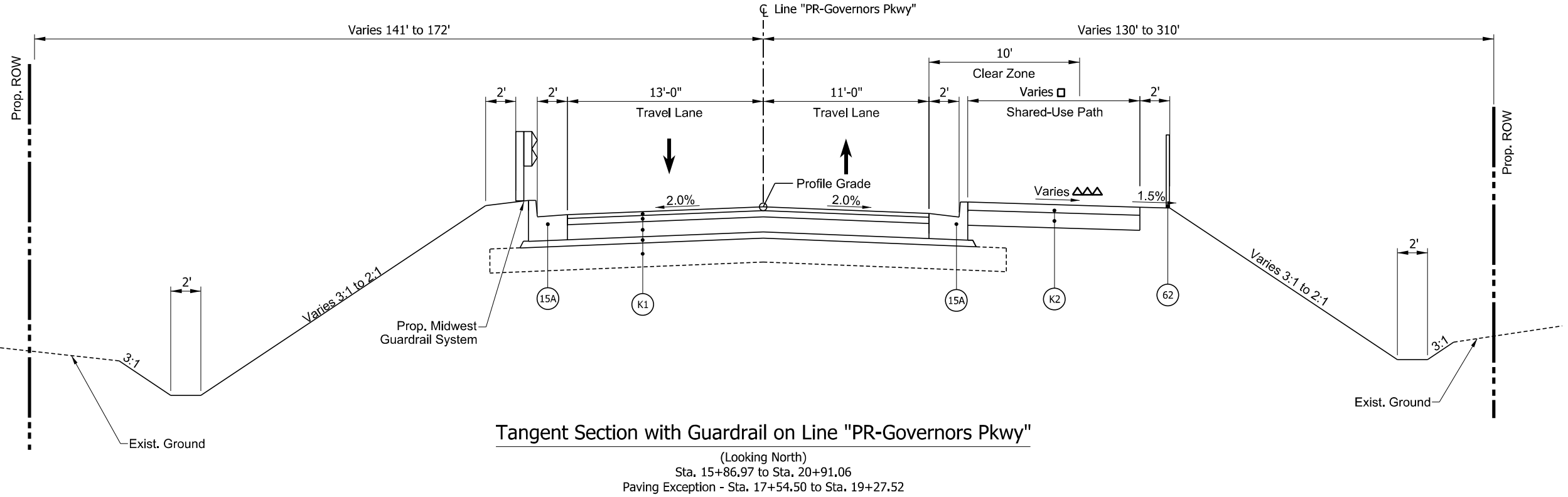
- See first typical section sheet for transitions.
- ▣ See first typical section sheet for transitions.
- △ See first typical section sheet for transitions.
- ▲▲▲ 1.5%  
From Sta. 15+86.97 to Sta. 17+04.50
- Varies 1.5% to -1.5%  
From Sta. 17+04.50 to Sta. 17+54.50
- Varies -1.5% to 1.5%  
From Sta. 19+27.52 to Sta. 19+77.52
- 1.5%  
From Sta. 19+77.52 to Sta. 20+91.06



### Proposed Legend

- (F) Concrete Sidewalk
- (K) Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type IBC
- (K1) Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type I
- (K2) HMA for Sidewalk:  
140 lbs/sy HMA Surface, Type B  
220 lbs/sy HMA Intermediate, Type B  
6" CA53 Stone  
Subgrade Treatment, Type III
- (15A) Curb and Gutter, Concrete, Modified
- (62) Pedestrian Fence

NOTE: HMA PAVEMENT DESIGN TO BE DETERMINED DURING FINAL DESIGN.



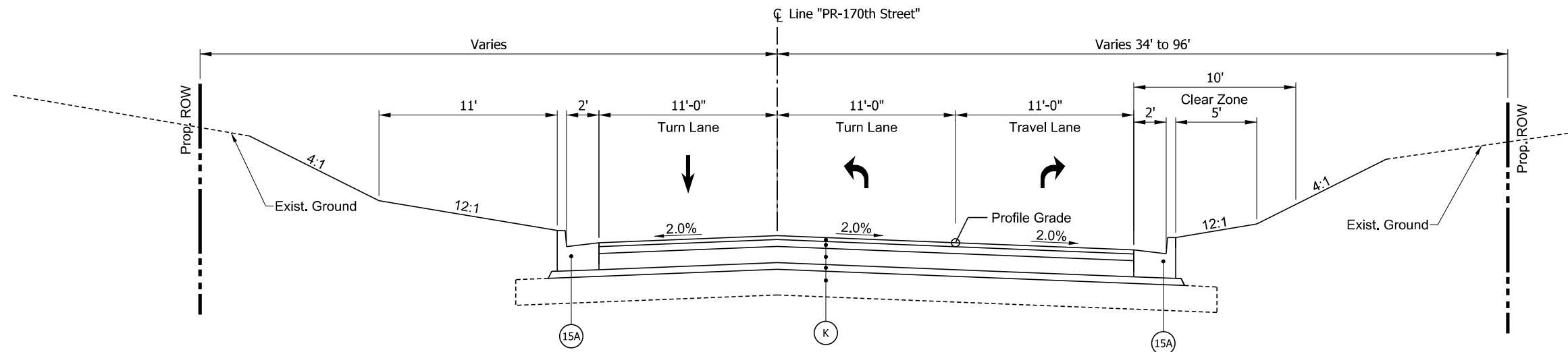
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ BIZ _____	DRAWN: _____ NAH _____	
CHECKED: _____ JLP _____	CHECKED: _____ JLP _____	

INDIANA  
DEPARTMENT OF TRANSPORTATION

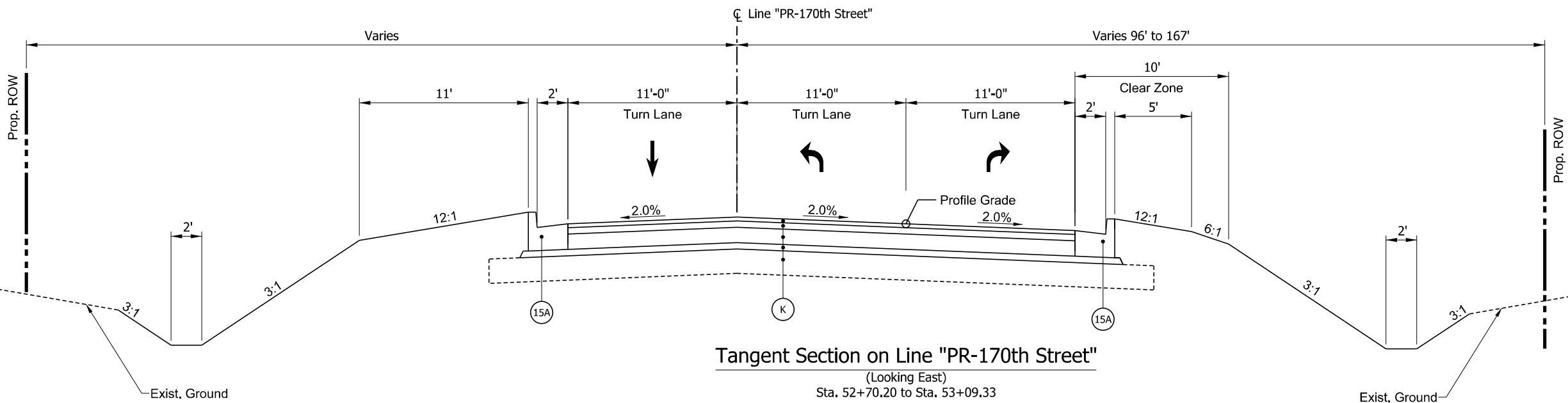
TYPICAL SECTIONS  
LINE "PR-GOVERNORS PKWY"

SCALE 1/4" = 1'-0"	BRIDGE FILE N/A
	DESIGNATION 1801907
SURVEY BOOK ELECTRONIC	SHEETS 4 of 83
CONTRACT B-41851	PROJECT 1801907



**Tangent Section on Line "PR-170th Street"**

(Looking East)  
Sta. 50+16.85 to Sta. 52+70.20



**Tangent Section on Line "PR-170th Street"**

(Looking East)  
Sta. 52+70.20 to Sta. 53+09.33

**Proposed Legend**

- (F) Concrete Sidewalk
- (K) Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type IBC
- (K1) Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type I
- (K2) HMA for Sidewalk:  
140 lbs/sy HMA Surface, Type B  
220 lbs/sy HMA Intermediate, Type B  
6" CA53 Stone  
Subgrade Treatment, Type III
- (15A) Curb and Gutter, Concrete, Modified
- (62) Pedestrian Fence

NOTE: HMA PAVEMENT DESIGN TO BE DETERMINED DURING FINAL DESIGN.

**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ BJB _____	DRAWN: _____ NAH _____	
CHECKED: _____ JLP _____	CHECKED: _____ JLP _____	

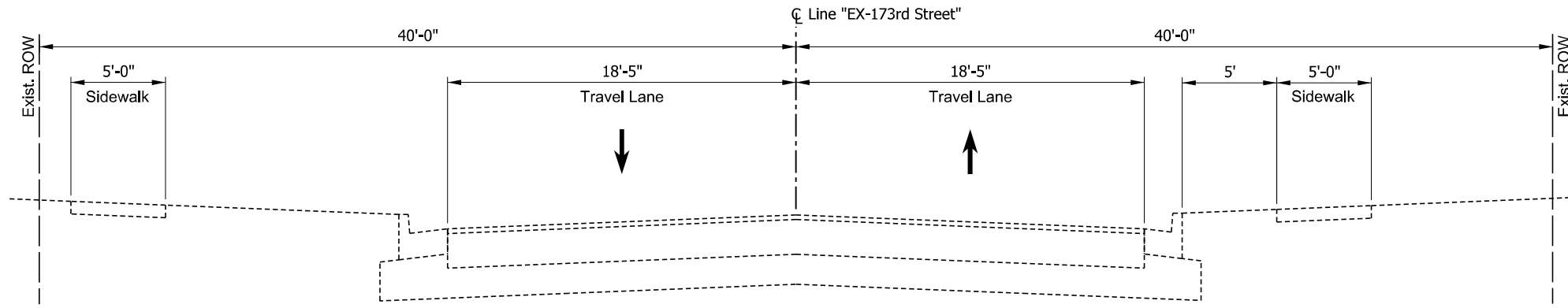
INDIANA  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
LINE "PR-170TH ST"

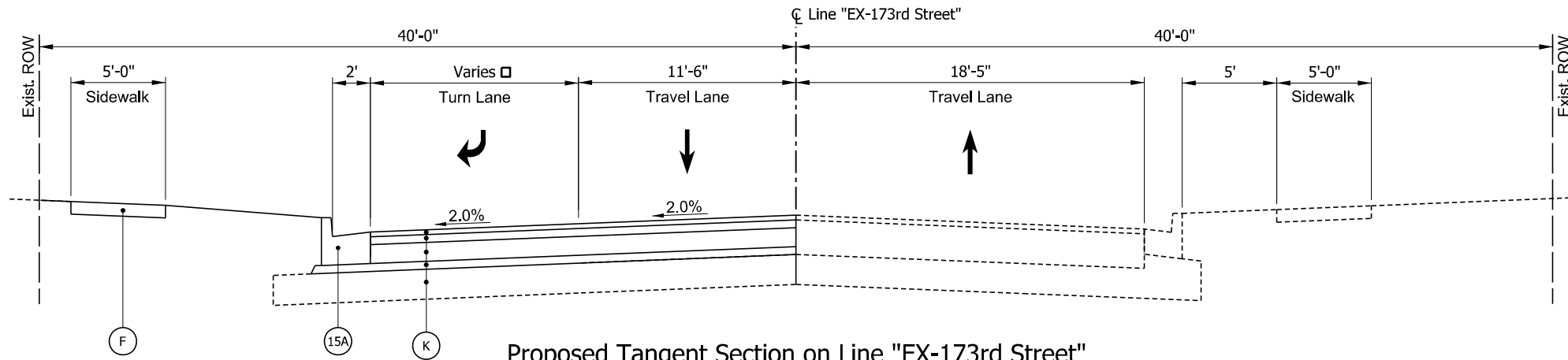
SCALE 1/4" = 1'-0"	BRIDGE FILE N/A
	DESIGNATION 1801907
SURVEY BOOK ELECTRONIC	SHEETS 5 of 83
CONTRACT B-41851	PROJECT 1801907



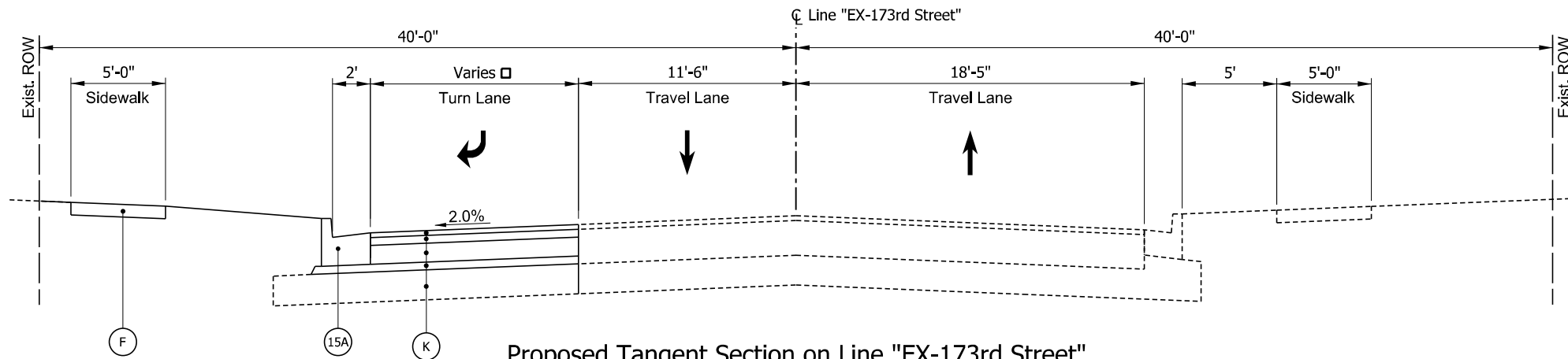
- 18'-7"  
From Sta. 12+44.44 to Sta. 13+49.40
- Varies 25'-6" to 22'-6"  
From Sta. 14+19.31 to Sta. 14+64.43
- 11'-0"  
From Sta. 14+64.43 to Sta. 15+00.42
- Varies 11'-0" to 7'-0"  
From Sta. 15+00.42 to Sta. 15+34.25



Existing Section on Line "EX-173rd Street"  
(Looking East)



Proposed Tangent Section on Line "EX-173rd Street"  
(Looking East)  
Sta. 10+44.45 to Sta. 14+64.43



Proposed Tangent Section on Line "EX-173rd Street"  
(Looking East)  
Sta. 14+64.43 to Sta. 15+34.25

**Proposed Legend**

- (F) Concrete Sidewalk
- (K) Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type IBC
- (K1) Full Depth HMA:  
165 lbs/sy QC/QA-HMA, 3, 70, Surface, 12.5 mm  
275 lbs/sy QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
330 lbs/sy QC/QA-HMA, 3, 64, Base, 25.0 mm  
3" CA53 Stone  
Subgrade Treatment, Type I
- (K2) HMA for Sidewalk:  
140 lbs/sy HMA Surface, Type B  
220 lbs/sy HMA Intermediate, Type B  
6" CA53 Stone  
Subgrade Treatment, Type III
- (15A) Curb and Gutter, Concrete, Modified
- (62) Pedestrian Fence

NOTE: HMA PAVEMENT DESIGN TO BE DETERMINED DURING FINAL DESIGN.

P:\PLOT\_DRAWING\_TEMPLATES\PLAN\_PMS

**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ BIZ _____	DRAWN: _____ NAH _____	
CHECKED: _____ JLP _____	CHECKED: _____ JLP _____	

INDIANA DEPARTMENT OF TRANSPORTATION	
<b>TYPICAL SECTIONS LINE "PR-173RD ST"</b>	

SCALE 1/4" = 1'-0"	BRIDGE FILE N/A
	DESIGNATION 1801907
SURVEY BOOK ELECTRONIC	SHEETS 6 of 83
CONTRACT B-41851	PROJECT 1801907

- 1 CHARLES KALLAS
- 2 MICHAEL DYE AND KRISTINA MORGAN
- 3 JAIME E ZAMORA AND ESMERALDA PALMERIN ZAMORA
- 4 ESCO INVESTMENT GROUP, LLC
- 5 ALICE M. CERVANTES
- 6 AMI R. GLIDEWELL
- 7 JOHN T. AND VIRGINIA L. RATAJCZAK
- 8 SHAYNE STEWART AND VICKY STEWART
- 9 MARTIN A. PACHECO
- 10 HUMBERTO LOPEZ

- 11 CHERYL DOWNEY
- 12 THE HAMMOND REDEVELOPMENT COMMISSION
- 13 CITY OF HAMMOND DEPARTMENT OF REDEVELOPMENT
- 14 LAKE COUNTY BOARD OF COMMISSIONERS



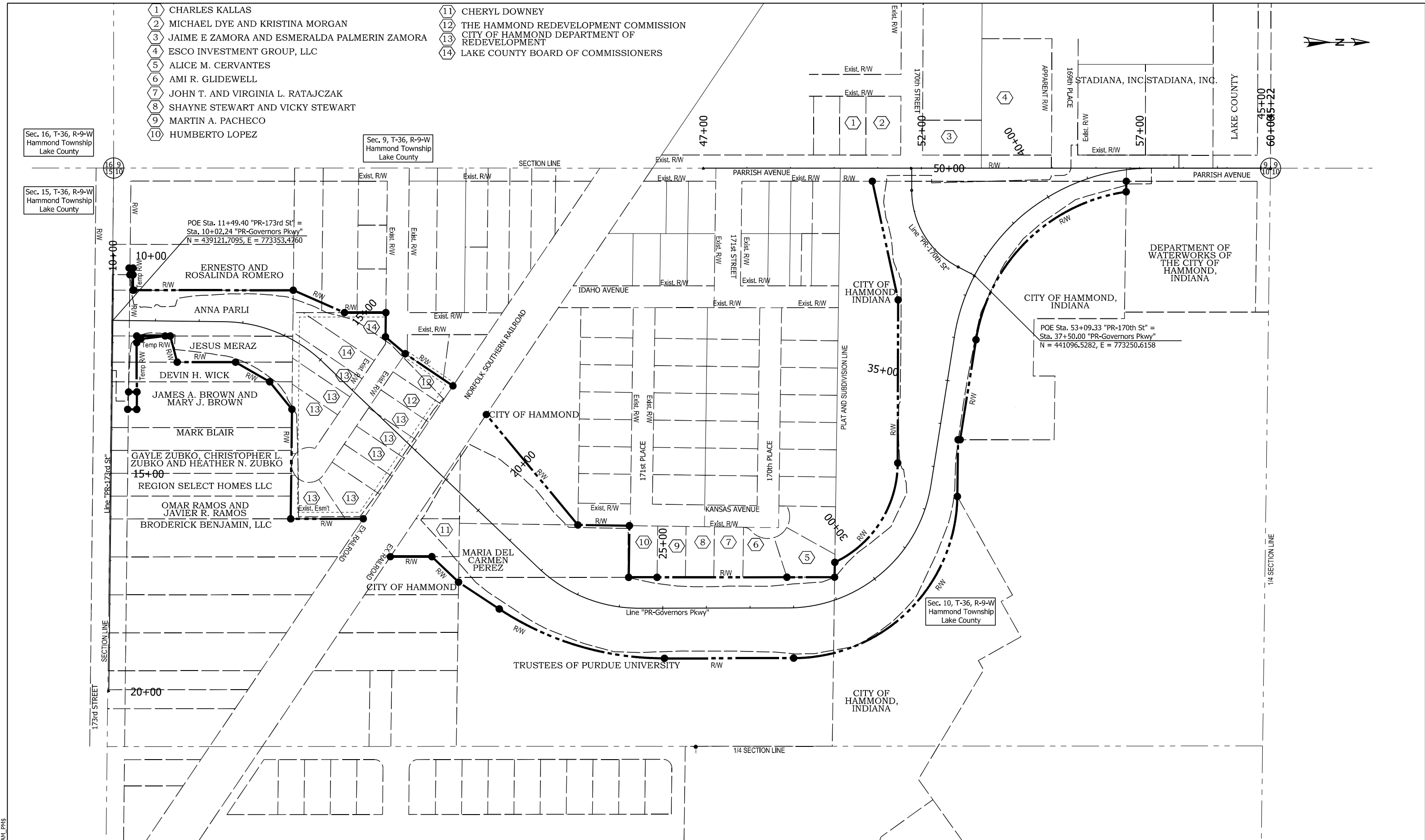
Sec. 16, T-36, R-9-W  
Hammond Township  
Lake County

Sec. 9, T-36, R-9-W  
Hammond Township  
Lake County

Sec. 15, T-36, R-9-W  
Hammond Township  
Lake County

POE Sta. 11+49.40 "PR-173rd St" =  
Sta. 10+02.24 "PR-Governors Pkwy"  
N = 439121.7095, E = 773353.4760

POE Sta. 53+09.33 "PR-170th St" =  
Sta. 37+50.00 "PR-Governors Pkwy"  
N = 441096.5282, E = 773250.6158



P:\PLOT\_DRAWING\_TITLES\_S&M\_PMS

**NOT FOR CONSTRUCTION**

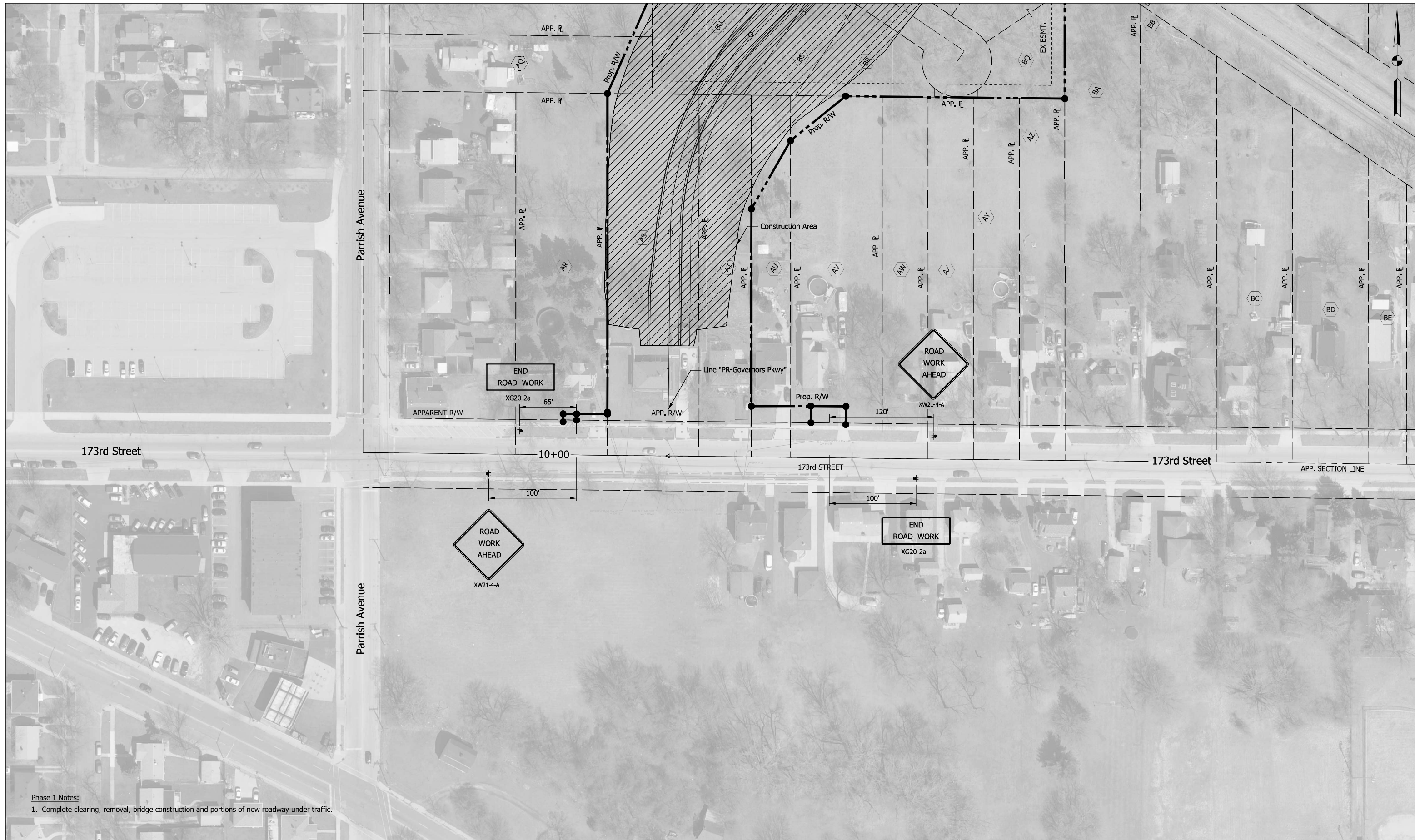
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DESIGNED: BJB	DRAWN: CMD	
CHECKED: JLP	CHECKED: JLP	

**INDIANA  
DEPARTMENT OF TRANSPORTATION**

**PLAT NO. 1**

SCALE 1" = 100'	BRIDGE FILE N/A
	DESIGNATION 1801907
SURVEY BOOK ELECTRONIC	SHEETS 7 of 83
CONTRACT B-41851	PROJECT 1801907





Phase 1 Notes:  
 1. Complete clearing, removal, bridge construction and portions of new roadway under traffic.

**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
		7/27/2022
DESIGNED: DDB	7/27/2022	DRAWN: DDB 7/27/2022
CHECKED: LNB	7/27/2022	CHECKED: LNB 7/27/2022

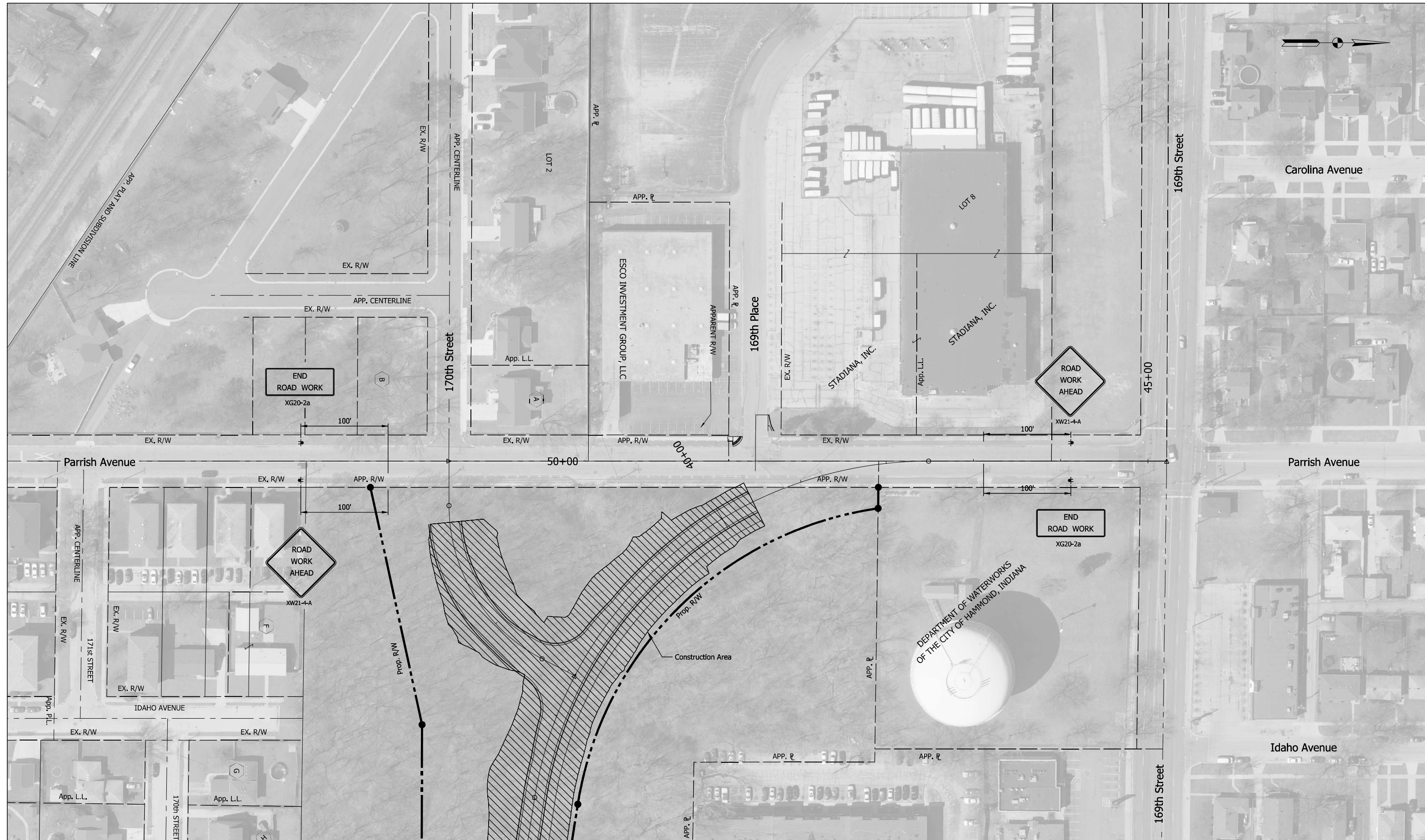
**INDIANA DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PHASE 1**

SCALE	BRIDGE FILE
1" = 50'	N/A
	DESIGNATION
	1801907
SURVEY BOOK	SHEETS
Electronic	9 of 83
CONTRACT	PROJECT
B-41851	1801907

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Plot: 7/27/2022 11:47:29 AM

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**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	7/27/2022	DATE
DESIGNED: DDB	7/27/2022	DRAWN: DDB	7/27/2022
CHECKED: LNB	7/27/2022	CHECKED: LNB	7/27/2022

**INDIANA DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PHASE 1**

SCALE	BRIDGE FILE
1" = 50'	N/A
	DESIGNATION
	1801907
SURVEY BOOK	SHEETS
Electronic	10 of 83
CONTRACT	PROJECT
B-41851	1801907





**Phase 2A Notes**  
 1. Shift vehicular traffic along 173rd Street to the south and construct the new Governors Parkway approach and new westbound right turn lane. Close existing sidewalk along the north side of 173rd Street and detour pedestrians around the phase work zone through the use of temporary sidewalks and temporary curb ramps.

**Legend**

- (A) Full Depth HMA Consisting of:  
 275 lbs/sys QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
 440 lbs/sys QC/QA-HMA, 3, 64, Base, 25.0 mm  
 6.5" CA53 Stone
- (B) Subgrade Treatment, Type IBC

**NOT FOR CONSTRUCTION**

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	7/27/2022	DATE
DESIGNED: DDB	7/27/2022	DRAWN: DDB	7/27/2022
CHECKED: LNB	7/27/2022	CHECKED: LNB	7/27/2022

**INDIANA DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PHASE 2A**

SCALE	BRIDGE FILE
1" = 50'	N/A
	DESIGNATION
	1801907
SURVEY BOOK	SHEETS
Electronic	11 of 83
CONTRACT	PROJECT
B-41851	1801907

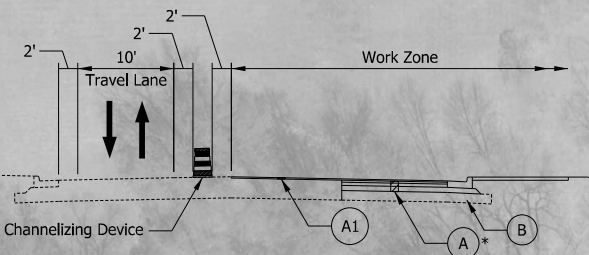
Plot: 7/27/2022 11:49:02 AM





173rd Street

173rd Street



Section A-A

\* Intermediate, Base, Subbase, and Subgrade Treatment layers were constructed in Stage 2A.

- Phase 2B Notes
1. Complete overlay of 173rd Street under traffic with the use of temporary flaggers. Open new sidewalks and curb ramps for use.

Legend

- (A) Full Depth HMA Consisting of:  
275 lbs/sys QC/QA-HMA, 3, 64, Intermediate, 12.5 mm  
440 lbs/sys QC/QA-HMA, 3, 64, Base, 25.0 mm  
6.5" CA53 Stone
- (A1) HMA Resurfacing Consisting of:  
165 lbs/sys QC/QA-HMA, 3, 70, Surface, 12.5 mm
- (B) Subgrade Treatment, Type IBC

NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	7/27/2022	DATE
DESIGNED: DDB	7/27/2022	DRAWN: DDB	7/27/2022
CHECKED: LNB	7/27/2022	CHECKED: LNB	7/27/2022

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 2B

SCALE	BRIDGE FILE
1" = 50'	N/A
	DESIGNATION
	1801907
SURVEY BOOK	SHEETS
Electronic	12 of 83
CONTRACT	PROJECT
B-41851	1801907

Plot: 7/27/2022 11:50:36 AM