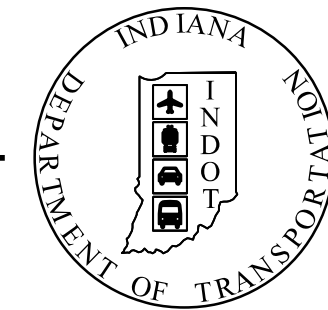


PROJECT	DESIGNATION
2001172	2002114 & 2002115
CONTRACT	BRIDGE FILE
R-42909	I65-177-02402 CNBL & JDSBL

INDIANA DEPARTMENT OF TRANSPORTATION



STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
I65-177-02402 CNBL I65-177-02402 JDSBL	CONTINUOUS COMPOSITE STEEL BEAM BRIDGE	02402 CNBL 6 SPANS: 50', 70', 70', 90', 105' & 90' 02402 JDSBL 6 SPANS: 50', 80.5', 80.5', 90', 105' & 90' SKEW: 14°46'00" LT. TO TANGENT	CSX R.R., 9TH STREET, BURNETTS CREEK & WABASH TRAIL	876+17.25

KIN PROJECT INFORMATION		
DESIGNATION	PROJECT DESCRIPTION	
	ROAD	
2001172	ADDED TRAVEL LANES FROM 1.54 MILES NORTH OF SR 25 TO 2.46 MILES NORTH OF SR 43	LEAD DES.
	BRIDGE	
2002114	I-65 NB OVER CSX R.R., 9TH STREET, AND BURNETTS CREEK	
2002115	I-65 SB OVER CSX R.R., 9TH STREET, AND BURNETTS CREEK	
1601088	I-65 NB OVER SR 43	
1601090	I-65 SB OVER SR 43	
2002116	I-65 NB OVER PROPHETS ROCK ROAD	
2002117	I-65 SB OVER PROPHETS ROCK ROAD	
2002364	CR 725 N OVER I-65	

BRIDGE REHABILITATION PLANS

FOR SPANS OVER 20 FEET

ROUTE: I-65 AT: RP 177+32

PROJECT NO. 2001172 P.E. CONST.

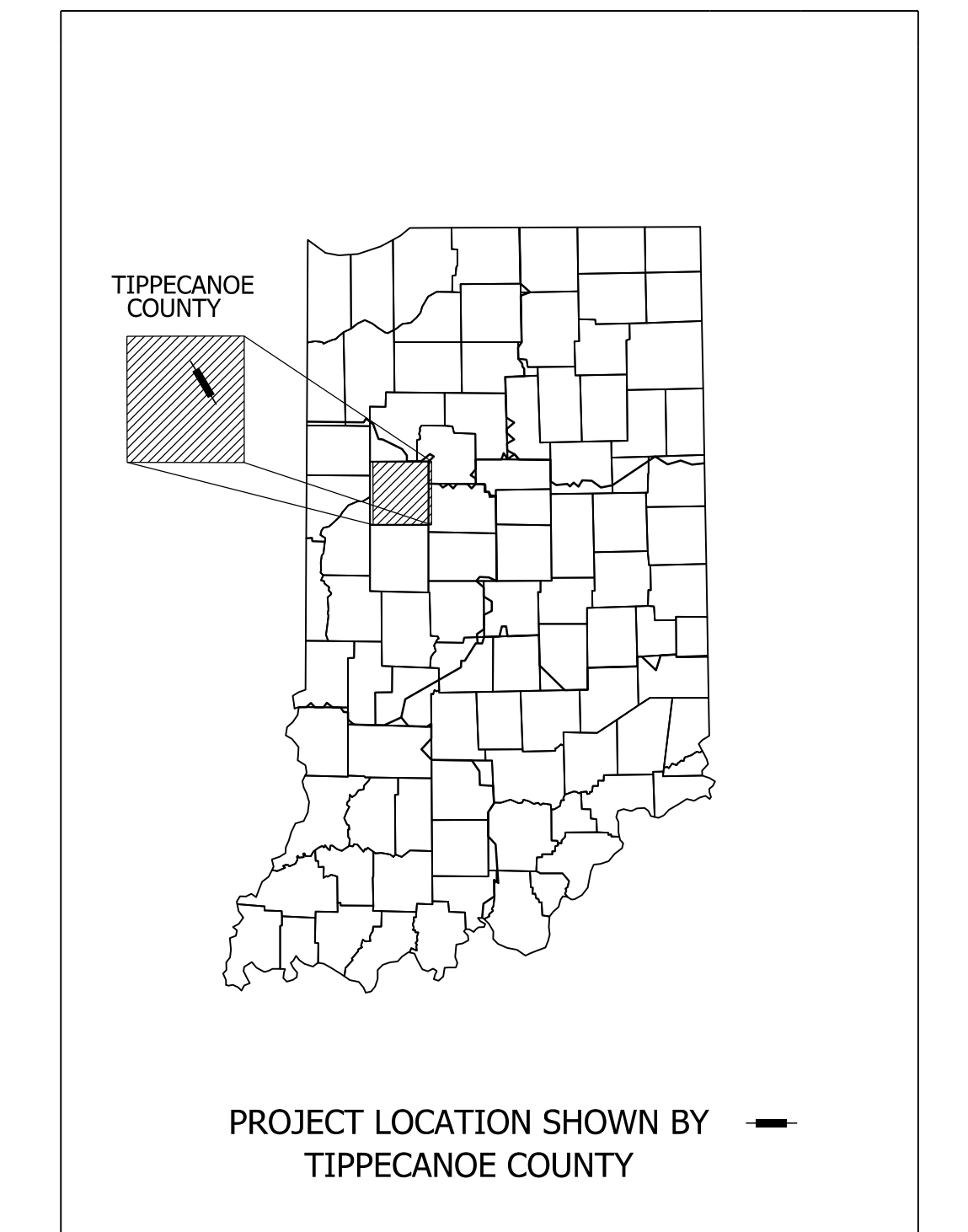
NO ADDITIONAL RIGHT-OF-WAY
REQUIRED FOR THIS PROJECT

BRIDGE DECK REPLACEMENT, WIDENING AND SUPERSTRUCTURE REHABILITATION ON
STRUCTURE I65-177-02402 BNBL AND STRUCTURE I65-177-02402 JCSBL.
THE STRUCTURES ARE LOCATED ON INTERSTATE 65 (I-65) APPROXIMATELY 1.15 MILES NORTH
OF THE STATE ROAD (SR) 25 INTERCHANGE. IN SECTION 27 TOWNSHIP 24 N, RANGE 4 WEST
IN TIPPECANOE TOWNSHIP, TIPPECANOE COUNTY, INDIANA.

RR COORDINATION PLAN SUBMITTAL
JANUARY 13, 2021

TRAFFIC DATA		
		I-65
A.A.D.T. (2024)		47,400 V.P.D.
A.A.D.T. (2044)		56,670 V.P.D.
D.H.V. (2044)		4,690 V.P.H.
DIRECTIONAL DISTRIBUTION		50 %
TRUCKS		38 % A.A.D.T. 23 % D.H.V.

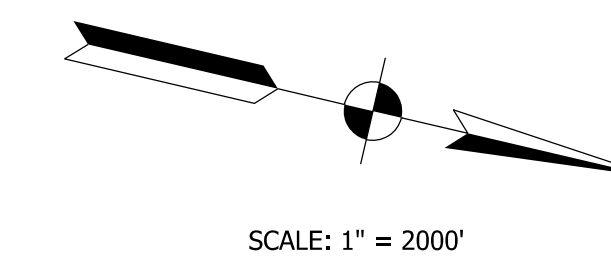
DESIGN DATA	
DESIGN SPEED	65 M.P.H.
PROJECT DESIGN CRITERIA	RECONSTRUCTION (FREEWAY)
FUNCTIONAL CLASSIFICATION	INTERSTATE
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	FULL



LATITUDE: 40°29'31" N LONGITUDE: 86°51'36" W

BRIDGE LENGTH: 0.095 MI.
ROADWAY LENGTH: SEE ROADWAY PLANS MI.
TOTAL LENGTH: SEE ROADWAY PLANS MI.
MAX. GRADE: 2.16 %

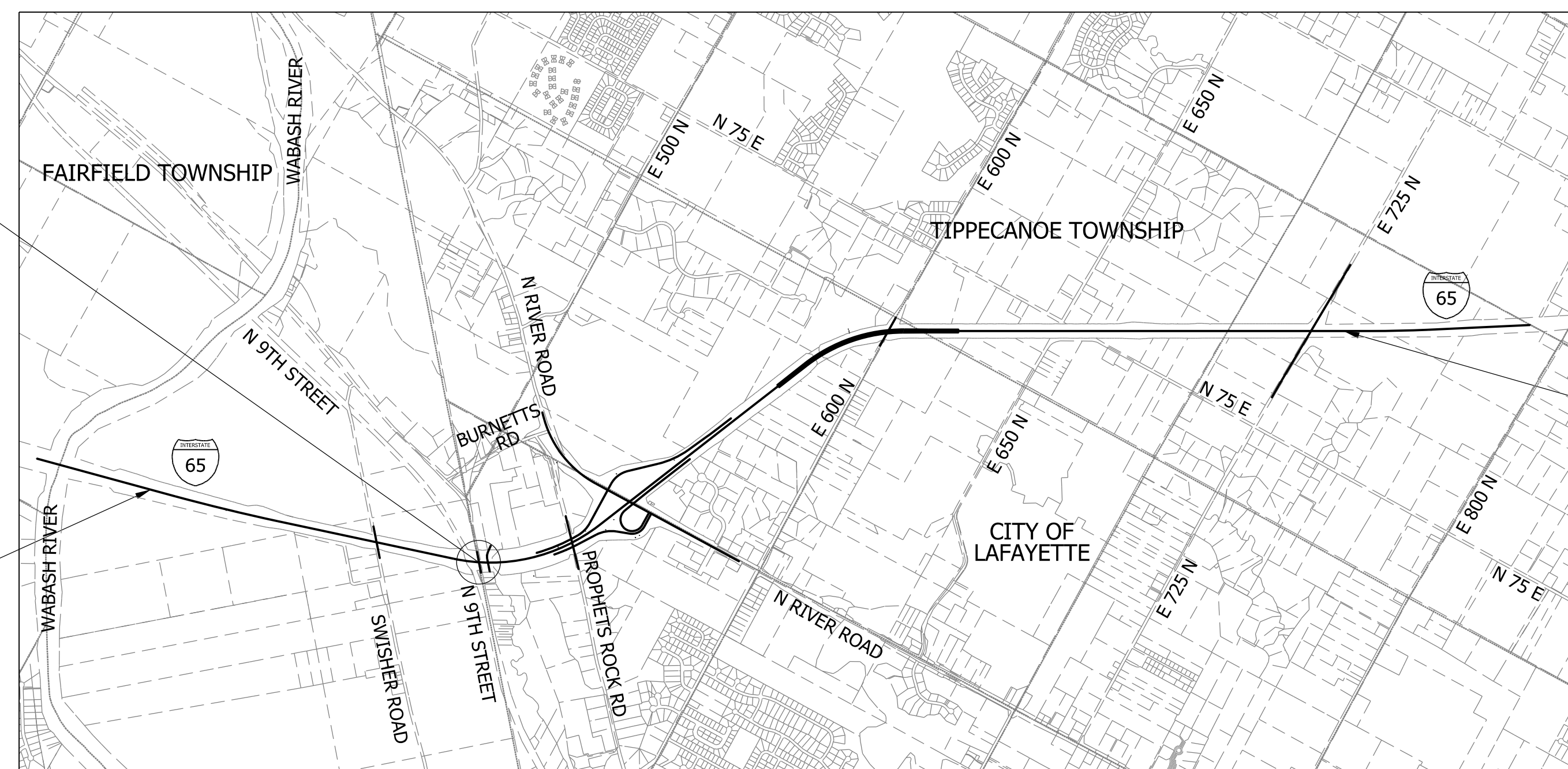
HUC: 051201080202



BRIDGE FILE NO.
I65-177-02402 CNBL
I65-177-02402 JDSBL
I-65 OVER CSX R.R., 9TH STREET
AND BURNETTS CREEK

BEGIN PROJECT
PROJECT NO. 2001172
STA. 822+25.00
LINE "PR-A"

END PROJECT
PROJECT NO. 2001172
STA. 1036+03.00
LINE "PR-A"



LOCATION MAP
TIPPECANOE COUNTY

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS.

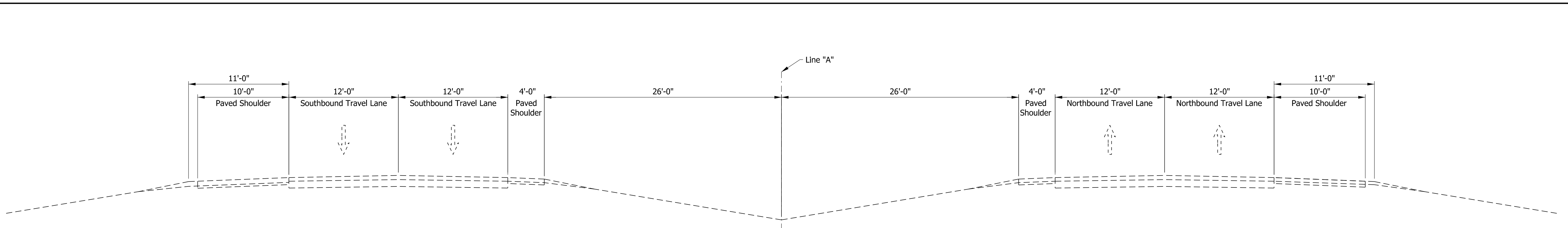


HNTB Indiana, Inc.
The HNTB Companies
Infrastructure Solutions
111 Monument Circle
Suite 1200
Indianapolis, IN 46204

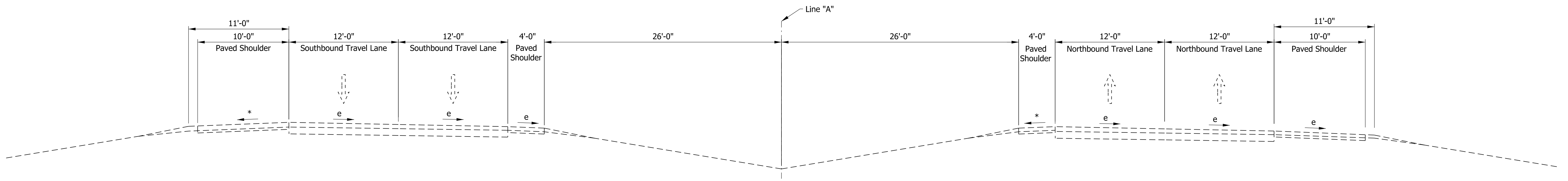
DRAFT
NOT FOR CONSTRUCTION

PLANS PREPARED BY: HNTB Indiana, Inc. (317) 636-4682
PHONE NUMBER
CERTIFIED BY: _____ DATE
APPROVED FOR LETTING: _____ DATE
INDIANA DEPARTMENT OF TRANSPORTATION

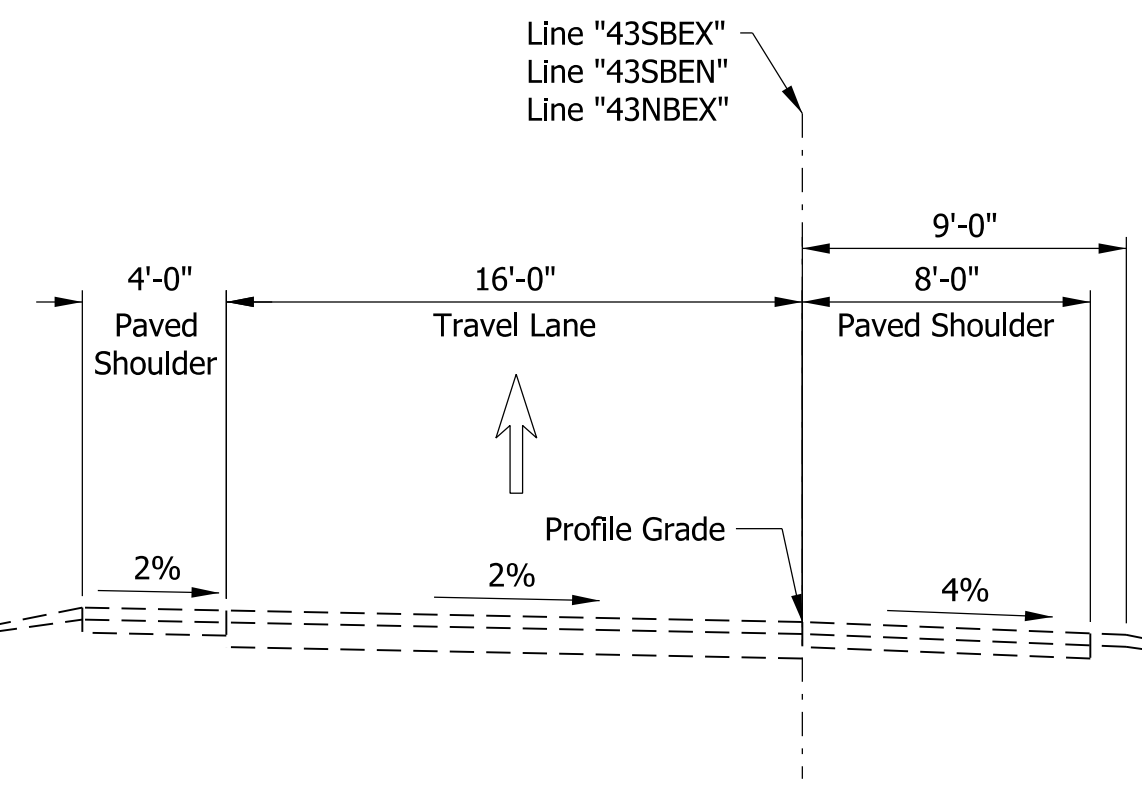
BRIDGE FILE		
I65-177-02402 CNBL & JDSBL		
DESIGNATION		
2002114 & 2002115		
SURVEY BOOK	SHEETS	TI-01
ELECTRONIC	1 of	30
CONTRACT		PROJECT
R-42909		2001172



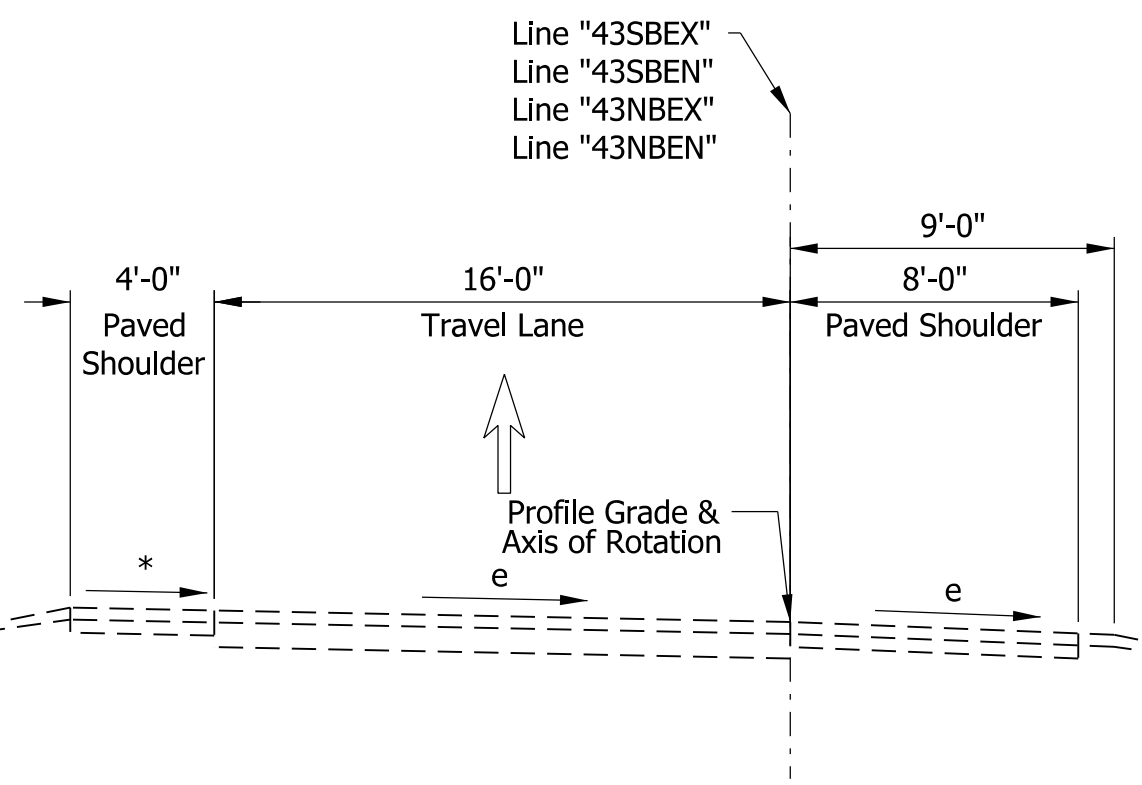
EXISTING I-65 TYPICAL SECTION



EXISTING I-65 TYPICAL SECTION IN SUPERELEVATION



EXISTING I-65 RAMP TYPICAL SECTION



EXISTING I-65 RAMP TYPICAL SECTION IN SUPERELEVATION

FOR INFORMATION ONLY
SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

- Notes:
1. See DET-01 for Legend.
 2. See TS-XX for Safety Edge and Underdrain Details.
 3. See Landscaping plans for seeding information.

c:\p1\162021_5\46:35 pm
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 file: c:\cadd\lib\pwr\hmb_krales\graafakes\0249926\2001172-s-pr-is01.dgn

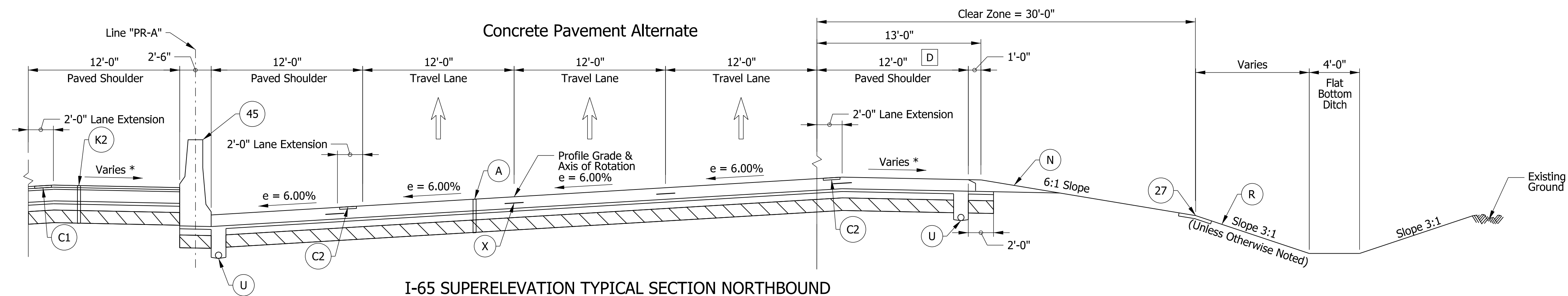
DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ DCK _____	DRAWN: _____ DCK _____	
CHECKED: _____ MKM _____	CHECKED: _____ MKM _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

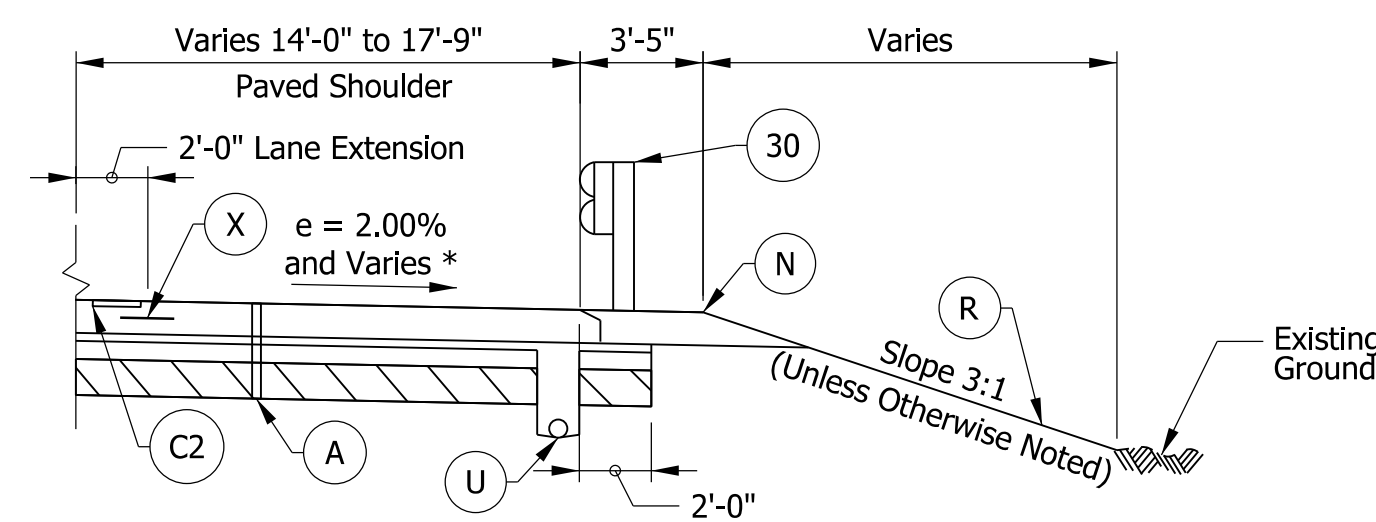
TYPICAL CROSS SECTIONS
I-65 LINE "PR-A"

HORIZONTAL SCALE	BRIDGE FILE
3/16"=1'-0"	I65-177-02402 CNBL & JDSBL
VERTICAL SCALE	DESIGNATION
N/A	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	4 of 30
CONTRACT	PROJECT
R-42909	2001172



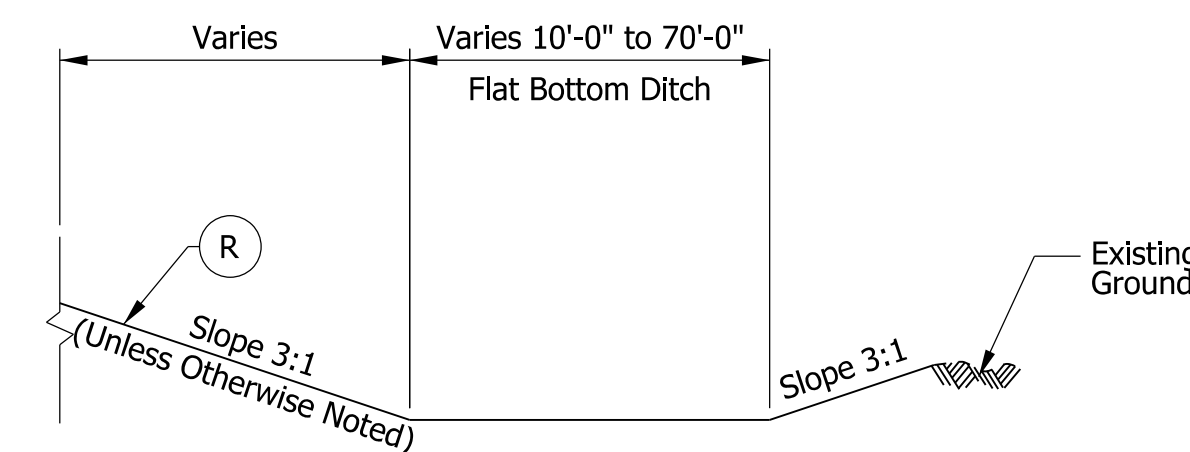
I-65 SUPERELEVATION TYPICAL SECTION NORTHBOUND

A STA. 867+17.00 "PR-A" to STA. 873+15.76 "PR-A"
 STA. 878+87.58 "PR-A" to STA. 892+50.89 "PR-A"
 STA. 894+26.50 "PR-A" to STA. 903+41.00 "PR-A"



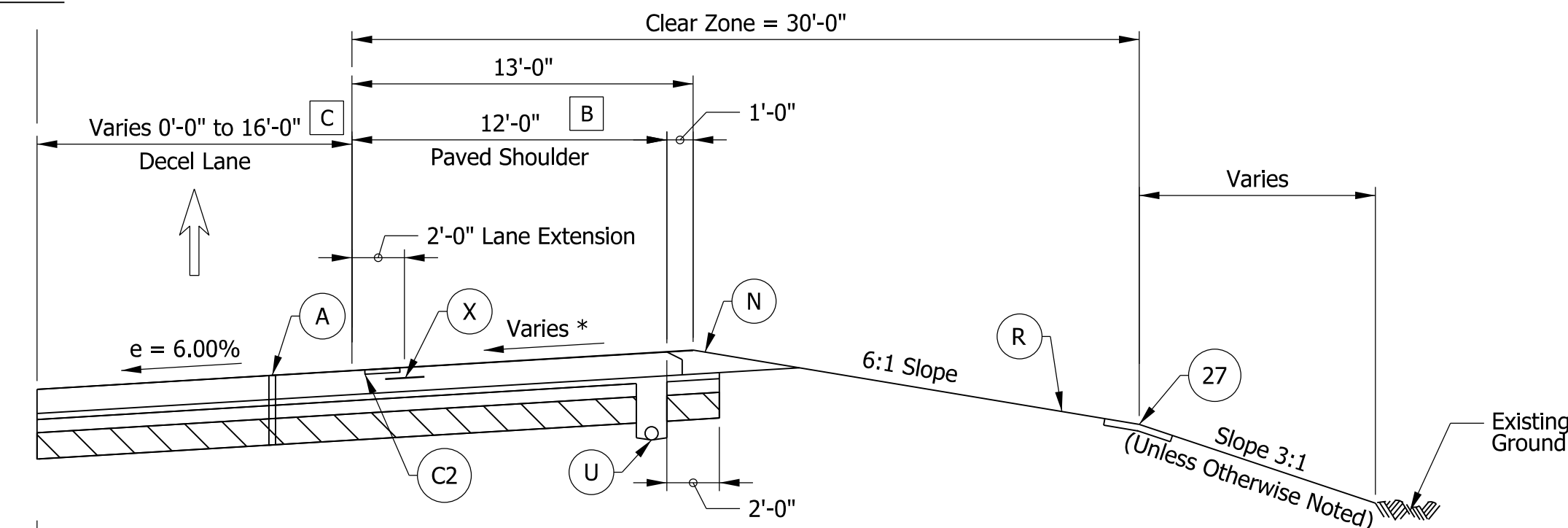
NB GUARDRAIL AND OUTSIDE SHOULDER DETAIL

STA. 870+45.03 "PR-A" to STA. 873+15.76 "PR-A"
 STA. 888+73.91 "PR-A" to STA. 892+50.89 "PR-A"



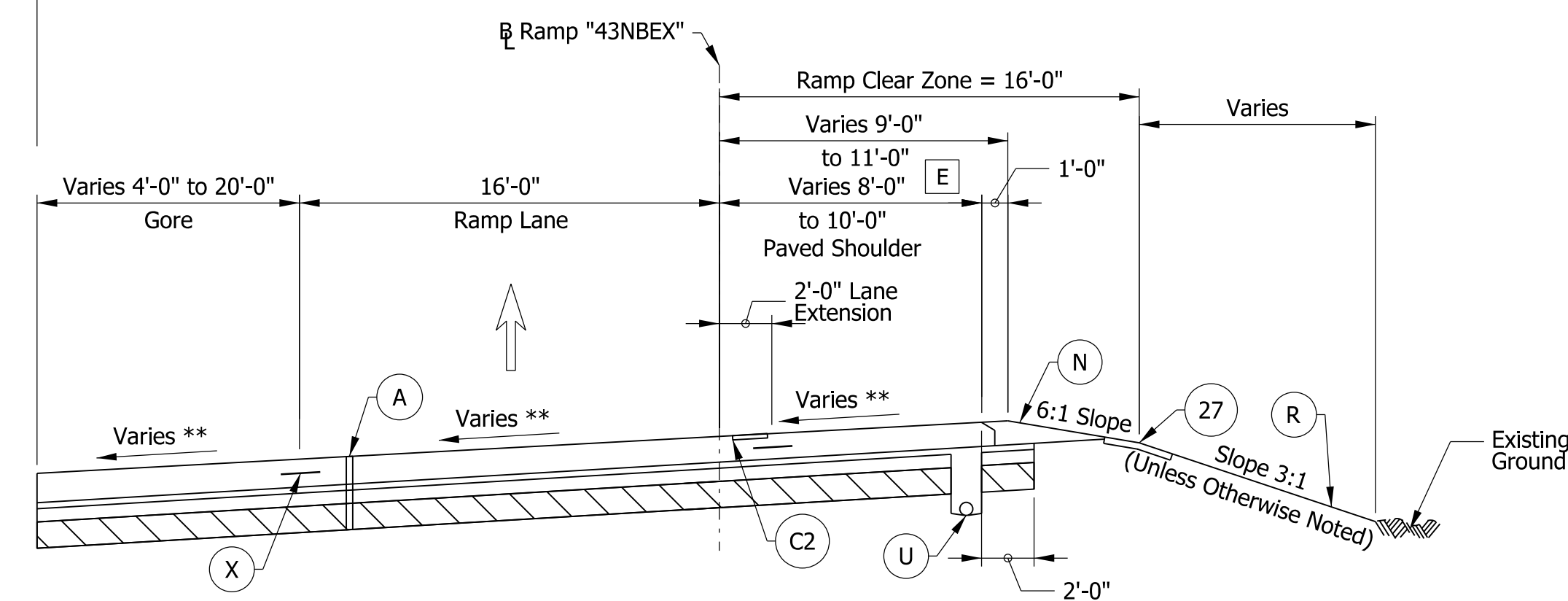
OUTSIDE WIDE DITCH DETAIL

STA. 886+00.00 "PR-A" to STA. 889+00.00 "PR-A" NB



DECELERATION LANE - I-65 NORTHBOUND AT SR 43

STA. 885+70.00 "PR-A" to STA. 892+07.73 "PR-A"



EXIT RAMP GORE SECTION - I-65 NORTHBOUND AT SR 43

STA. 892+07.73 "PR-A" to STA. 892+50.89 "PR-A"
 STA. 894+26.50 "PR-A" to STA. 896+10.44 "PR-A"

- A NB Bridge Paving Exception from STA. 873+15.76 "PR-A" to STA. 878+87.58 "PR-A"
 NB Bridge Paving Exception from STA. 892+50.89 "PR-A" to STA. 894+26.50 "PR-A"
- B Width varies at bridge approaches. Shoulder widening at 15:1 taper rate.
 See Construction Detail sheets for detailed dimensions.
- C See Construction Detail sheets for limits of acceleration and deceleration lanes
 and taper limits.
- D Width varies from 12'-0" to 16'-1.5" from STA. 878+87.58 "PR-A" to STA. 879+65.00 "PR-A"
- E Width varies from 12'-0" to 12'-11.5" from STA. 894+26.50 "PR-A" to STA. 894+63.05 "PR-A"

FOR INFORMATION ONLY
 SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

Notes:

- 1. See DET-01 for Legend.
- * Max rollover between shoulder and travel lane not to exceed 8%.
 See Superelevation Detail for transition information
- ** Slope varies. See Gore Details.

NOTE TO REVIEWER:

Pavement design assumed to be asphalt for estimating purposes. Pavement design to be approved in a future submittal.

NOTE TO REVIEWER:

Safety Edge detail to be provided in a future submittal.

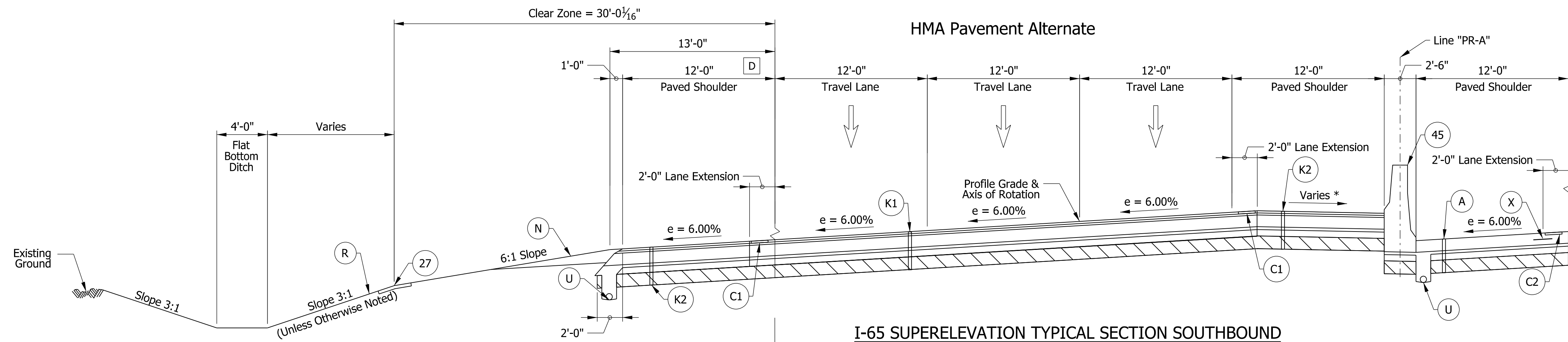
DRAFT
 NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: _____	DCK	DRAWN: _____
CHECKED: _____	MKM	CHECKED: _____
	MKM	

INDIANA
 DEPARTMENT OF TRANSPORTATION

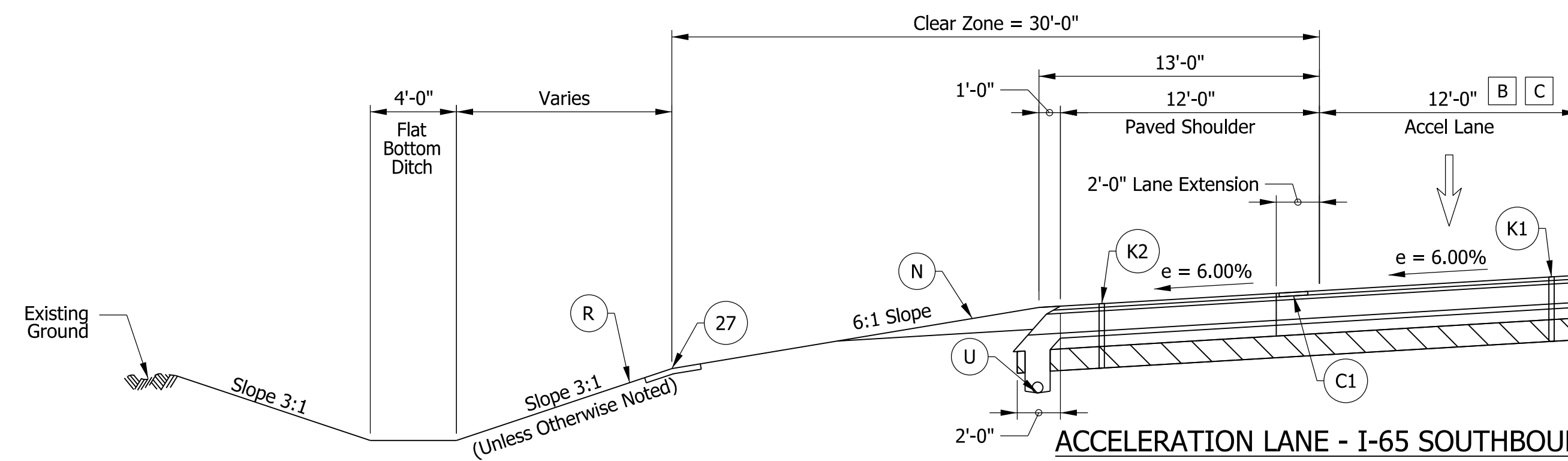
TYPICAL CROSS SECTIONS
 I-65 LINE "PR-A"

HORIZONTAL SCALE	BRIDGE FILE
3/16"=1'-0"	165-177-02402 CNBL & JDSBL
VERTICAL SCALE	DESIGNATION
N/A	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	5 of 30
CONTRACT	PROJECT
R-42909	2001172



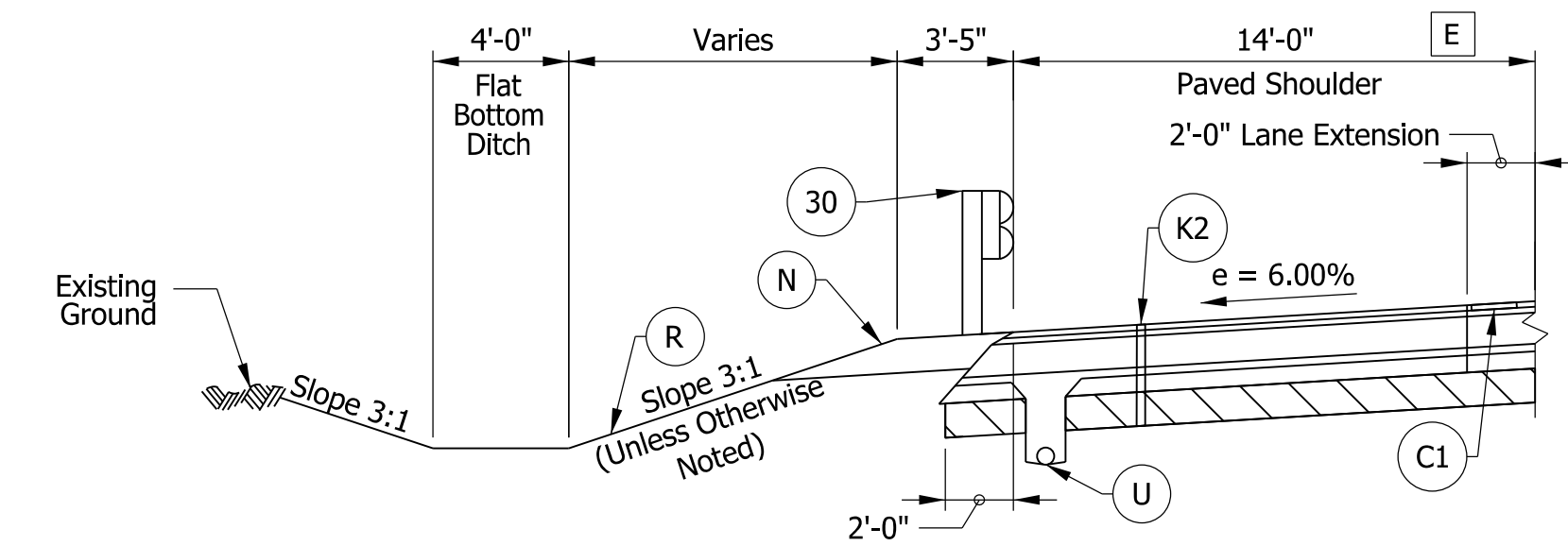
I-65 SUPERELEVATION TYPICAL SECTION SOUTHBOUND

A STA. 867+17.00 "PR-A" to STA. 872+72.35 "PR-A"
 STA. 878+76.30 "PR-A" to STA. 892+70.14 "PR-A"
 STA. 894+49.55 "PR-A" to STA. 903+41.00 "PR-A"



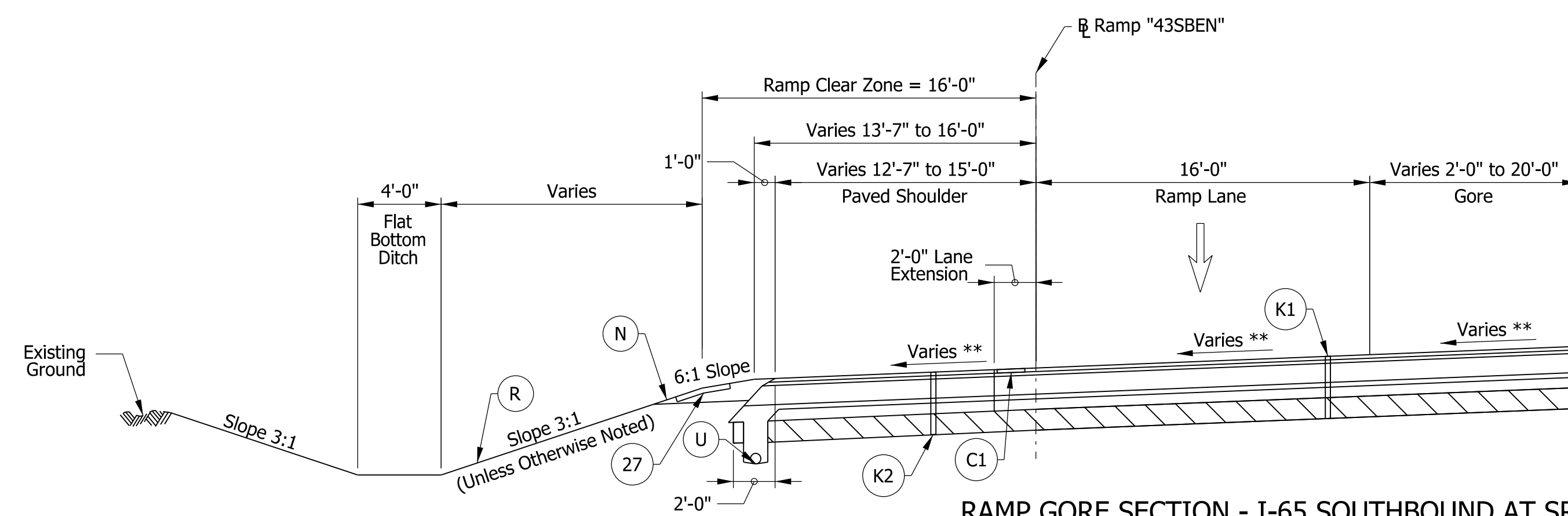
ACCELERATION LANE - I-65 SOUTHBOUND AT SR 43

STA. 879+14.50 "PR-A" to STA. 892+70.14 "PR-A"
 STA. 894+49.57 "PR-A" to STA. 895+94.65 "PR-A"



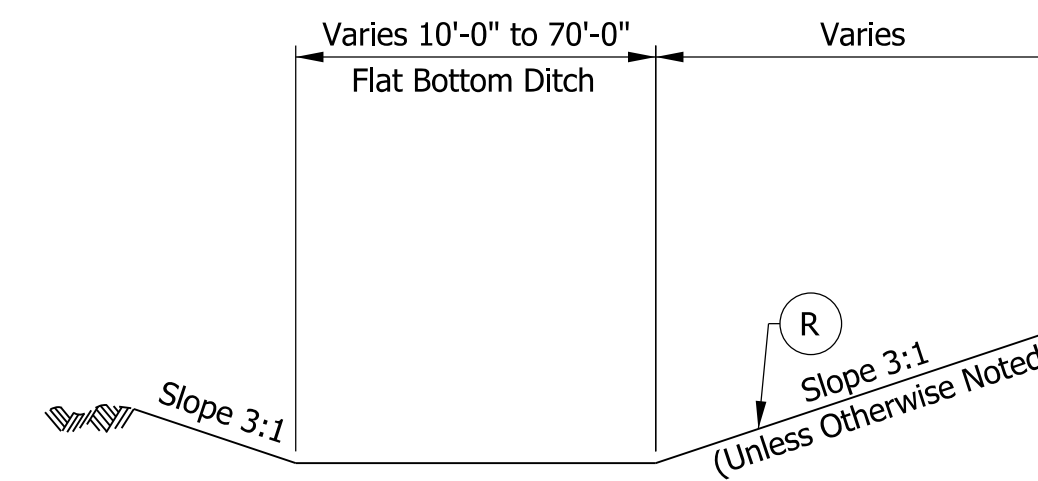
SB GUARDRAIL AND OUTSIDE SHOULDER DETAIL

STA. 878+76.30 "PR-A" to STA. 881+31.48 "PR-A"
 STA. 894+49.57 "PR-A" to STA. 896+40.77 "PR-A"



RAMP GORE SECTION - I-65 SOUTHBOUND AT SR 43

STA. 895+94.65 "PR-A" to STA. 896+87.97 "PR-A"



OUTSIDE WIDE DITCH DETAIL

STA. 871+00.00 "PR-A" to STA. 872+50.00 "PR-A" SB
 STA. 885+50.00 "PR-A" to STA. 891+50.00 "PR-A" SB

FOR INFORMATION ONLY
 SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

- A SB Bridge Paving Exception from STA. 872+72.35 "PR-A" to STA. 878+76.30 "PR-A"
 SB Bridge Paving Exception from STA. 892+70.14 "PR-A" to STA. 894+49.55 "PR-A"
- B See Construction Detail sheets for limits of acceleration and deceleration lanes and taper limits.
- C Width varies from 0'-0" to 12'-0" from STA. 879+14.50 "PR-A" to STA. 886+64.50 "PR-A"
 Width varies from 12'-0" to 16'-0" from STA. 895+00.43 "PR-A" to STA. 895+94.65 "PR-A"
- D Width varies from 12'-0" to 19'-10.5" from STA. 891+38.00 "PR-A" to STA. 892+70.14 "PR-A"
- E Width varies from 18'-0" to 13'-8" from STA. 894+49.57 "PR-A" to STA. 895+00.43 "PR-A"
 Width varies from 14'-10" to 14'-0" from STA. 878+76.30 "PR-A" to STA. 879+33.64 "PR-A"

Notes:

1. See DET-01 for Legend.
 2. Cross slope varies. See Superelevation Detail and Intersection Detail Sheets for cross slope transition locations.
- * Max rollover between shoulder and travel lane not to exceed 8%. See Superelevation Detail for transition information
- ** Slope varies. See Gore Details.

NOTE TO REVIEWER:

Pavement design assumed to be asphalt for estimating purposes. Pavement design to be approved in a future submittal.

NOTE TO REVIEWER:

Safety Edge detail to be provided in a future submittal.

DRAFT
 NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: DCK	DRAWN: DCK	
CHECKED: MKM	CHECKED: MKM	

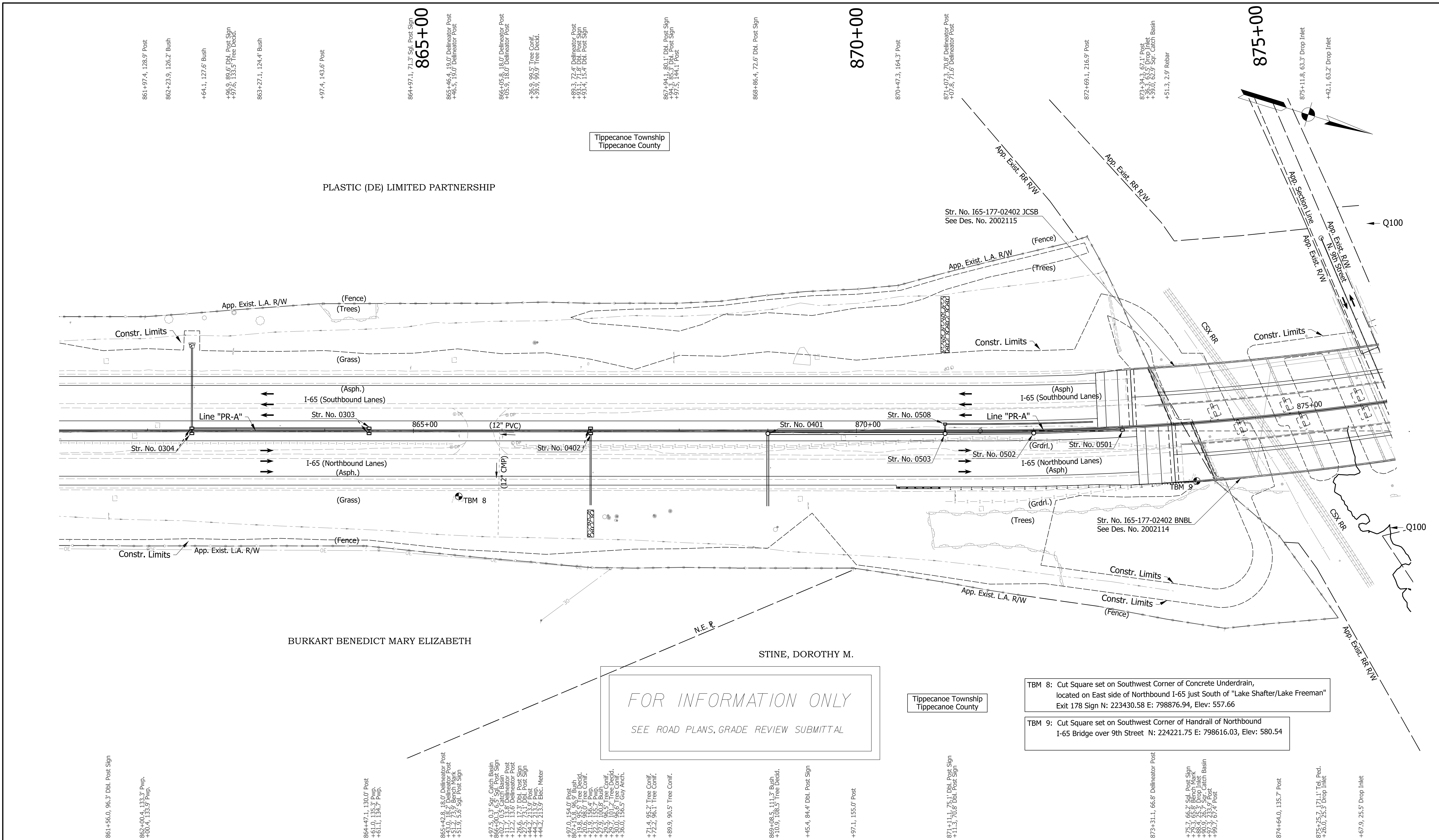
INDIANA
 DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
 I-65 LINE "PR-A"

HORIZONTAL SCALE	BRIDGE FILE	
3/16"=1'-0"	165-177-02402 CNBL & JDSBL	
VERTICAL SCALE	DESIGNATION	
N/A	2002114 & 2002115	
SURVEY BOOK	SHEETS	TS-07
ELECTRONIC	6 of	30
CONTRACT	PROJECT	
R-42909	2001172	

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 file: c:\p01\172\172.dgn



FOR INFORMATION ONLY
 SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

TBM 8: Cut Square set on Southwest Corner of Concrete Underdrain, located on East side of Northbound I-65 just South of "Lake Shafter/Lake Freeman" Exit 178 Sign N: 223430.58 E: 798876.94, Elev: 557.66

TBM 9: Cut Square set on Southwest Corner of Handrail of Northbound I-65 Bridge over 9th Street N: 224221.75 E: 798616.03, Elev: 580.54

- Notes:**
- Existing topography notes taken from Line "PR-A".
 - For geometric information, see Geometric Layout Sheets.
 - For control point information, see Reference Tie Sheets.
 - Line "PR-A" to be constructed.

DRAFT

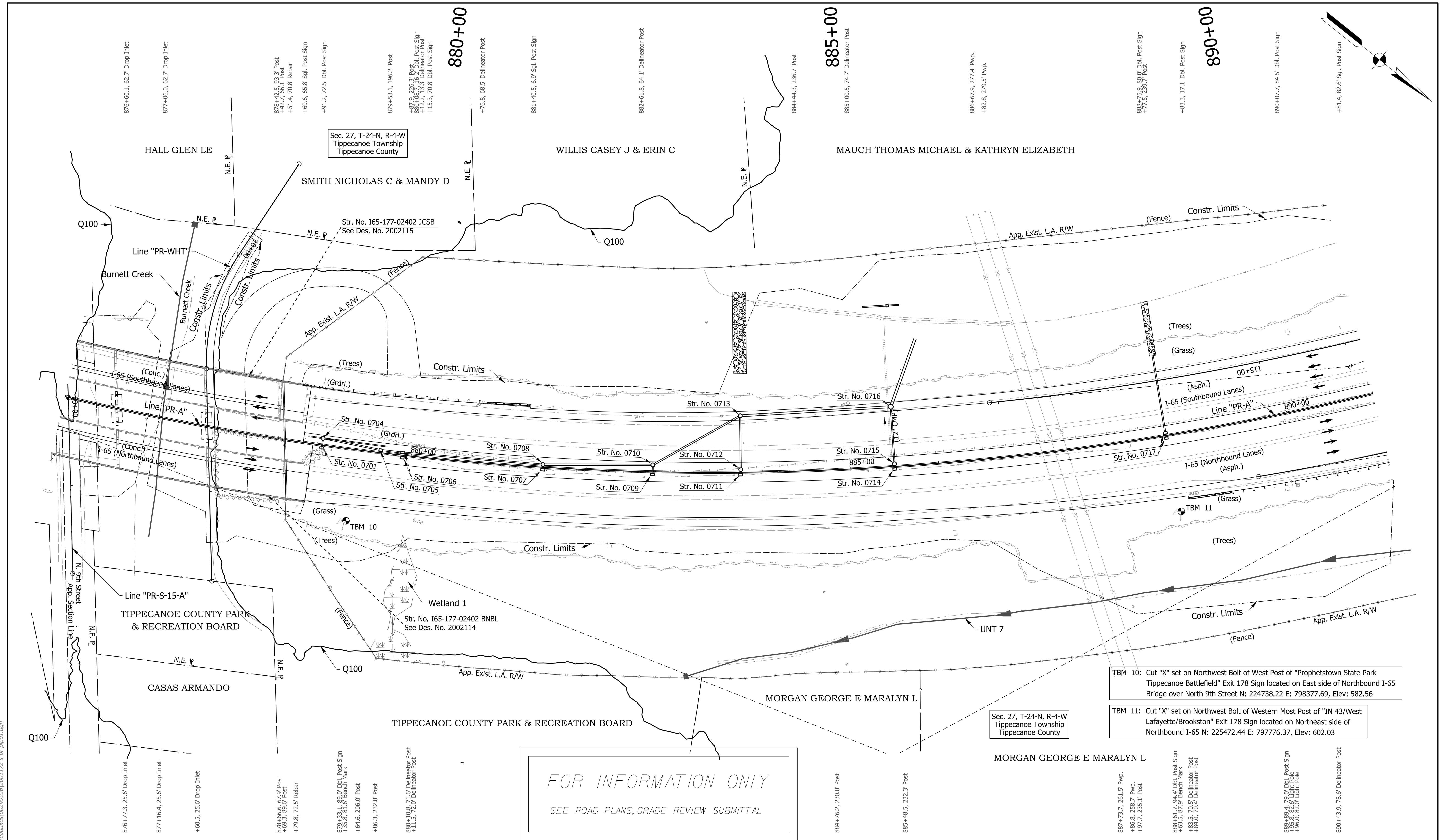
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMA	DRAWN: CEB	
CHECKED: MKM	CHECKED: MKM	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-A
STA. 860+00 TO STA. 876+00

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE 165-177-02402 CNBL & JDSBL
VERTICAL SCALE N/A	DESIGNATION 2002114 & 2002115
SURVEY BOOK ELECTRONIC	SHEETS 9 of 30
CONTRACT R-42909	PROJECT 2001172



FOR INFORMATION ONLY
SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

TBM 10: Cut "X" set on Northwest Bolt of West Post of "Prophetstown State Park Tippecanoe Battlefield" Exit 178 Sign located on East side of Northbound I-65 Bridge over North 9th Street N: 224738.22 E: 798377.69, Elev: 582.56

TBM 11: Cut "X" set on Northwest Bolt of Western Most Post of "IN 43/West Lafayette/Brookston" Exit 178 Sign located on Northeast side of Northbound I-65 N: 225472.44 E: 797776.37, Elev: 602.03

- Notes:**
- Existing topography notes taken from Line "PR-A".
 - For geometric information, see Geometric Layout Sheets.
 - For control point information, see Reference Tie Sheets.
 - Line "PR-A" to be constructed.

DRAFT
NOT FOR CONSTRUCTION

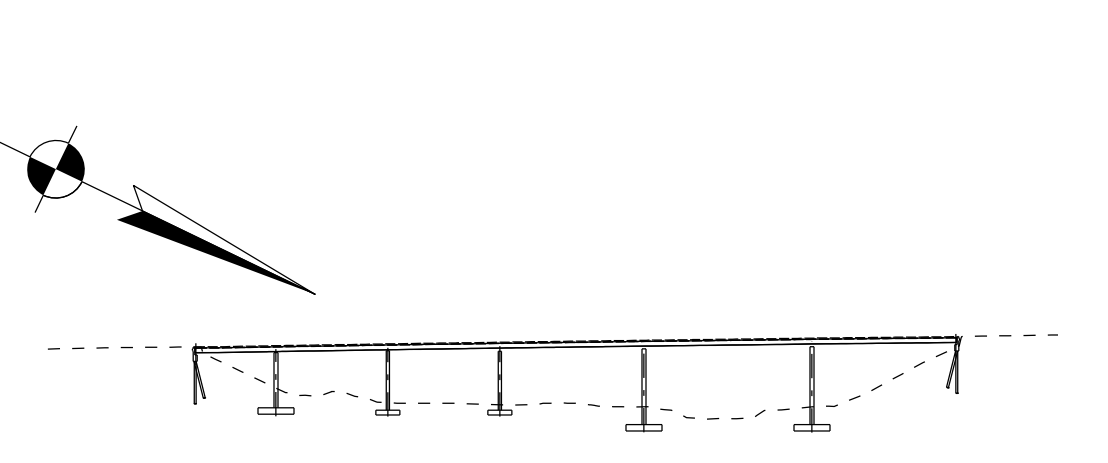
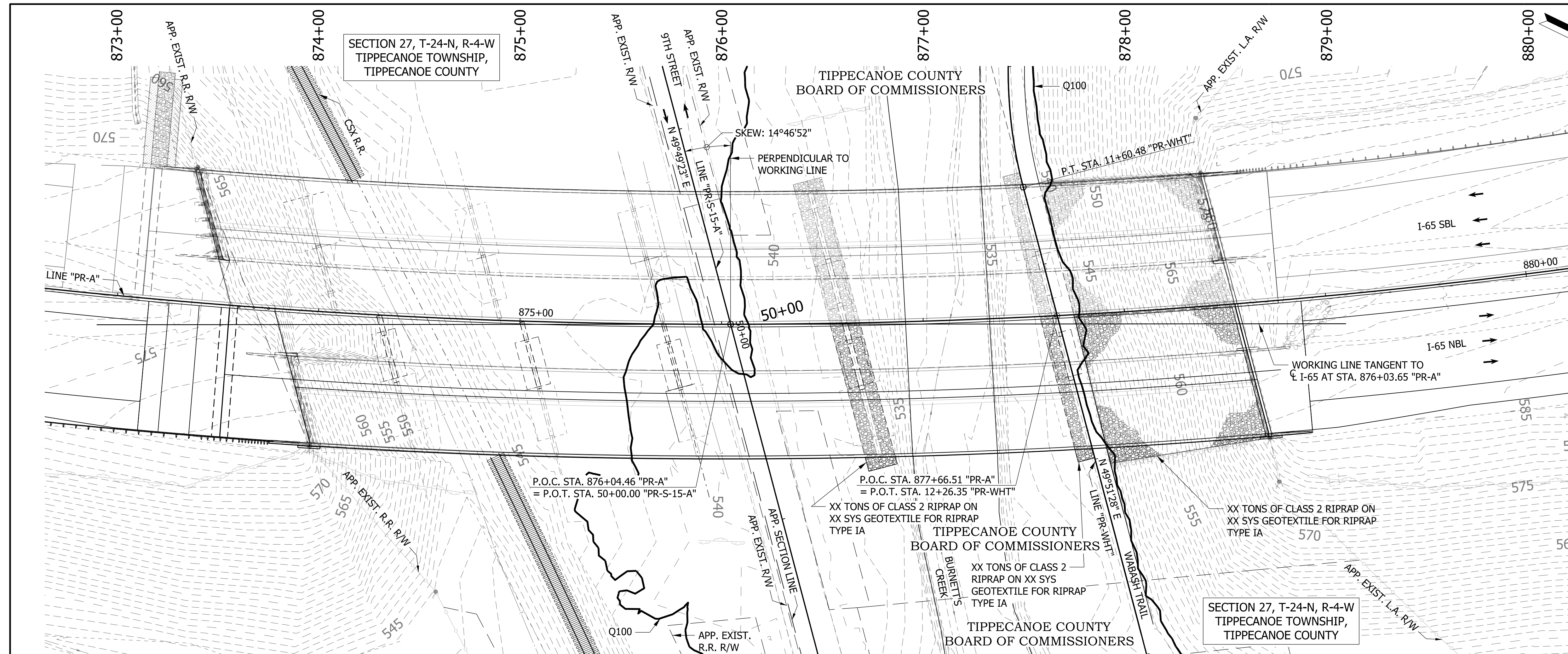
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMA	DRAWN: CEB	
CHECKED: MKM	CHECKED: MKM	

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-A"
STA. 876+00 TO STA. 891+00

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	165-177-02402 CNBL & JDSBL
VERTICAL SCALE	DESIGNATION
N/A	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	11 of 30
CONTRACT	PROJECT
R-42909	2001172

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EXISTING STRUCTURE
 THE EXISTING STRUCTURE IS A 6 SPAN, CONTINUOUS COMPOSITE 36WF STEEL BEAM BRIDGE, SKEWED 14°46'00" LT. TO TANGENT WITH SPANS OF 50'-0", 70'-0", 70'-0", 90'-0", 105'-0" & 90'-0" AND A CLEAR ROADWAY WIDTH OF 40'-8". THE STRUCTURE WAS BUILT IN 1968 WITH A DECK RECONSTRUCTION IN 1993. EXISTING PLANS ARE ON FILE WITH INDIANA DEPARTMENT OF TRANSPORTATION UNDER BRIDGE FILE I-65-180-2402 S, I-65-180-2402 A AND I-65-180-2402 B.

EARTHWORK TABULATION

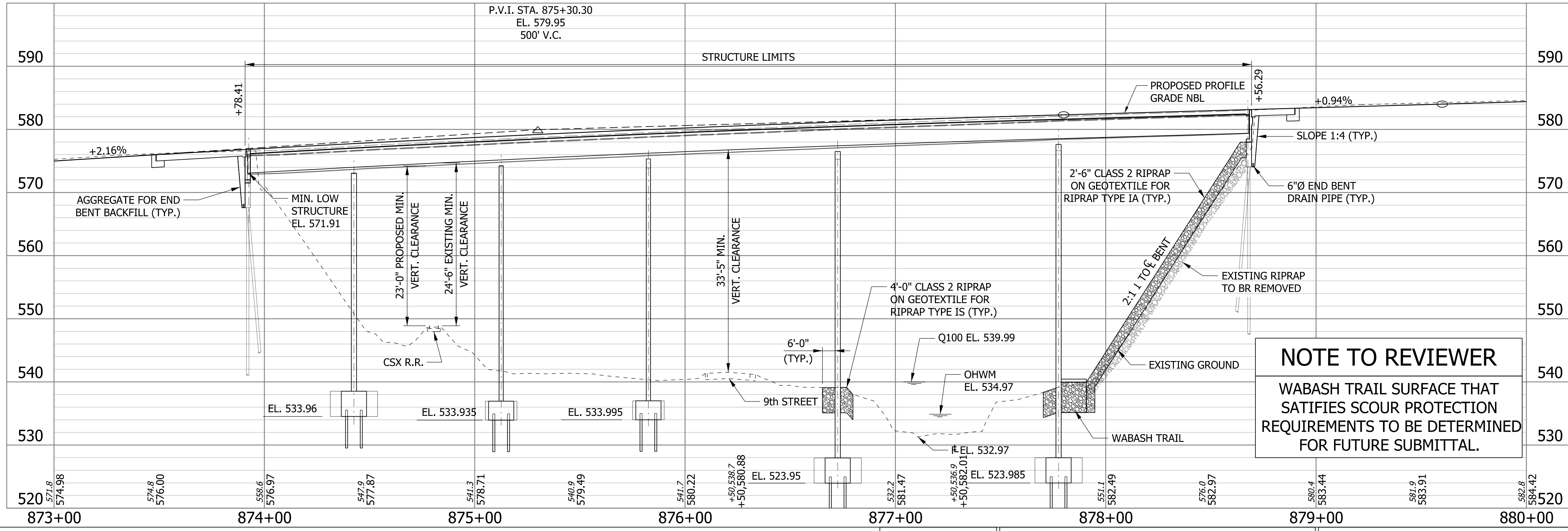
FILL + 25%	XXX CYS
COMMON EXCAVATION	XXX CYS
BORROW	XXX CYS
EXCAVATION, FOUNDATION, UNCLASSIFIED	XXX CYS

HYDRAULIC DATA

WATERWAY OPENING REQUIRED	555.2 SFT
WATERWAY OPENING PROVIDED	555.2 SFT
DRAINAGE AREA	52.0 SQ MI
DESIGN DISCHARGE, Q100	6400 CFS
VELOCITY	12.69 FT/S
Q100 ELEV.	539.99 FT
ESTIMATED SCOUR ELEV.	523.14 FT
EXISTING WATERWAY OPENING	555.2 SFT
MIN. LOW STRUCTURE ELEV. REQUIRED	541.99 FT
EXISTING LOW STRUCTURE ELEV.	571.91 FT
OHWM EL.	534.97 FT

HORIZONTAL CURVE DATA FOR LINE "PR-A"

P.I. = STA. 886+24.35
P.C. = STA. 871+39.60
P.T. = STA. 899+18.85
Δ = 49°55'06" LT.
R = 3190.00'
T = 1484.76'
L = 2779.26'
Dc = 1°47'46"



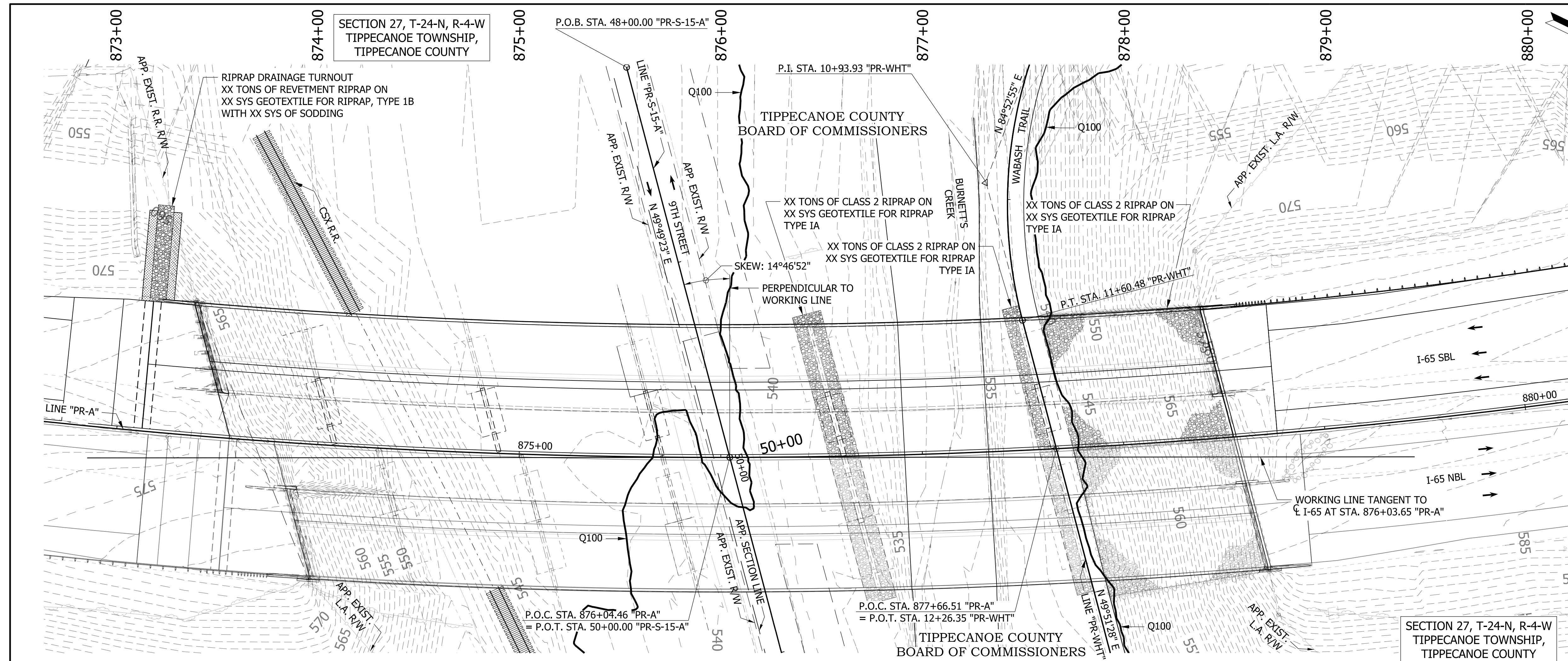
NOTE TO REVIEWER
 QUANTITIES AND EARTHWORK TABULATION TO BE COMPLETED AT FUTURE SUBMITTAL.

NOTE TO REVIEWER
 WABASH TRAIL SURFACE THAT SATISFIES SCOUR PROTECTION REQUIREMENTS TO BE DETERMINED FOR FUTURE SUBMITTAL.

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
 6 SPANS: 50'-0", 70'-0", 70'-0", 90'-0", 105'-0", 90'-0"
 SKEW: 14°46'00" LT. TO TANGENT
 CLEAR ROADWAY 64'-0"
 I-65 NBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK & WABASH TRAIL
 TIPPECANOE COUNTY

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION	
DESIGNED: MEW	DRAWN: CLF				SCALE: 1"=30'	
CHECKED: JJO	CHECKED: JJO				BRIDGE FILE: 165-177-02402 CNBL	
					DESIGNATION: 2002114	
					SURVEY BOOK: SHEETS LY-01	
					ELECTRONIC: 13 of 30	
					CONTRACT: PROJECT	
					R-42909: 2001172	

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EXISTING STRUCTURE
 THE EXISTING STRUCTURE IS A 6 SPAN, CONTINUOUS COMPOSITE 36WF STEEL BEAM BRIDGE, SKEWED 14°46'00" LT. TO TANGENT WITH SPANS OF 50'-0", 80'-6", 80'-6", 90'-0", 105'-0" & 90'-0" AND A CLEAR ROADWAY WIDTH OF 40'-8". THE STRUCTURE WAS BUILT IN 1968 WITH A DECK RECONSTRUCTION IN 1993. EXISTING PLANS ARE ON FILE WITH INDIANA DEPARTMENT OF TRANSPORTATION UNDER BRIDGE FILE I-65-180-2402 JS, I-65-180-2402JA AND I-65-180-2402 JC.

EARTHWORK TABULATION

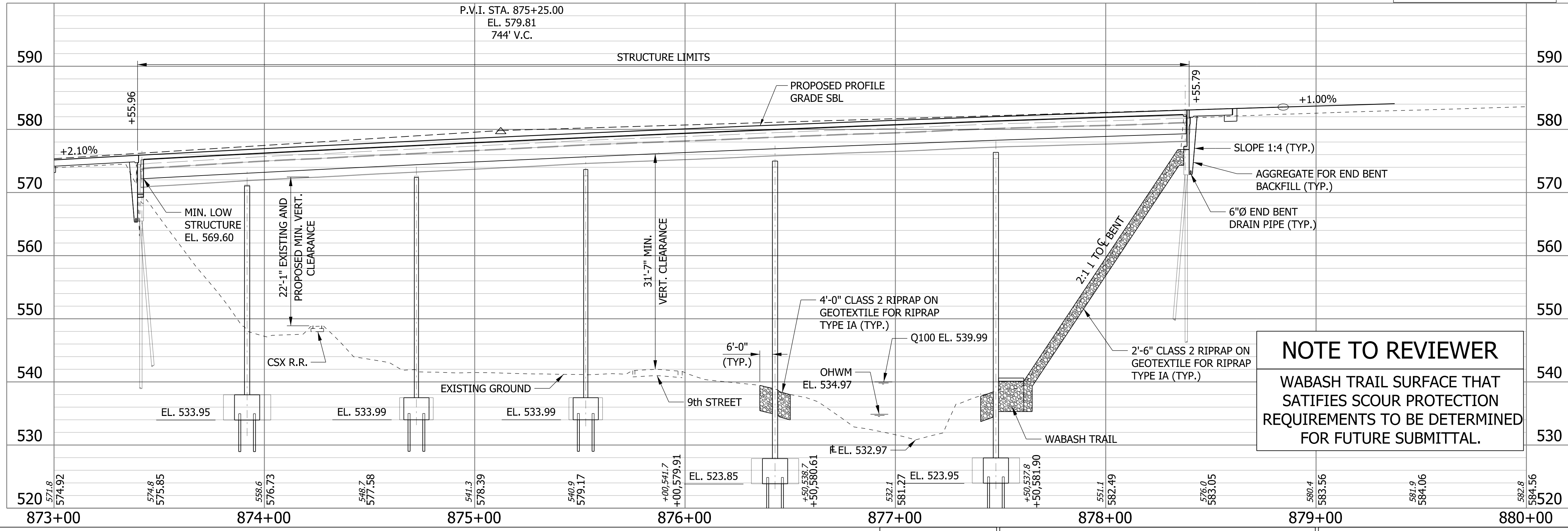
FILL + 25%	XXX CYS
COMMON EXCAVATION	XXX CYS
BORROW	XXX CYS
EXCAVATION, FOUNDATION, UNCLASSIFIED	XXX CYS

HYDRAULIC DATA

WATERWAY OPENING REQUIRED	555.2 SFT
WATERWAY OPENING PROVIDED	555.2 SFT
DRAINAGE AREA	52.0 SQ MI
DESIGN DISCHARGE, Q100	6400 CFS
VELOCITY	12.69 FT/S
Q100 ELEV.	539.99 FT
ESTIMATED SCOUR ELEV.	523.14 FT
EXISTING WATERWAY OPENING	555.2 SFT
MIN. LOW STRUCTURE ELEV. REQUIRED	541.99 FT
EXISTING LOW STRUCTURE ELEV.	569.60 FT
OHWM EL.	534.97 FT

HORIZONTAL CURVE DATA FOR LINE "PR-A"

P.I. = STA. 886+24.35
P.C. = STA. 871+39.60
P.T. = STA. 899+18.85
Δ = 49°55'06" LT.
R = 3190.00'
T = 1484.76'
L = 2779.26'
Dc = 1°47'46"



NOTE TO REVIEWER

QUANTITIES AND EARTHWORK TABULATION TO BE COMPLETED AT FUTURE SUBMITTAL.

NOTE TO REVIEWER

LEVEL 1 DESIGN EXCEPTION PROVIDED FOR VERTICAL CLEARANCE OVER CSX RR.

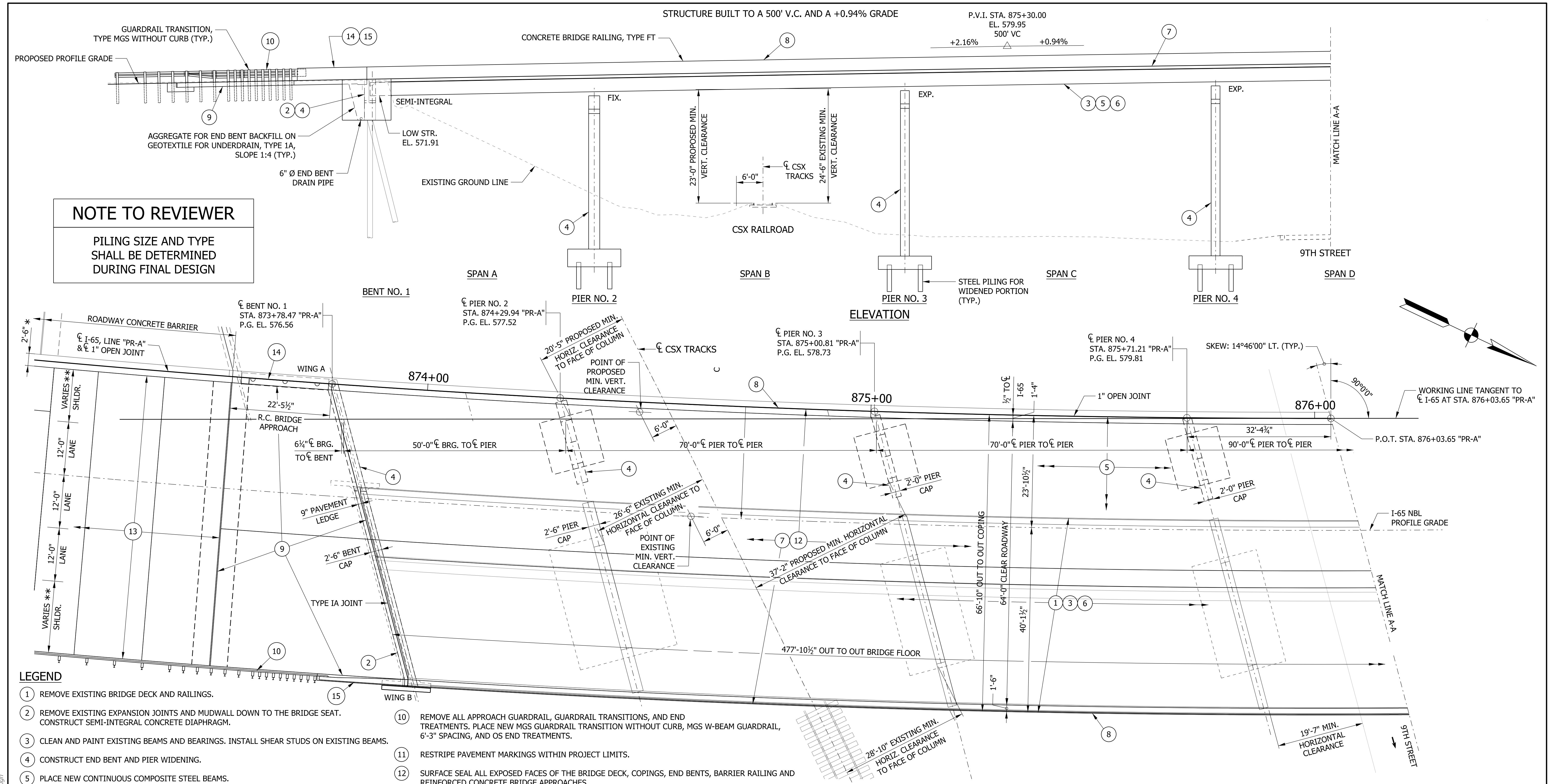
NOTE TO REVIEWER

WABASH TRAIL SURFACE THAT SATISFIES SCOUR PROTECTION REQUIREMENTS TO BE DETERMINED FOR FUTURE SUBMITTAL.

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
 6 SPANS: 50'-0", 80'-6", 80'-6", 90'-0", 105'-0", 90'-0"
 SKEW: 14°46'00" LT. TO TANGENT
 CLEAR ROADWAY 63'-0"
 I-65 SBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK & WABASH TRAIL
 TIPPECANOE COUNTY

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	INDIANA DEPARTMENT OF TRANSPORTATION	
DESIGNED: MEW	DRAWN: CLF				LAYOUT - SBL	
CHECKED: JJO	CHECKED: JJO				SCALE: 1"=30'	
					BRIDGE FILE: I65-177-02402 JDSBL	
					DESIGNATION: 2002115	
					SHEETS: LY-02	
					14 of 30	
					PROJECT: R-42909	
					2001172	

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NOTE TO REVIEWER

PIILING SIZE AND TYPE SHALL BE DETERMINED DURING FINAL DESIGN

LEGEND

- ① REMOVE EXISTING BRIDGE DECK AND RAILINGS.
- ② REMOVE EXISTING EXPANSION JOINTS AND MUDWALL DOWN TO THE BRIDGE SEAT. CONSTRUCT SEMI-INTEGRAL CONCRETE DIAPHRAGM.
- ③ CLEAN AND PAINT EXISTING BEAMS AND BEARINGS. INSTALL SHEAR STUDS ON EXISTING BEAMS.
- ④ CONSTRUCT END BENT AND PIER WIDENING.
- ⑤ PLACE NEW CONTINUOUS COMPOSITE STEEL BEAMS.
- ⑥ REMOVE EXISTING WELDED DIAPHRAGMS AND REPLACE WITH NEW BOLTED DIAPHRAGMS.
- ⑦ CONSTRUCT NEW FULL WIDTH COMPOSITE CONCRETE DECK.
- ⑧ CONSTRUCT NEW TYPE FT CONCRETE BRIDGE RAILING AND INSTALL BARRIER DELINEATORS ON RAILING FACE AT 40'-0" MAX. SPACING.
- ⑨ REMOVE AND REPLACE EXISTING APPROACH SLABS WITH NEW FULL WIDTH APPROACH SLABS. INSTALL TYPE I-A JOINT INTERFACE OF APPROACH SLAB AND CONCRETE BRIDGE DECK.
- ⑩ REMOVE ALL APPROACH GUARDRAIL, GUARDRAIL TRANSITIONS, AND END TREATMENTS. PLACE NEW MGS GUARDRAIL TRANSITION WITHOUT CURB, MGS W-BEAM GUARDRAIL, 6'-3" SPACING, AND OS END TREATMENTS.
- ⑪ RESTRIPE PAVEMENT MARKINGS WITHIN PROJECT LIMITS.
- ⑫ SURFACE SEAL ALL EXPOSED FACES OF THE BRIDGE DECK, COPINGS, END BENTS, BARRIER RAILING AND REINFORCED CONCRETE BRIDGE APPROACHES.
- ⑬ INSTALL NEW TERMINAL JOINT, TYPE PCCP.
- ⑭ CONSTRUCT NEW CONCRETE BRIDGE RAILING, TYPE FT ON APPROACH SLAB EXTENSIONS.
- ⑮ CONSTRUCT NEW CONCRETE BRIDGE RAILING TRANSITION, TYPE TFT ON APPROACH SLAB EXTENSIONS.
- ⑯ REMOVE EXISTING EXPANSION JOINTS AND TOP OF MUDWALL DOWN TO THE PAVEMENT LEDGE. INSTALL NEW EXPANSION JOINT, CLASS M.
- * ROADWAY MEDIAN BARRIER TRANSITIONS TO BARRIER WIDTH ON BRIDGE.
- ** SHOULDER WIDTH VARIES FROM 12'-0" TYPICAL TO MATCH SHOULDER WIDTH ON BRIDGE.

NOTES:

1. FOR CSX RAILROAD NOTES, GENERAL NOTES AND DESIGN DATA, SEE SHEET GP-07.

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
 6 SPANS: 50'-0", 70'-0", 70'-0", 90'-0", 105'-0", 90'-0"
 SKEW: 14°46'00" LT. TO TANGENT
 CLEAR ROADWAY 64'-0"
 I-65 NBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK
 & WABASH TRAIL
 TIPPECANOE COUNTY

DRAFT
 NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MEW	DRAWN: CLF	
CHECKED: JJO	CHECKED: JJO	

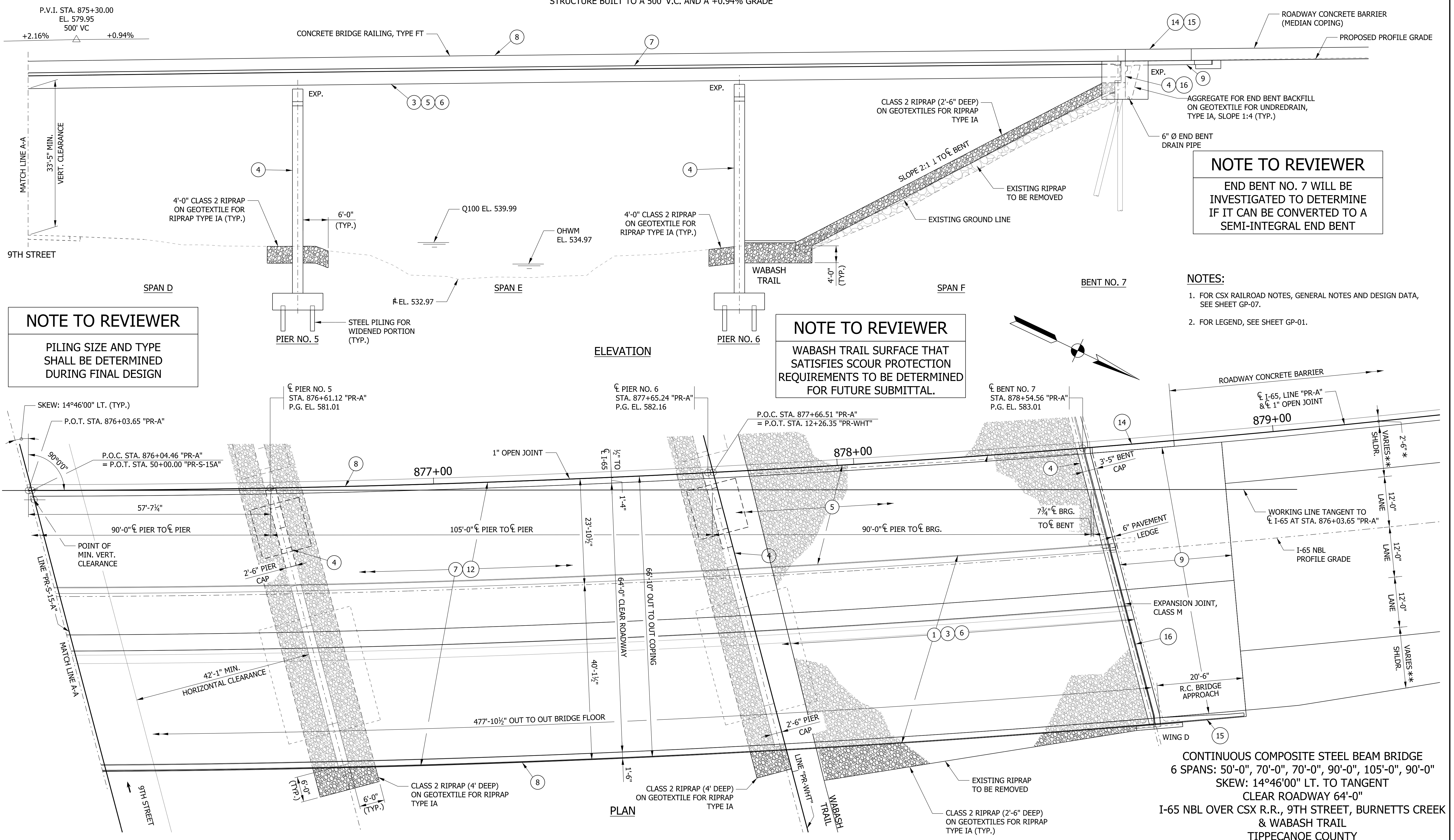
INDIANA
 DEPARTMENT OF TRANSPORTATION

GENERAL PLAN - NBL

SCALE	BRIDGE FILE
3/2" = 1'-0"	165-177-02402 CNBL & JDSBL
VERTICAL SCALE	DESIGNATION
3/2" = 1'-0"	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	15 of 30
CONTRACT	PROJECT
R-42909	2001172

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STRUCTURE BUILT TO A 500' V.C. AND A +0.94% GRADE



NOTE TO REVIEWER
 END BENT NO. 7 WILL BE INVESTIGATED TO DETERMINE IF IT CAN BE CONVERTED TO A SEMI-INTEGRAL END BENT

NOTE TO REVIEWER
 PILING SIZE AND TYPE SHALL BE DETERMINED DURING FINAL DESIGN

NOTE TO REVIEWER
 WABASH TRAIL SURFACE THAT SATISFIES SCOUR PROTECTION REQUIREMENTS TO BE DETERMINED FOR FUTURE SUBMITTAL.

- NOTES:**
- FOR CSX RAILROAD NOTES, GENERAL NOTES AND DESIGN DATA, SEE SHEET GP-07.
 - FOR LEGEND, SEE SHEET GP-01.

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
 6 SPANS: 50'-0", 70'-0", 70'-0", 90'-0", 105'-0", 90'-0"
 SKEW: 14°46'00" LT. TO TANGENT
 CLEAR ROADWAY 64'-0"
 I-65 NBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK & WABASH TRAIL
 TIPPECANOE COUNTY

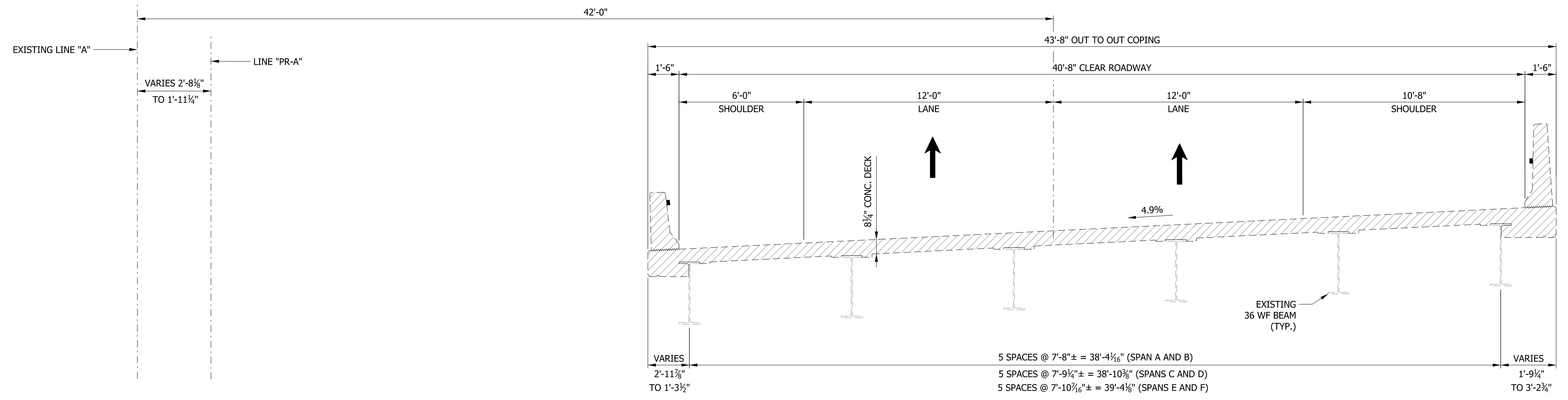
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MEW	DRAWN: CLF	
CHECKED: JJO	CHECKED: JJO	

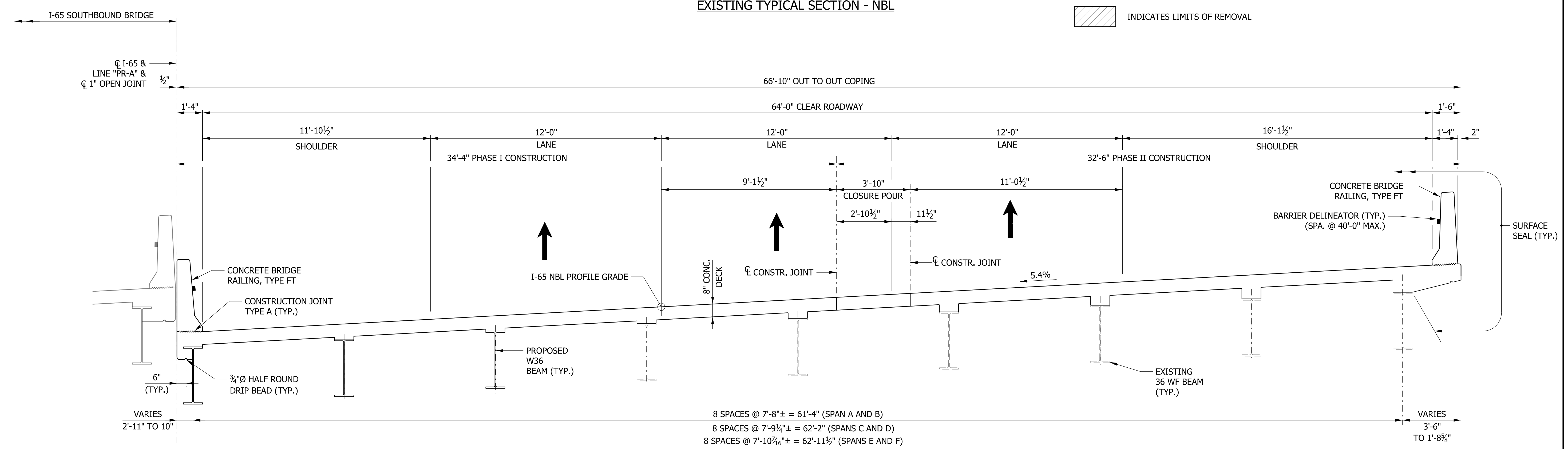
INDIANA
 DEPARTMENT OF TRANSPORTATION
 GENERAL PLAN - NBL

SCALE	BRIDGE FILE
3/8" = 1'-0"	165-177-02402 CNBL & JDSBL
VERTICAL SCALE	DESIGNATION
3/8" = 1'-0"	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	16 of 30
CONTRACT	PROJECT
R-42909	2001172



EXISTING TYPICAL SECTION - NBL

INDICATES LIMITS OF REMOVAL



PROPOSED TYPICAL SECTION - NBL

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
 6 SPANS: 50'-0", 70'-0", 70'-0", 90'-0", 105'-0", 90'-0"
 SKEW: 14°46'00" LT. TO TANGENT
 CLEAR ROADWAY 64'-0"
 I-65 NBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK
 & WABASH TRAIL
 TIPPECANOE COUNTY

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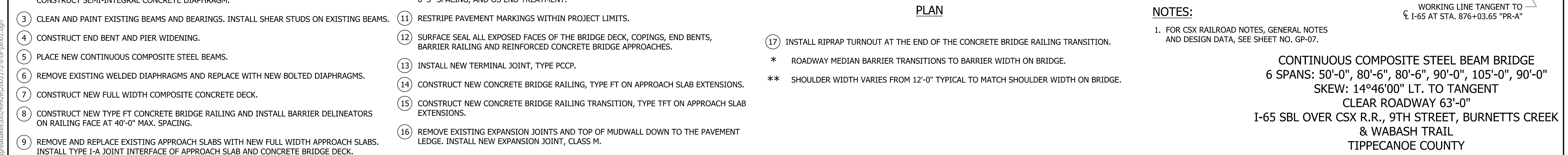
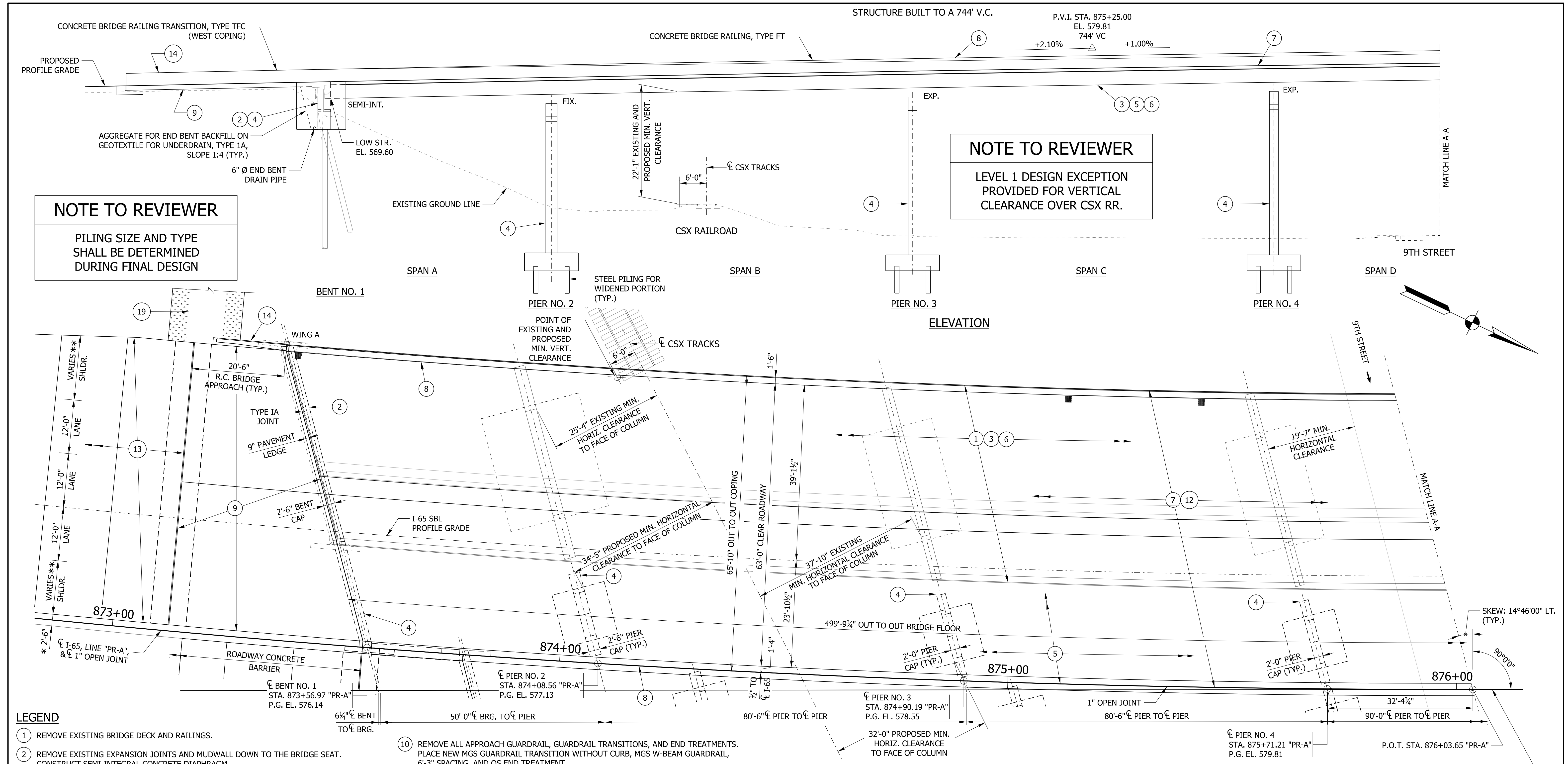
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MEW	DRAWN: CLF	
CHECKED: JJO	CHECKED: JJO	

INDIANA
 DEPARTMENT OF TRANSPORTATION

GENERAL PLAN - NBL

SCALE	BRIDGE FILE
3/8" = 1'-0"	165-177-02402 CNBL & JDSBL
VERTICAL SCALE	DESIGNATION
3/8" = 1'-0"	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	17 of 30
CONTRACT	PROJECT
R-42909	2001172



LEGEND

- | | | |
|---|---|--|
| <p>① REMOVE EXISTING BRIDGE DECK AND RAILINGS.</p> <p>② REMOVE EXISTING EXPANSION JOINTS AND MUDWALL DOWN TO THE BRIDGE SEAT. CONSTRUCT SEMI-INTEGRAL CONCRETE DIAPHRAGM.</p> <p>③ CLEAN AND PAINT EXISTING BEAMS AND BEARINGS. INSTALL SHEAR STUDS ON EXISTING BEAMS.</p> <p>④ CONSTRUCT END BENT AND PIER WIDENING.</p> <p>⑤ PLACE NEW CONTINUOUS COMPOSITE STEEL BEAMS.</p> <p>⑥ REMOVE EXISTING WELDED DIAPHRAGMS AND REPLACE WITH NEW BOLTED DIAPHRAGMS.</p> <p>⑦ CONSTRUCT NEW FULL WIDTH COMPOSITE CONCRETE DECK.</p> <p>⑧ CONSTRUCT NEW TYPE FT CONCRETE BRIDGE RAILING AND INSTALL BARRIER DELINEATORS ON RAILING FACE AT 40'-0" MAX. SPACING.</p> <p>⑨ REMOVE AND REPLACE EXISTING APPROACH SLABS WITH NEW FULL WIDTH APPROACH SLABS. INSTALL TYPE I-A JOINT INTERFACE OF APPROACH SLAB AND CONCRETE BRIDGE DECK.</p> | <p>⑩ REMOVE ALL APPROACH GUARDRAIL, GUARDRAIL TRANSITIONS, AND END TREATMENTS. PLACE NEW MGS GUARDRAIL TRANSITION WITHOUT CURB, MGS W-BEAM GUARDRAIL, 6'-3" SPACING, AND OS END TREATMENT.</p> <p>⑪ RESTRIPE PAVEMENT MARKINGS WITHIN PROJECT LIMITS.</p> <p>⑫ SURFACE SEAL ALL EXPOSED FACES OF THE BRIDGE DECK, COPINGS, END BENTS, BARRIER RAILING AND REINFORCED CONCRETE BRIDGE APPROACHES.</p> <p>⑬ INSTALL NEW TERMINAL JOINT, TYPE PCCP.</p> <p>⑭ CONSTRUCT NEW CONCRETE BRIDGE RAILING, TYPE FT ON APPROACH SLAB EXTENSIONS.</p> <p>⑮ CONSTRUCT NEW CONCRETE BRIDGE RAILING TRANSITION, TYPE TFT ON APPROACH SLAB EXTENSIONS.</p> <p>⑯ REMOVE EXISTING EXPANSION JOINTS AND TOP OF MUDWALL DOWN TO THE PAVEMENT LEDGE. INSTALL NEW EXPANSION JOINT, CLASS M.</p> | <p>⑰ INSTALL RIPRAP TURNOUT AT THE END OF THE CONCRETE BRIDGE RAILING TRANSITION.</p> <p>* ROADWAY MEDIAN BARRIER TRANSITIONS TO BARRIER WIDTH ON BRIDGE.</p> <p>** SHOULDER WIDTH VARIES FROM 12'-0" TYPICAL TO MATCH SHOULDER WIDTH ON BRIDGE.</p> |
|---|---|--|

NOTES:

1. FOR CSX RAILROAD NOTES, GENERAL NOTES AND DESIGN DATA, SEE SHEET NO. GP-07.

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
 6 SPANS: 50'-0", 80'-6", 80'-6", 90'-0", 105'-0", 90'-0"
 SKEW: 14°46'00" LT. TO TANGENT
 CLEAR ROADWAY 63'-0"
 I-65 SBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK
 & WABASH TRAIL
 TIPPECANOE COUNTY

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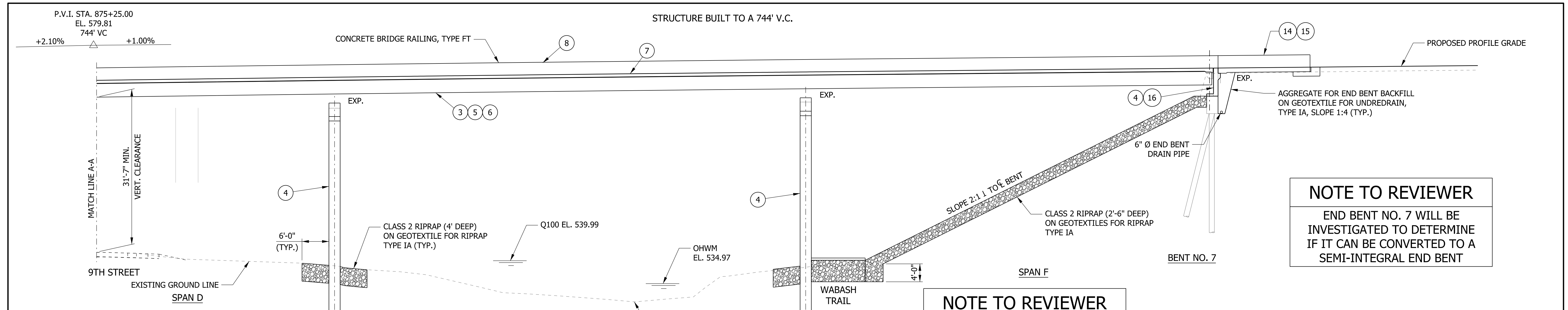
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MEW	DRAWN: CLF	
CHECKED: JJO	CHECKED: JJO	

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN - SBL

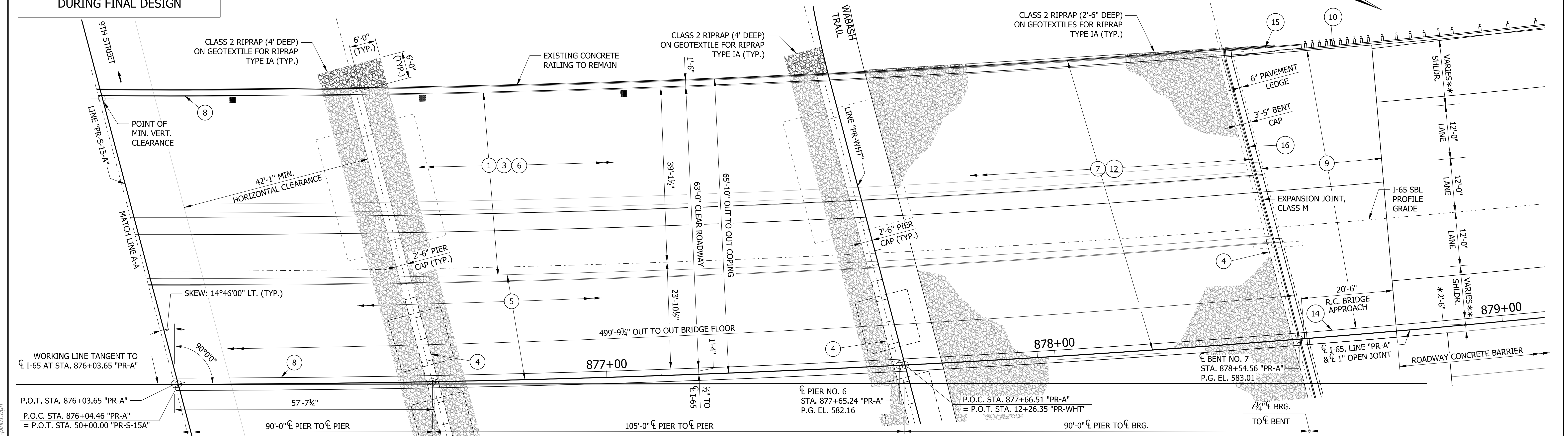
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3/2" = 1'-0"	165-177-02402 CNBL & JDSBL
VERTICAL SCALE	DESIGNATION
3/2" = 1'-0"	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	18 of 30
CONTRACT	PROJECT
R-42909	2001172



NOTE TO REVIEWER
 PILING SIZE AND TYPE SHALL BE DETERMINED DURING FINAL DESIGN

NOTE TO REVIEWER
 WABASH TRAIL SURFACE THAT SATISFIES SCOUR PROTECTION REQUIREMENTS TO BE DETERMINED FOR FUTURE SUBMITTAL.

NOTE TO REVIEWER
 END BENT NO. 7 WILL BE INVESTIGATED TO DETERMINE IF IT CAN BE CONVERTED TO A SEMI-INTEGRAL END BENT



PLAN

- NOTES:**
- FOR CSX RAILROAD NOTES, GENERAL NOTES AND DESIGN DATA, SEE SHEET GP-07.
 - FOR LEGEND, SEE SHEET GP-04.

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
 6 SPANS: 50'-0", 80'-6", 80'-6", 90'-0", 105'-0", 90'-0"
 SKEW: 14°46'00" LT. TO TANGENT
 CLEAR ROADWAY 63'-0"
 I-65 SBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK & WABASH TRAIL
 TIPPECANOE COUNTY

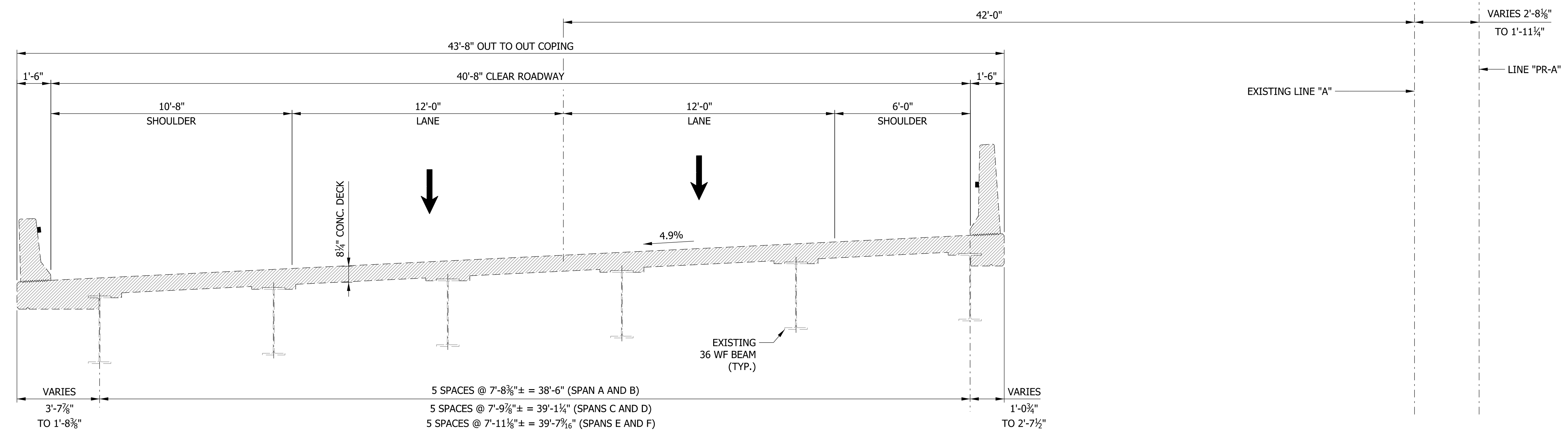
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MEW	DRAWN: CLF	
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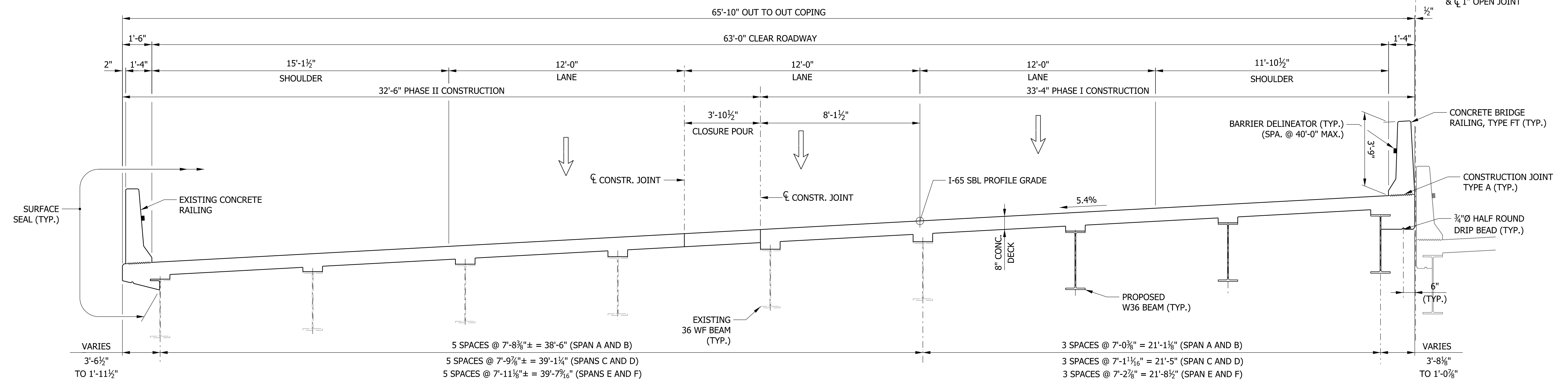
INDIANA DEPARTMENT OF TRANSPORTATION
 GENERAL PLAN - SBL

SCALE	BRIDGE FILE
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VERTICAL SCALE	DESIGNATION
1/2" = 1'-0"	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	19 of 30
CONTRACT	PROJECT
R-42909	2001172



EXISTING TYPICAL SECTION - SBL

INDICATES LIMITS OF REMOVAL



PROPOSED TYPICAL SECTION - SBL

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
 6 SPANS: 50'-0", 80'-6", 80'-6", 90'-0", 105'-0", 90'-0"
 SKEW: 14°46'00" LT. TO TANGENT
 CLEAR ROADWAY 63'-0"
 I-65 SBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK
 & WABASH TRAIL
 TIPPECANOE COUNTY

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RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ MEW _____	DRAWN: _____ CLF _____	
CHECKED: _____ JJO _____	CHECKED: _____ JJO _____	

INDIANA
 DEPARTMENT OF TRANSPORTATION
 GENERAL PLAN - SBL

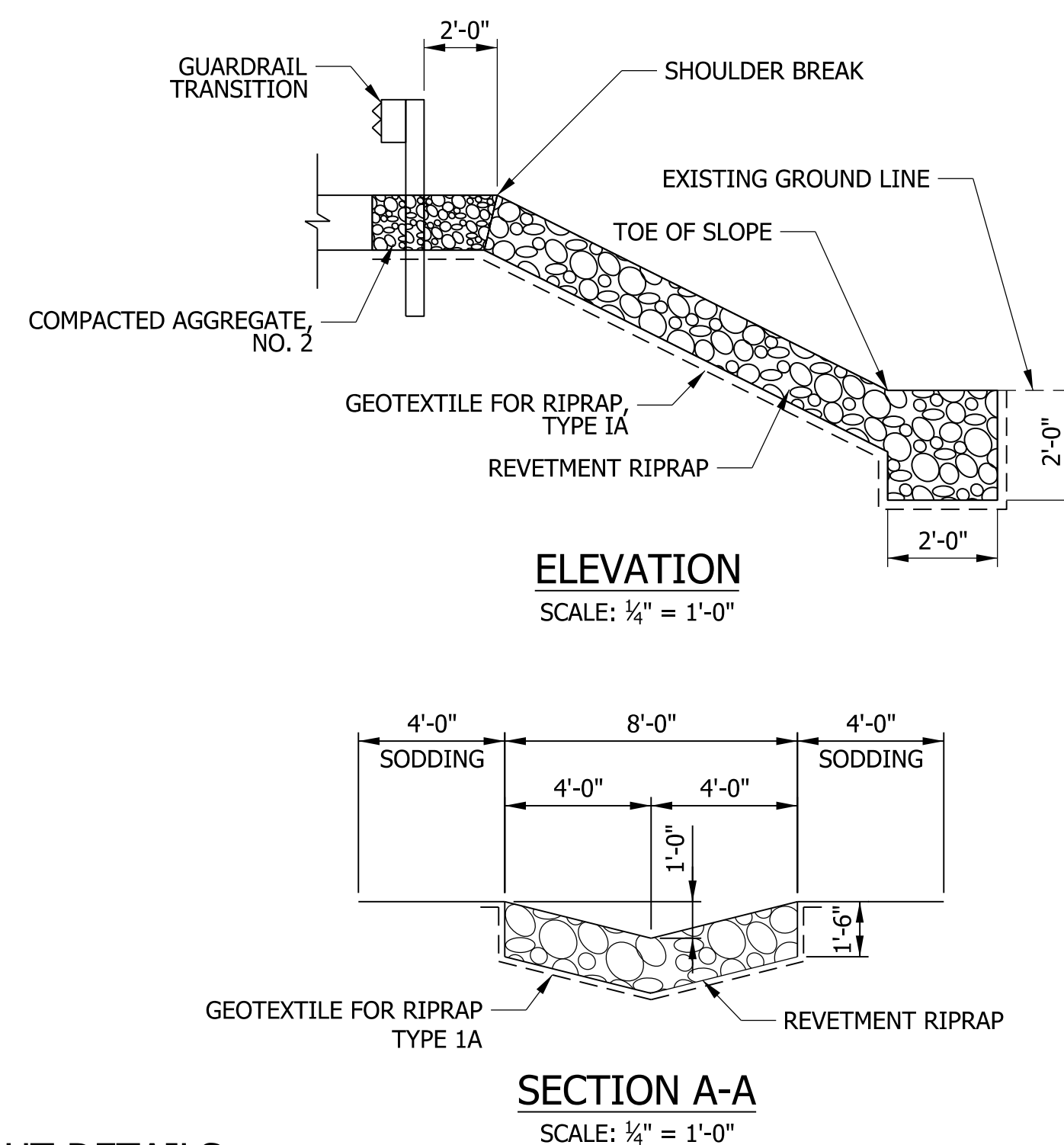
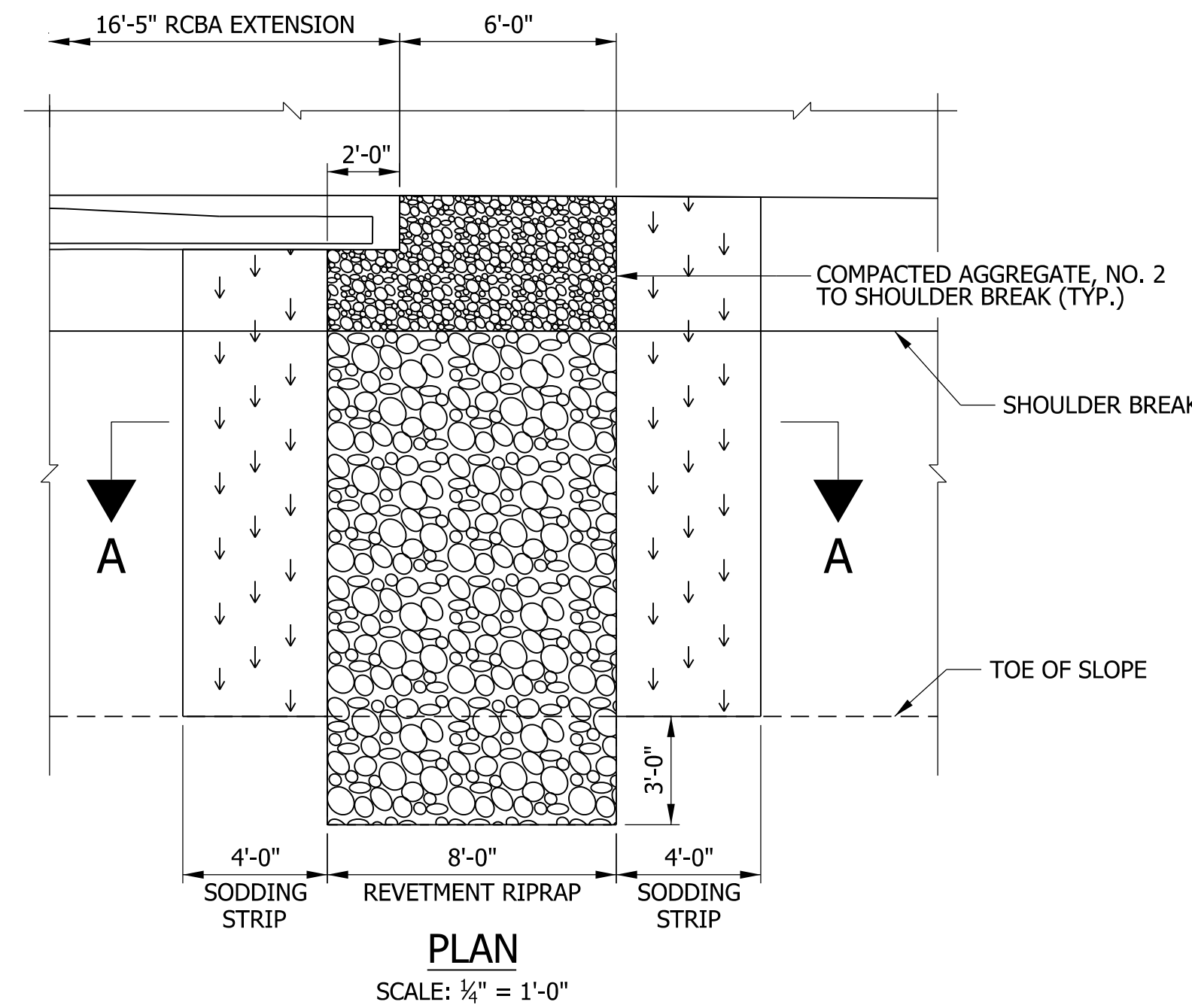
SCALE 3/8" = 1'-0"	BRIDGE FILE 165-177-02402 CNBL & JDSBL
VERTICAL SCALE 3/8" = 1'-0"	DESIGNATION 2002114 & 2002115
SURVEY BOOK ELECTRONIC	SHEETS 20 of 30
CONTRACT R-42909	PROJECT 2001172

NOTES FOR CSX RAILROAD:

- PLANS AND SPECIFICATIONS SHALL BE IN ACCORDANCE WITH THE CURRENT CSX TRANSPORTATION CRITERIA FOR OVERHEAD BRIDGES, DATE ISSUED JULY 2017.
- CSX VALUATION STATION AND DISTANCE FROM THE NEAREST MILEPOST AT INTERSECTION OF CENTERLINE TRACK AND CENTERLINE BRIDGE SHALL BE FURNISHED IN FUTURE SUBMITTAL.
- PROTECTIVE FENCING WILL NOT BE INSTALLED SINCE THIS IS A NON-PEDESTRIAN, LIMITED ACCESS FREEWAY.
- ALL HIGHWAY AND BRIDGE DRAINAGE SHALL BE DIRECTED AWAY FROM THE RAILROAD RIGHT-OF-WAY.
- IF TEMPORARY SHORING IS REQUIRED FOR THE PROPOSED PIERS, CONSTRUCTION AND EXCAVATION SHALL BE IN ACCORDANCE WITH INDOT AND CSX SPECIFICATION REQUIREMENTS.
- AFTER CONSTRUCTION IS COMPLETE, EXISTING DITCHES SHALL BE RESTORED TO ORIGINAL CONDITION.
- PROPOSED METHOD OF EROSION CONTROL SHALL BE PROVIDED IN A FUTURE SUBMITTAL.
- ALL WASTE MATERIALS GENERATED BY THIS PROJECT, INCLUDING WASHING WITH WATER, CLEANING SOLVENTS, BLASTING, SCRAPING, BRUSHING, AND PAINTING OPERATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE CONTAINED, COLLECTED, AND PROPERLY DISPOSED OF BY THE CONTRACTOR. THE CONTRACTOR SHALL AGREE TO FULLY COMPLY WITH ALL FEDERAL, STATE, AND LOCAL ENVIRONMENTAL LAWS, REGULATIONS, STATUTES, AND ORDINANCES AT ALL TIMES.
- THE PROPOSED PIER IS LOCATED IN INDOT AND CSX RAILROAD RIGHT OF WAY THEREFORE REQUIRING AN EASEMENT SIMILAR TO THE EXISTING PIER. HORIZONTAL CLEARANCE REQUIREMENTS FOR CSX RAILROAD CONSTRAIN THE PROPOSED PIER TO THE SAME LOCATION AS EXISTING. A LEGAL DESCRIPTION AND EASEMENT REQUEST FROM INDOT IS FORTHCOMING.
- REFER TO THE CSX TRANSPORTATION PUBLIC PROJECT INFORMATION MANUAL FOR ADDITIONAL REQUIREMENTS NEEDED FOR WORKING ON/ABOVE/ADJACENT TO CSX. SPECIFIC SECTIONS THAT PERTAIN TO THIS PROJECT ARE: SPECIAL PROVISIONS FOR CONSTRUCTION NEAR CSXT PROPERTY, OVERHEAD BRIDGE CRITERIA, CONSTRUCTION SUBMISSION CRITERIA, SOIL AND WATER MANAGEMENT POLICY, AND INSURANCE REQUIREMENTS FOR PUBLIC PROJECTS.
- UPON COMPLETION OF CONSTRUCTION, A FULL SET OF ELECTRONIC AS-BUILT DRAWINGS, SHOWING ACTUAL MEASURED VERTICAL AND HORIZONTAL CLEARANCES, SHALL BE FURNISHED TO CSX.
- CONTRACTOR ACCESS WILL BE LIMITED TO THE IMMEDIATE PROJECT AREA ONLY. THE CSX RIGHT-OF-WAY OUTSIDE THE PROJECT AREA MAY NOT BE USED FOR CONTRACTOR ACCESS TO THE PROJECT SITE AND NO TEMPORARY AT-GRADE CROSSINGS WILL BE ALLOWED.
- CSX MAY REQUIRE THE CONTRACTOR TO INSTALL FILTER FABRIC OVER THE TRACK(S) AND BALLAST TO PREVENT ANY CONSTRUCTION DEBRIS FROM FOULING THE BALLAST. THIS WILL BE DETERMINED DURING ACTUAL CONSTRUCTION ACTIVITIES BY CSX OR ITS REPRESENTATIVE. FABRIC WILL REMAIN IN PLACE UNTIL ALL CONSTRUCTION ACTIVITIES ARE COMPLETE.
- IF CONTRACTOR HAS THE POTENTIAL TO PENETRATE THE DECK DURING THE DECK REHABILITATION WORK, THEN CONTRACTOR WILL BE REQUIRED TO INSTALL FALSEWORK/DEMO SHIELD PROTECTION DIRECTLY OVER THE CSX RAILROAD SPAN. THE FALSEWORK/DEMO SHIELD PROTECTION WILL BE INSTALLED PRIOR TO THE DECK BEING PENETRATED AND WILL STAY IN PLACE FOR THE DURATION OF THE CONSTRUCTION ACTIVITIES. THE FALSEWORK/DEMO SHIELD SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE TO CSX'S CONSTRUCTION SUBMISSION CRITERIA.
- THE CONTRACTOR MAY NOT USE CSX RIGHT-OF-WAY FOR STORAGE OF MATERIALS OR EQUIPMENT DURING CONSTRUCTION WITHOUT PRIOR CSX APPROVAL. THE CSX RIGHT-OF-WAY MUST REMAIN CLEAR FOR RAILROAD USE AT ALL TIMES. EQUIPMENT MAY NOT BE POSITIONED TO BLOCK THE RAILROAD ACCESS ROAD, TRACK AREA OR ANY PART OF THE CSX RIGHT-OF-WAY WITHOUT PRIOR CSX APPROVAL.
- TEMPORARY CONSTRUCTION CLEARANCE - ENSURE ALL FALSEWORK, BRACING OR FORMS HAVE A MINIMUM HORIZONTAL CLEARANCE OF 12 FEET MEASURED PERPENDICULAR TO THE CENTERLINE OF THE NEAREST TRACK, AND A MINIMUM VERTICAL CLEARANCE OF 22 FEET AS MEASURED FROM THE TOP OF RAIL PROFILE.
- EXISTING TRACK DITCHES MUST BE MAINTAINED AT ALL TIMES THROUGHOUT THE CONSTRUCTION PERIOD. AFTER THE CONSTRUCTION HAS BEEN COMPLETED, ALL EROSION AND SILTATION MUST BE REMOVED AND THE DITCHES MUST BE RESTORED.
- THE CONTRACTOR WILL BE REQUIRED TO ABIDE BY THE PROVISIONS OF THE AGENCY/CSX CONSTRUCTION AGREEMENT. PERIODICALLY, THROUGHOUT THE PROJECT DURATION, THE CONTRACTOR WILL BE REQUIRED TO MEET, DISCUSS AND, IF NECESSARY, TAKE IMMEDIATE ACTION AT THE DISCRETION OF CSX PERSONNEL AND/OR THEIR AUTHORIZED REPRESENTATIVE, TO COMPLY WITH PROVISIONS OF THAT AGREEMENT AND THESE SPECIFICATIONS.
- CSX SHALL BE NOTIFIED AT LEAST 5 DAYS IN ADVANCE OF THE PRE-CONSTRUCTION MEETING.
- UPON COMPLETION OF THE WORK ON CSXT PROPERTY, THE CONTRACTOR SHALL REQUEST THE OWNER TO ARRANGE A FINAL INSPECTION OF THE PROJECT WITH THE RAILROAD'S PROJECT ENGINEER OR HIS AUTHORIZED REPRESENTATIVE.
- CSX HAS SOLE AUTHORITY TO DETERMINE THE NEED FOR TRACK PROTECTION REQUIRED TO PROTECT ITS OPERATIONS AND PROPERTY. IN GENERAL, TRACK PROTECTION WILL BE REQUIRED WHENEVER CONTRACTOR OR EQUIPMENT ARE, OR ARE LIKELY TO BE, WORKING WITHIN FIFTY FEET OF TRACK OR OTHER TRACK CLEARANCES AS SPECIFIED BY CSX.

NOTE TO REVIEWER

VALUES AND SEISMIC DATA TO BE POPULATED FOR FUTURE SUBMISSION



RIPRAP TURNOUT DETAILS

GENERAL NOTES

WHERE NEW WORK IS TO BE FITTED TO OLD WORK, THE CONTRACTOR SHALL CHECK ALL DIMENSIONS AND CONDITIONS IN THE FIELD AND REPORT ALL ERRORS OR DISCREPANCIES TO THE ENGINEER AND ASSUME RESPONSIBILITY FOR THEIR CORRECTNESS AND FIT OF THE NEW PART TO THE OLD.

REINFORCING BAR COVER SHALL BE 2" UNLESS OTHERWISE NOTED.

EPOXY RESIN ADHESIVE SHALL BE USED WHERE NEW CONCRETE ABUTS EXISTING CONCRETE.

CLEAN AND SURFACE SEAL ALL EXPOSED FACES OF THE CONCRETE RAILING, COPING, TRANSITIONS, TOP OF DECK, END BENTS AND WINGWALLS, PIER, AND APPROACH SLABS.

ALL PAVEMENT MARKINGS AND SNOWPLOWABLE RAISED PAVEMENT MARKERS SHALL BE REPLACED IN KIND.

XXX TONS OF NO. 2 STONE AND XXX SYS OF GEOTEXTILES ARE PROVIDED FOR CONSTRUCTION ENTRANCES ONTO THE PROJECT SITE.

ALL DIMENSIONS AND ELEVATIONS ARE IN FEET (FT) UNLESS OTHERWISE NOTED.

ALL CONCRETE SHALL BE CLASS "C" UNLESS OTHERWISE NOTED.

DESIGN DATA

DESIGN STRENGTHS	
CLASS "A" CONCRETE	f _c =3,500 psi
CLASS "B" CONCRETE	f _c =3,000 psi
CLASS "C" CONCRETE	f _c =4,000 psi
REINFORCING BARS	f _y =60,000 psi
STRUCTURAL STEEL	f _y =50,000 psi

EXISTING STRUCTURE DESIGNED FOR HS-20 LOADING, IN ACCORDANCE WITH THE AASHTO 1961 SPECIFICATIONS.

LIVE LOAD
SUPERSTRUCTURE AND SUBSTRUCTURE DESIGNED FOR HL-93 LOADING IN ACCORDANCE WITH AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION, 2017.

DEAD LOAD (NORTHBOUND)
DECK DESIGNED WITH 7.5 INCH STRUCTURAL DEPTH AND 0.5 INCH INTEGRAL WEARING SURFACE.

DESIGNED FOR ACTUAL DEAD LOAD PLUS 35 PSF FUTURE WEARING SURFACE (COMPOSITE).

DEAD LOAD (SOUTHBOUND)
DECK DESIGNED WITH 7.75 INCH STRUCTURAL DEPTH AND NO INTEGRAL WEARING SURFACE.

DESIGNED FOR ACTUAL DEAD LOAD PLUS VARIABLE DEPTH CONCRETE OVERLAY (COMPOSITE). LOAD PER BEAM BASED ON ACTUAL OVERLAY THICKNESS WITH A MINIMUM OF 35 PSF.

SEISMIC DESIGN DATA

SEISMIC PERFORMANCE ZONE = X
ACCELERATION COEFFICIENT = Xg
SEISMIC SOIL PROFILE TYPE = SITE CLASS X

CONSTRUCTION LOADING

THE EXTERIOR GIRDER HAS BEEN CHECKED FOR STRENGTH, DEFLECTION, AND OVERTURNING USING THE CONSTRUCTION LOADS SHOWN BELOW. CANTILEVER OVERHANG BRACKETS WERE ASSUMED FOR THE SUPPORT OF THE DECK OVERHANG PAST THE EDGE OF THE EXTERIOR BEAM. THE FINISHING MACHINE WAS ASSUMED TO BE SUPPORTED 6 IN. OUTSIDE THE VERTICAL COPING FORM. THE TOP OVERHANG BRACKETS WERE ASSUMED TO BE LOCATED 6 IN. PAST THE EDGE OF THE VERTICAL COPING FORM. THE BOTTOM OVERHANG BRACKETS WERE ASSUMED TO BE BRACED AGAINST THE INTERSECTION OF THE BEAM BOTTOM FLANGE AND WEB.

DECK FALSEWORK LOADS

DESIGNED FOR 15 PSF FOR PERMANENT METAL STAY-IN-PLACE DECK FORMS, REMOVABLE DECK FORMS, AND 2 FT EXTERIOR WALKWAY.

CONSTRUCTION LIVE LOAD

DESIGNED FOR 20 PSF EXTENDING 2 FT PAST THE EDGE OF THE COPING AND 75 LB/FT VERTICAL FORCE APPLIED AT A DISTANCE OF 6 IN. OUTSIDE THE FACE OF COPING OVER A 30 FT LENGTH OF THE DECK CENTERED WITH THE FINISHING MACHINE.

FINISHING-MACHINE LOAD

4500 LB DISTRIBUTED OVER 10 FT ALONG THE COPING

WIND LOAD

DESIGNED FOR 70 MPH HORIZONTAL WIND LOADING IN ACCORDANCE WITH LRFD 3.8.1.

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
6 SPANS: 50'-0", 70'-0", 70'-0", 90'-0", 105'-0", 90'-0" (NBL)
6 SPANS: 50'-0", 80'-6", 80'-6", 90'-0", 105'-0", 90'-0" (SBL)
SKEW: 14°46'00" LT. TO TANGENT
CLEAR ROADWAY: 64'-0" (NBL); VARIES 62'-8 1/8" TO 63'-1 1/8" (SBL)
I-65 NBL & SBL OVER CSX R.R., 9TH STREET, BURNETTS CREEK & WABASH TRAIL
TIPPECANOE COUNTY

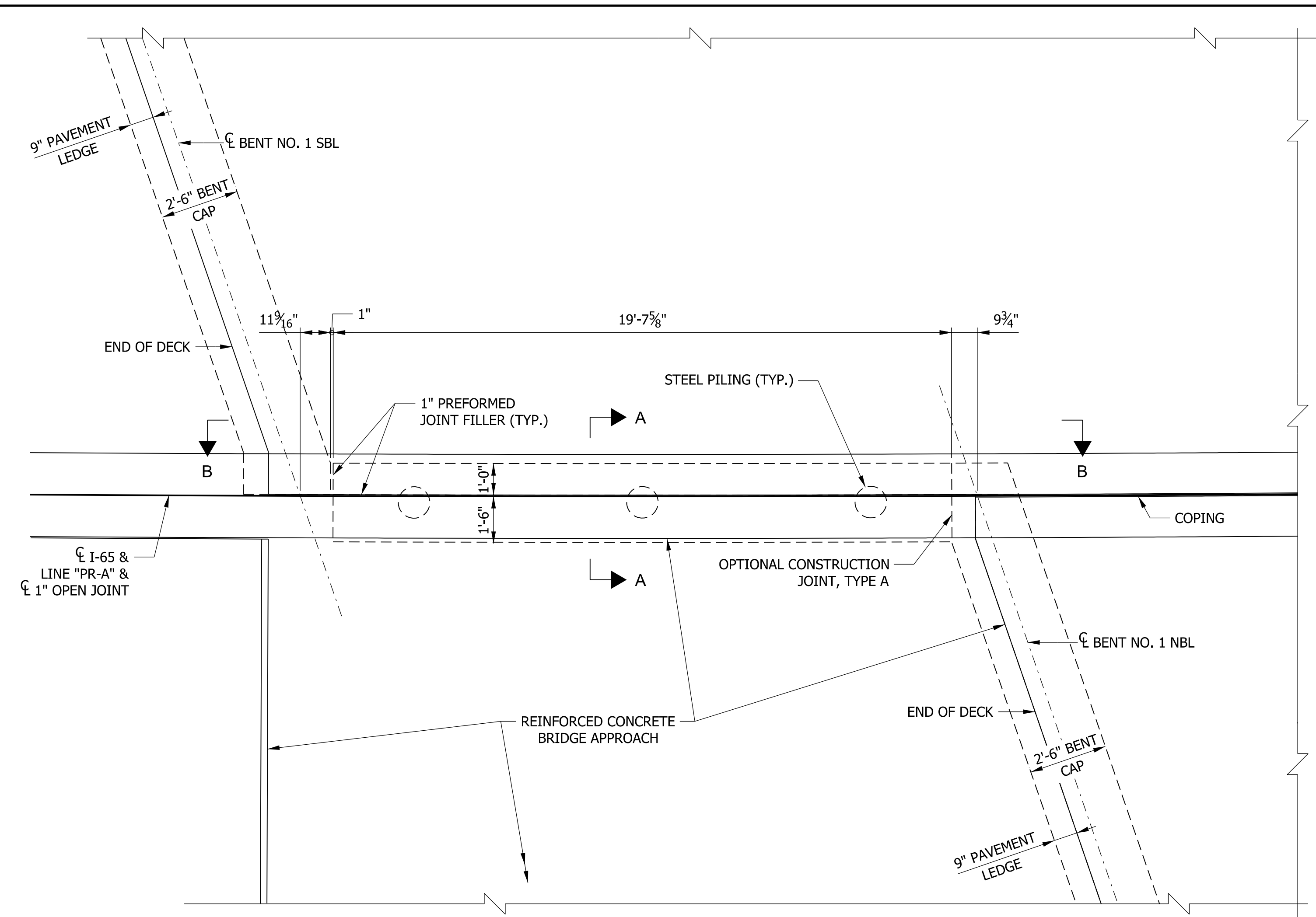
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NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MEW	DRAWN: CLF	
CHECKED: JJO	CHECKED: JJO	

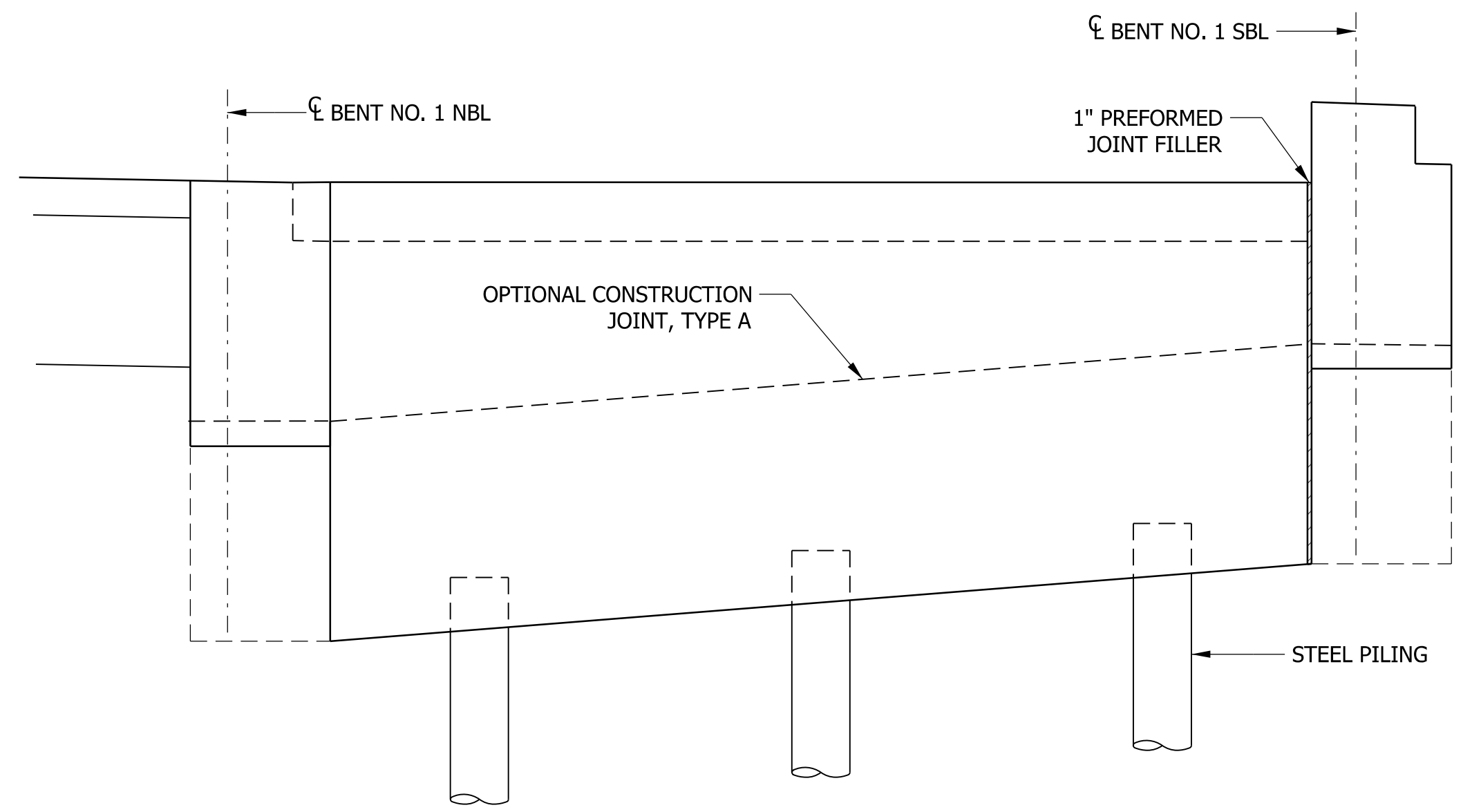
INDIANA DEPARTMENT OF TRANSPORTATION	
GENERAL PLAN	

SCALE	BRIDGE FILE
N/A	165-177-02402 CNBL & JDSBL
VERTICAL SCALE	DESIGNATION
N/A	2002114 & 2002115
SURVEY BOOK	SHEETS
ELECTRONIC	21 of 30
CONTRACT	PROJECT
R-42909	2001172

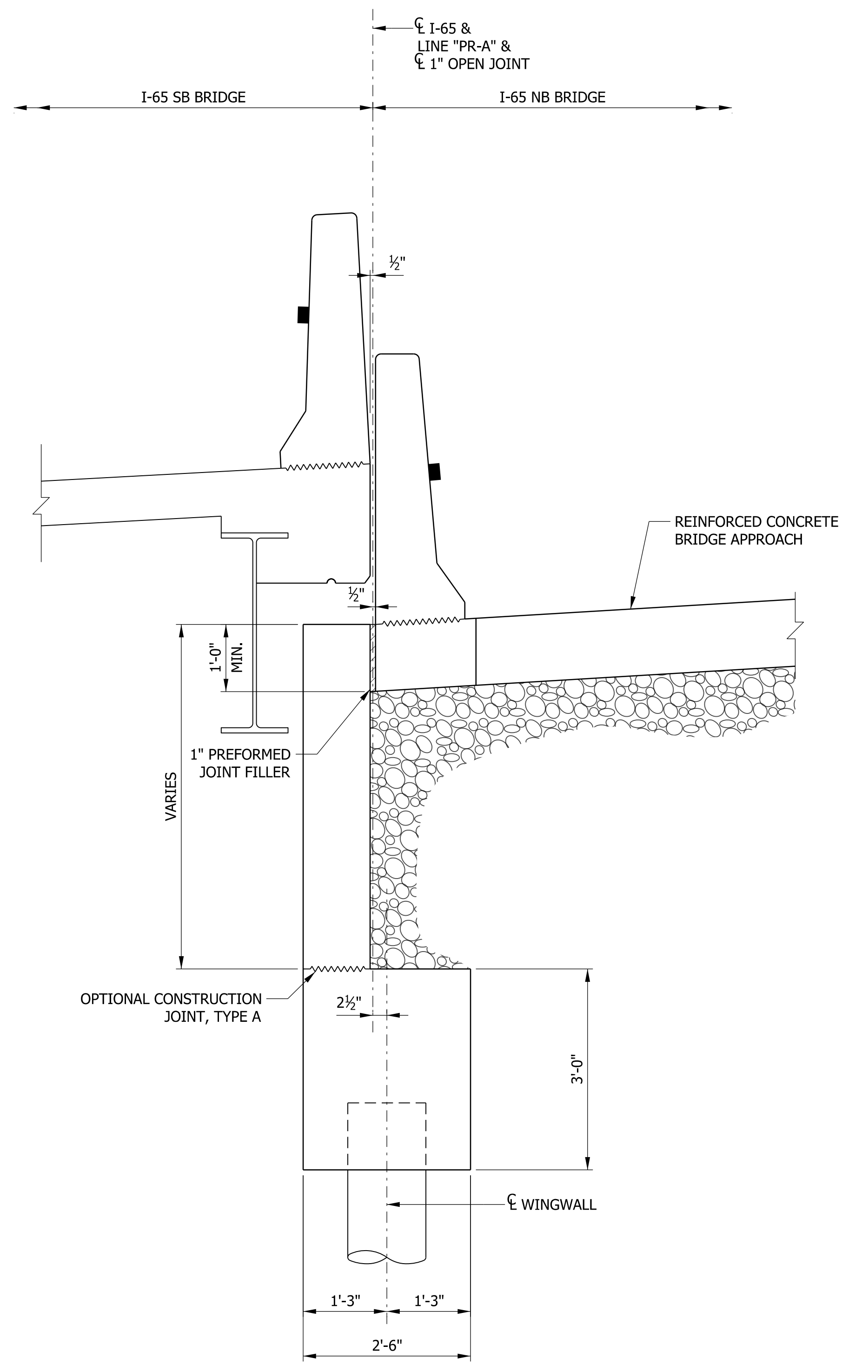
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PLAN
SCALE: 3/8" = 1'-0"



SECTION B-B
SCALE: 3/4" = 1'-0"



SECTION A-A
SCALE: 3/4" = 1'-0"

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NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ MEW _____	DRAWN: _____ CLF _____	
CHECKED: _____ JJO _____	CHECKED: _____ JJO _____	

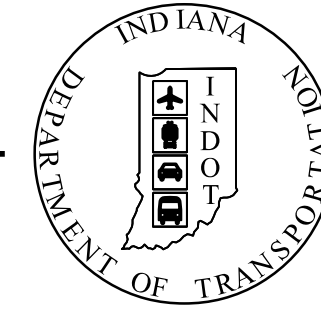
INDIANA
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS DETAILS

SCALE AS NOTED	BRIDGE FILE I65-177-02402 CNBL & JDSBL
VERTICAL SCALE AS NOTED	DESIGNATION 2002114 & 2002115
SURVEY BOOK ELECTRONIC	SHEETS 22 of 30
CONTRACT R-42909	PROJECT MS-01 2001172

PROJECT	DESIGNATION
2001172	2002116 & 2002117
CONTRACT	BRIDGE FILE
R-42909	I65-178-05485 CNBL & JCSBL

INDIANA DEPARTMENT OF TRANSPORTATION



STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
I65-178-05485 CNBL I65-178-05485 JCSBL	CONTINUOUS COMPOSITE STEEL BEAM BRIDGE	05485 CNBL 3 SPANS: 32', 40', 37', SKEW: 13°52'30" RT. 05485 JCSBL 3 SPANS: 32', 40', 37', SKEW: 13°52'30" RT.	PROPHETS ROCK ROAD	893+44.34

TRAFFIC DATA			
A.A.D.T.	(2024)	47,400	47,400
A.A.D.T.	(2044)	56,670	56,670
D.H.V.	(2044)	4,690	4,690
DIRECTIONAL DISTRIBUTION		50	50
TRUCKS		38	38
		23	23
		% A.A.D.T.	% D.H.V.

DESIGN DATA	
DESIGN SPEED	65 MPH
PROJECT DESIGN CRITERIA	RECONSTRUCTION FREEWAY
FUNCTIONAL CLASSIFICATION	INTERSTATE
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	FULL ACCESS CONTROL

KIN PROJECT INFORMATION		
DESIGNATION	PROJECT DESCRIPTION	
	ROAD	
2001172	ADDED TRAVEL LANES FROM 1.54 MILES NORTH OF SR 25 TO 2.46 MILES NORTH OF SR 43	LEAD DES.
	BRIDGE	
2002114	I-65 NB OVER CSX R.R., 9TH STREET, AND BURNETTS CREEK	
2002115	I-65 SB OVER CSX R.R., 9TH STREET, AND BURNETTS CREEK	
1601088	I-65 NB OVER SR 43	
1601090	I-65 SB OVER SR 43	
2002116	I-65 NB OVER PROPHETS ROCK ROAD	
2002117	I-65 SB OVER PROPHETS ROCK ROAD	
2002364	CR 725 N OVER I-65	

BRIDGE REHABILITATION PLANS

FOR SPANS OVER 20 FEET

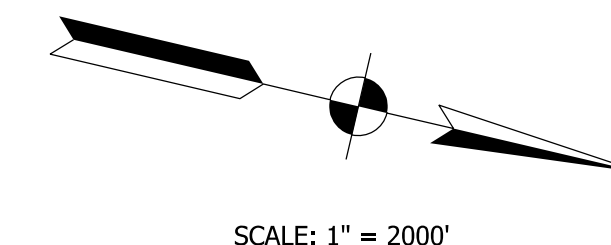
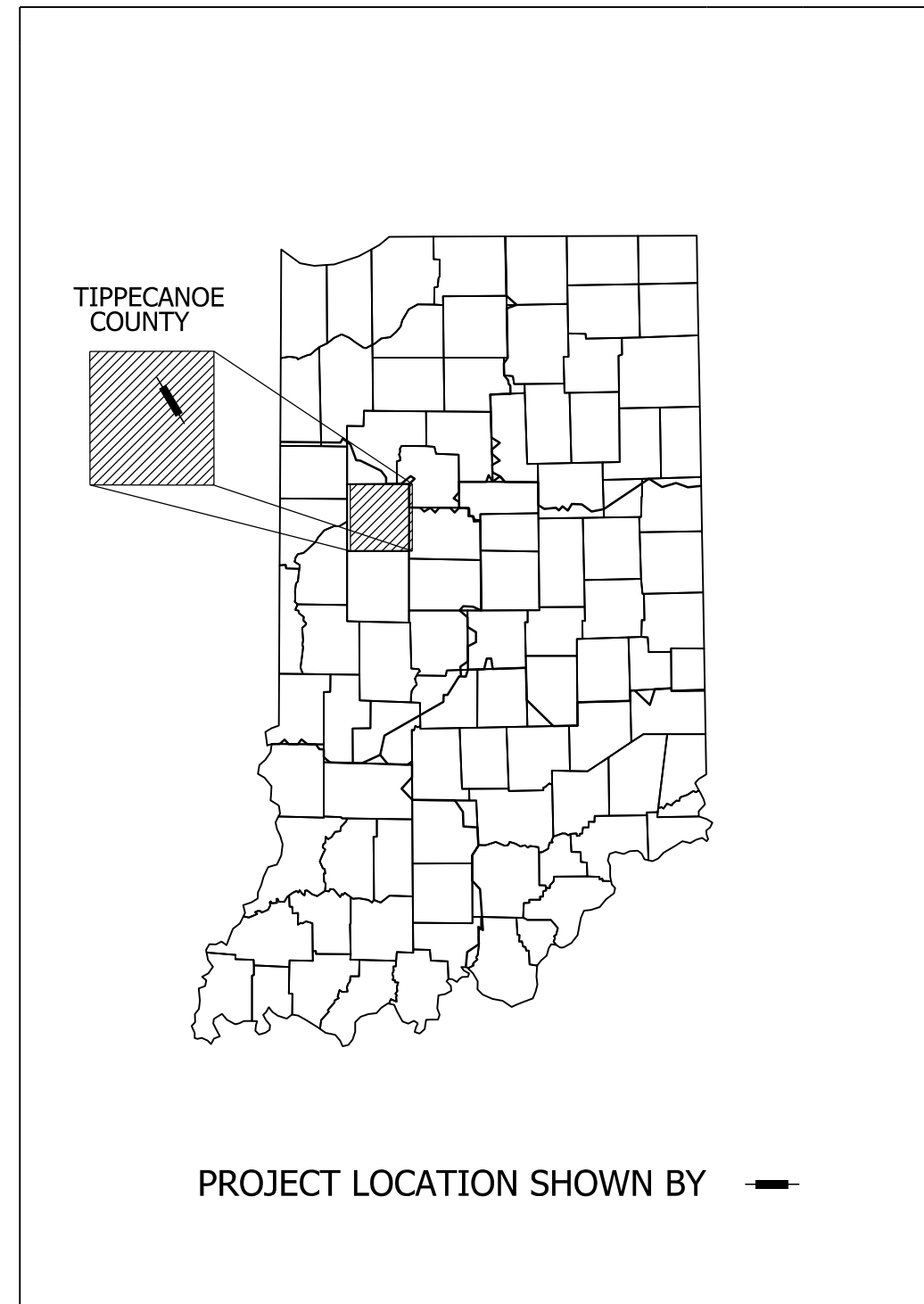
ROUTE: I-65 AT: RP 178+06

PROJECT NO. 2001172 P.E. CONST.

NO ADDITIONAL RIGHT-OF-WAY
REQUIRED FOR THIS PROJECT

BRIDGE DECK REPLACEMENT, WIDENING AND SUPERSTRUCTURE REHABILITATION ON STRUCTURE I65-178-05485 CNBL AND BRIDGE DECK REPLACEMENT, WIDENING AND SUPERSTRUCTURE REHABILITATION ON STRUCTURE I65-178-05485 JCSBL. THE STRUCTURES ARE LOCATED ON INTERSTATE 65 (I-65) APPROXIMATELY 0.24 MILE SOUTH OF THE STATE ROAD (SR) 43 INTERCHANGE. IN SECTION 27 TOWNSHIP 24 N, RANGE 4 WEST IN TIPPECANOE TOWNSHIP, TIPPECANOE COUNTY, INDIANA.

PRELIMINARY PLAN SUBMITTAL
OCTOBER 2020



LATITUDE: 40°29'44" LONGITUDE: -86°51'51"

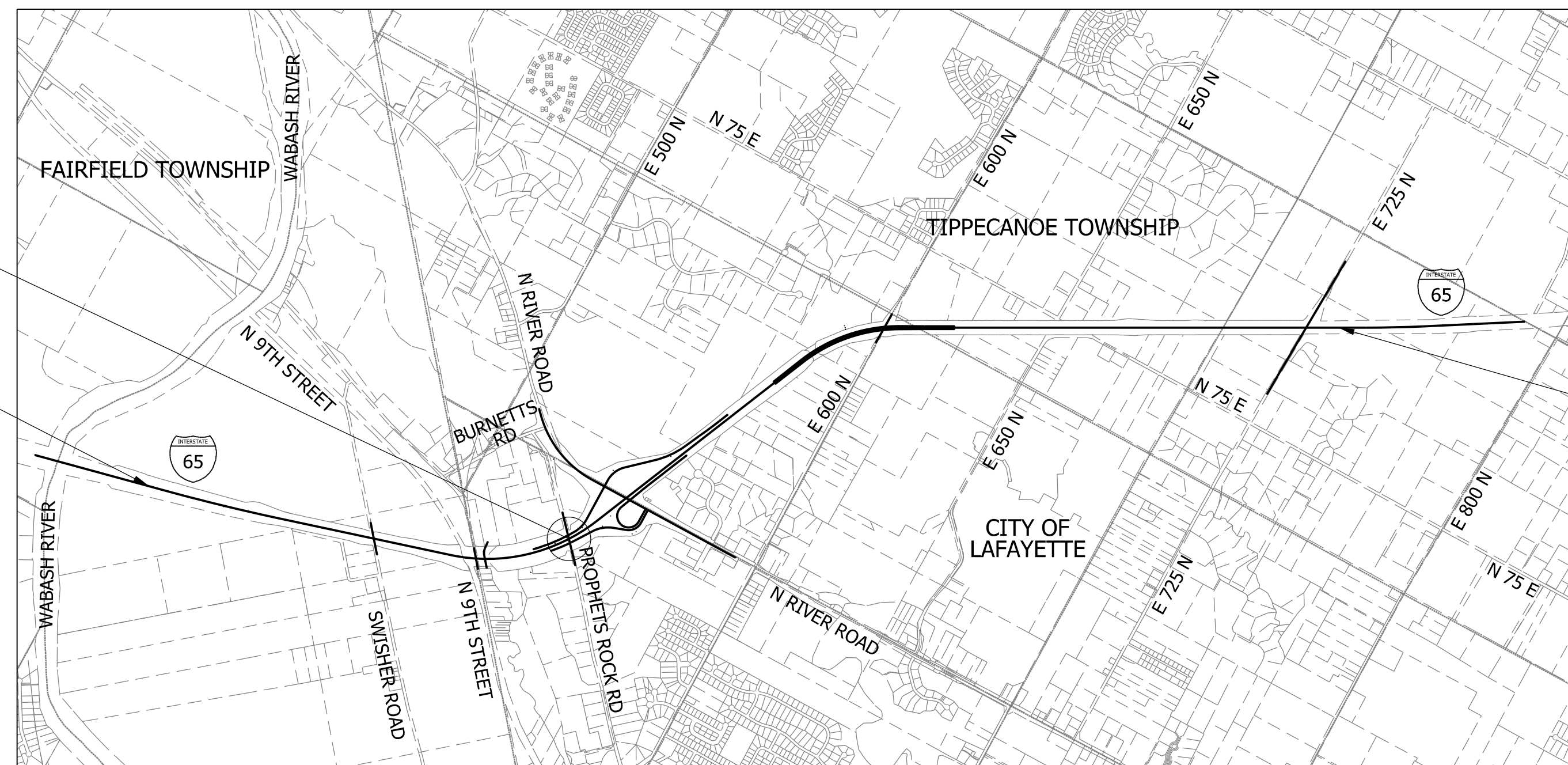
BRIDGE LENGTH: 0.02 MI.
ROADWAY LENGTH: 0.00 MI.
TOTAL LENGTH: 0.02 MI.
MAX. GRADE: 3.00% %

HUC: 05120108010030

BRIDGE FILE NO.
I65-178-05485 CNBL
I65-178-05485 JCSBL
I-65 OVER PROPHETS ROCK ROAD

BEGIN PROJECT
PROJECT NO. 2001172
STA. 822+25.00
LINE "PR-A"

END PROJECT
PROJECT NO. 2001172
STA. 1036+03.00
LINE "PR-A"



LOCATION MAP
TIPPECANOE COUNTY

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS.

INFRASTRUCTURE ENGINEERING | INCORPORATED
201 South Capitol Avenue | Suite 490 | Indianapolis, IN 46225
P 317.243.9800 | F 317.243.9100 | www.infrastructure-eng.com

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NOT FOR CONSTRUCTION

PLANS PREPARED BY: Infrastructure Engineering, Inc. (317) 243-9800
PHONE NUMBER
CERTIFIED BY: _____ DATE
APPROVED FOR LETTING: _____ DATE
INDIANA DEPARTMENT OF TRANSPORTATION

BRIDGE FILE	
I65-178-05485 CNBL & JCSBL	
DESIGNATION	
2002116 & 2002117	
SURVEY BOOK	SHEETS TI-01
ELECTRONIC	1 of 26
CONTRACT	PROJECT
R-42909	2001172

CONSTRUCTION LEGEND

PCCP Alternate-Bid Pavement Design (Preliminary)

- (A) Full Depth PCCP Pavement Design - I-65 Travel Lanes and Shoulders
 QC/QA-PCCP, 11 in. (Widened Slab 2 ft into Outside Shoulder) with D-1 Joints Spaced at 16 ft with 1 1/2 in. Diameter Dowel Bars Spaced at 12 in. on
 Drainage Layer Consisting of:
 300 lb/Syd QC/QA-HMA, 4, 76, Intermediate, OG 19.0mm on
 Separation Layer Consisting of:
 6" of Compacted Aggregate, No. 53 on
 Geotextile for Subgrade, Type 2B on
 Subgrade Treatment, Type 1B (12 in. Chemical Modification & 14 in. Foundation Soil Stabilization with Cement)

HMA Alternate-Bid Pavement Design (Preliminary)

- (K1) Full Depth HMA Pavement Design - I-65 Travel Lanes Including 2 ft Into Outside Shoulders
 220 #/Syd. QC/QA-HMA, 4, 76, Surface, 12.5mm - SMA on
 220 #/Syd. QC/QA-HMA, 4, 76, Intermediate, 12.5mm - SMA on
 1100 #/Syd. QC/QA-HMA, 4, 64, Base, 25.0mm on
 Drainage Layer Consisting of:
 300 #/Syd. QC/QA-HMA, 4, 76, Intermediate, OG 19.0mm on
 Separation Layer Consisting of:
 6 in. of Compacted Aggregate, No. 53 on
 Geotextile for Subgrade, Type 2B on
 Subgrade Treatment, Type 1B (12 in. Chemical Modification & 14 in. Foundation Soil Stabilization with Cement)

- (K2) Full Depth HMA Pavement Design - I-65 Shoulders Excluding 2 ft Adjacent to Outside Travel Lane
 220 #/Syd. QC/QA-HMA, 3, 70, Surface, 12.5mm on
 220 #/Syd. QC/QA-HMA, 2, 64, Intermediate, 12.5mm on
 1100 #/Syd. QC/QA-HMA, 2, 64, Base, 25.0mm on
 Drainage Layer Consisting of:
 300 #/Syd. QC/QA-HMA, 4, 76, Intermediate, OG 19.0mm on
 Separation Layer Consisting of:
 6 in. of Compacted Aggregate, No. 53 on
 Geotextile for Pavement, Type 2B on
 Subgrade Treatment, Type 1B (12 in. Chemical Modification & 14 in. Foundation Soil Stabilization with Cement)

- (K3) 165 #/Syd. QC/QA-HMA, 2, 64, Surface, 9.5mm on
 495 #/Syd. QC/QA-HMA, 2, 64, Base, 25.0mm on
 4 in. of Compacted Aggregate, No. 53 on
 Subgrade Treatment, Type 1C

(C1) Milled HMA Corruguations, Conventional

(C2) Milled PCCP Corruguations, Conventional

(N) Compacted Aggregate, No. 53

(R) Seeding

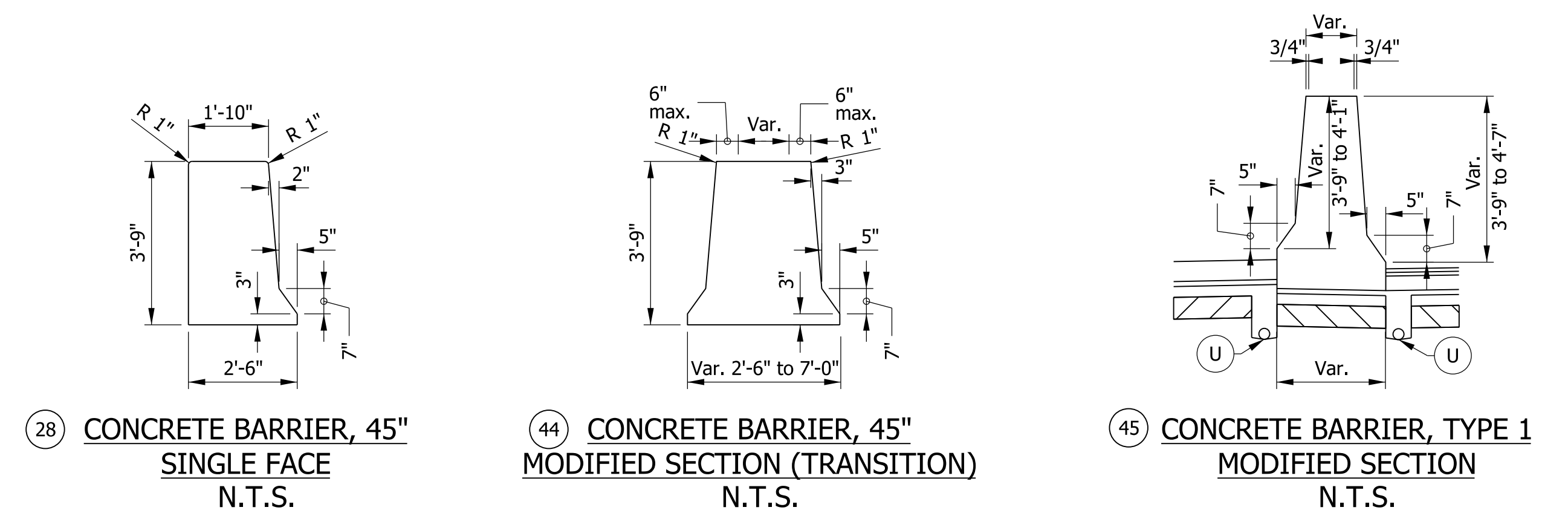
(R1) 165 #/Syd. QC/QA-HMA, 2, 64, Surface 9.5 mm on Milling, Asphalt 1 1/2 in.

(S) Saw Cut Full Depth

(U) 6 In. Underdrain

(X) Longitudinal Construction Joint

- (12) Impact Attenuator
 (21) Concrete Barrier (45")
 (22) Concrete Barrier, Modified Section
 (27) Sodding
 (28) Concrete Barrier, Modified Section (45", Single Face)
 (29) Guardrail Double faced Thrie Beam
 (30) Guardrail MGS W-Beam
 (43) Slopewall, Concrete, 4 In.
 (44) Concrete Barrier, Modified Section (Transition)
 (45) Concrete Barrier, Modified Section
 (50) High-Tension Cable-Barrier System



FOR INFORMATION ONLY
 SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

NOTE TO REVIEWER:
 This is a Pavement Alternate Bid Contract. Preliminary pavement designs for Line "PR-A" based on final approved pavement designs used for Des. No. 1802958/Contract No. R-41849 (I-65 ATL Project Near Lebanon). Final pavement designs to be completed in a future submittal.

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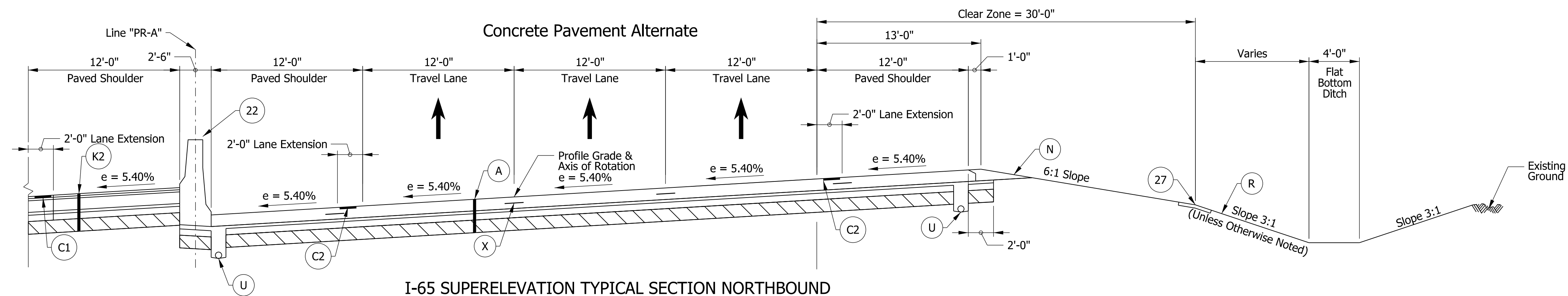
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ DCK _____	DRAWN: _____ DCK _____	
CHECKED: _____ MKM _____	CHECKED: _____ MKM _____	

INDIANA
 DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
 I-65 LINE "PR-A"

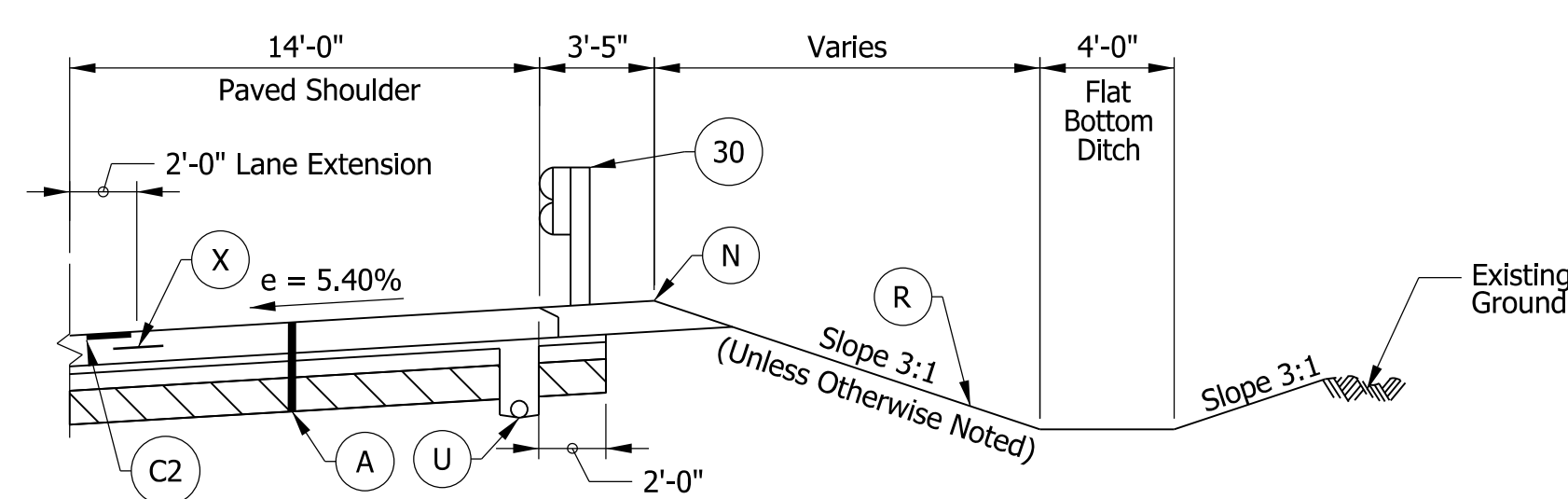
HORIZONTAL SCALE	BRIDGE FILE
3/16"=1'-0"	165-177-02402 CNBL & JCSBL
VERTICAL SCALE	DESIGNATION
N/A	2001172
SURVEY BOOK	SHEETS TS-01
ELECTRONIC	3 of 26
CONTRACT	PROJECT
R-42909	2001172

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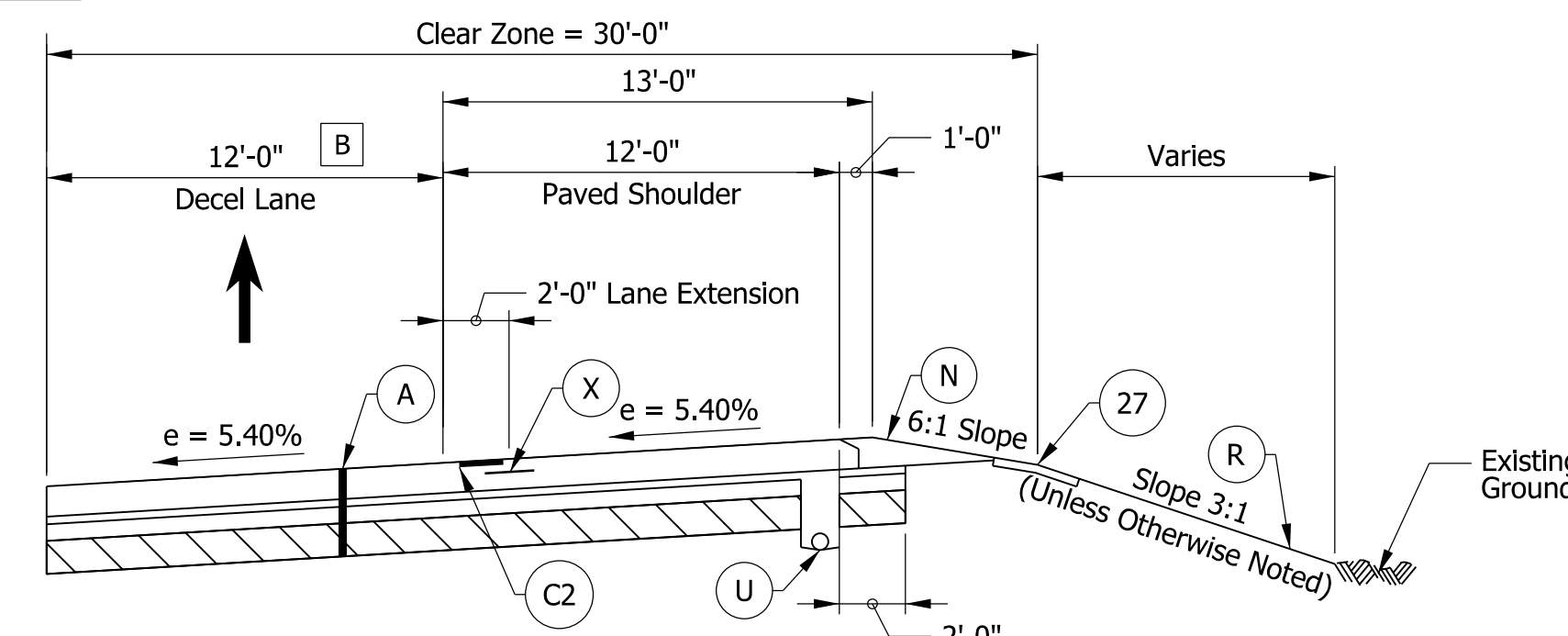


I-65 SUPERELEVATION TYPICAL SECTION NORTHBOUND

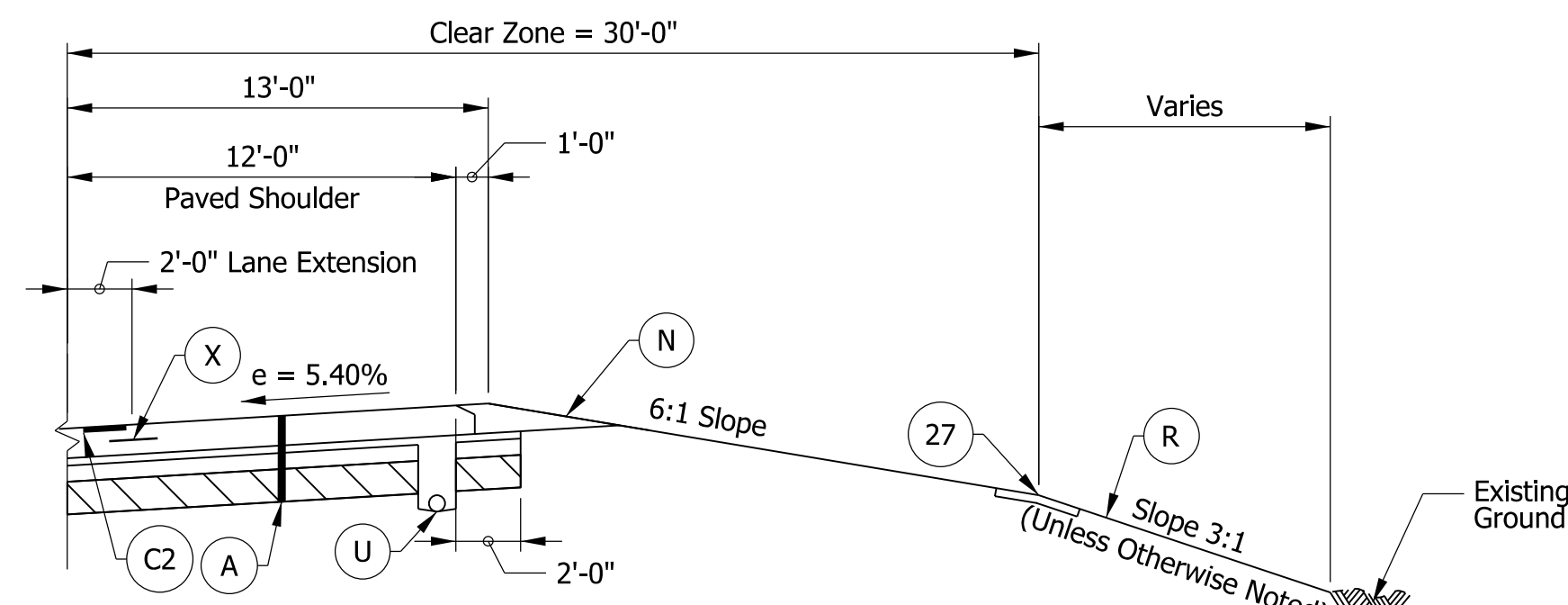
A STA. 870+80.00 "PR-A" to STA. 873+15.76 "PR-A"
 STA. 878+87.58 "PR-A" to STA. 880+10.00 "PR-A"
 STA. 891+09.00 "PR-A" to STA. 892+50.88 "PR-A"
 STA. 894+26.51 "PR-A" to STA. 895+39.00 "PR-A"



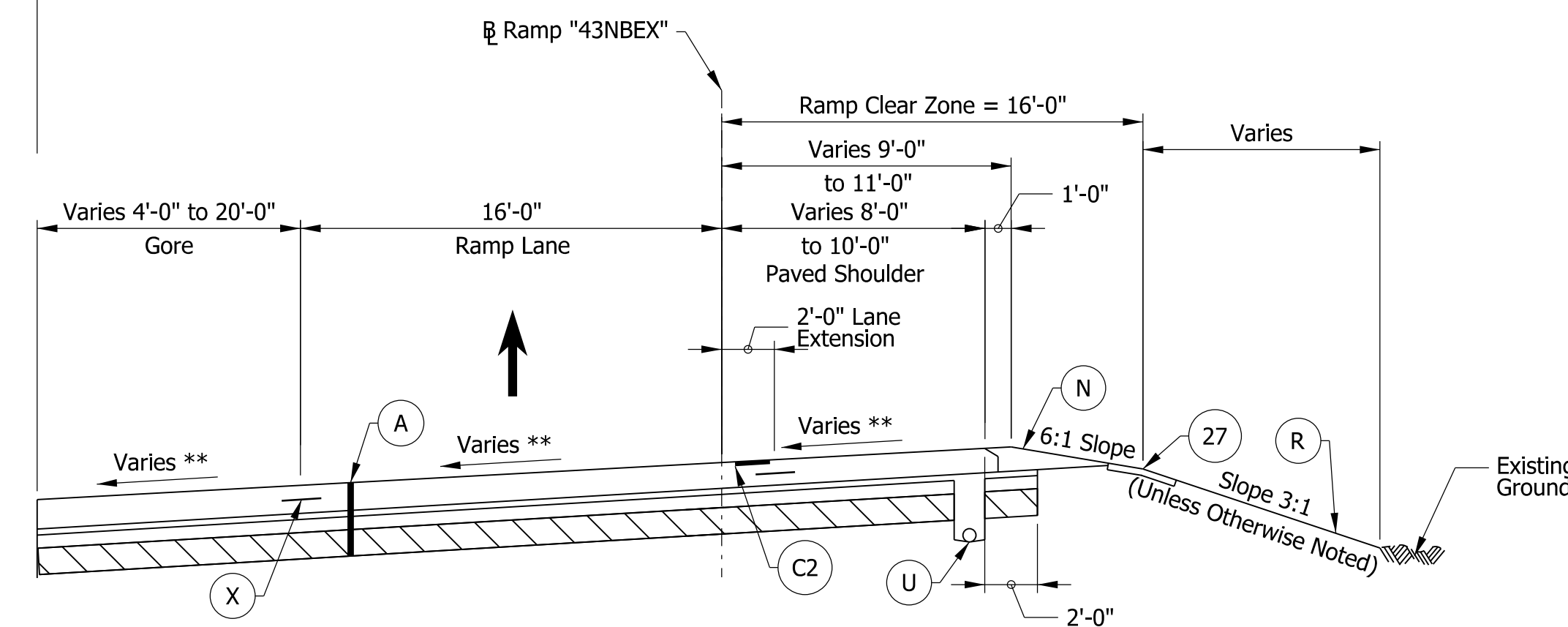
NB GUARDRAIL AND OUTSIDE SHOULDER DETAIL
 STA. 870+45.03 "PR-A" to STA. 873+15.76 "PR-A" NB



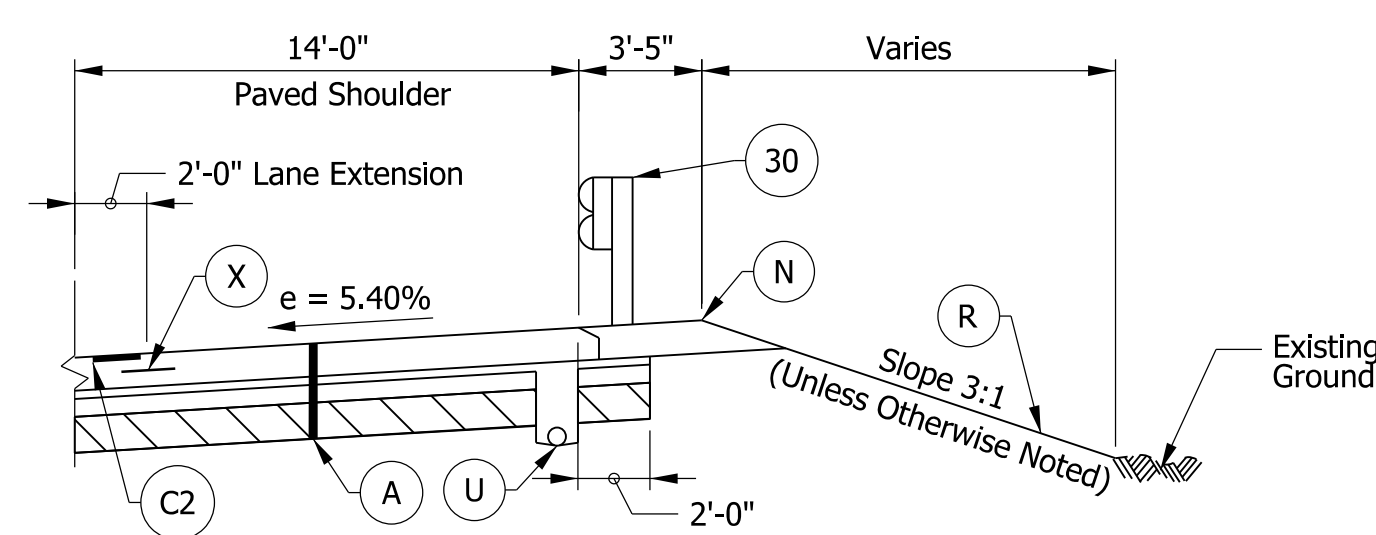
DECELERATION LANE - I-65 NORTHBOUND AT SR 43
 STA. 885+70.00 "PR-A" to STA. 892+07.73 "PR-A"



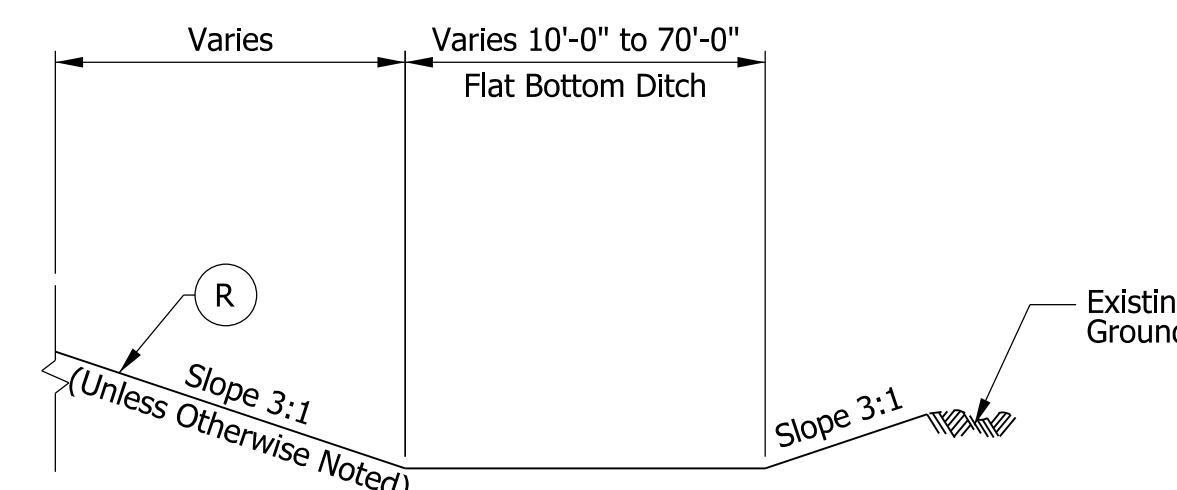
NB OUTSIDE SHOULDER DETAIL
 STA. 878+89.67 "PR-A" to STA. 885+70.00 "PR-A" NB



EXIT RAMP GORE SECTION - I-65 NORTHBOUND AT SR 43
 STA. 892+07.73 "PR-A" to STA. 892+50.88 "PR-A"
 STA. 894+26.51 "PR-A" to STA. 896+10.44 "PR-A"



NB GUARDRAIL AND OUTSIDE SHOULDER DETAIL
 STA. 890+68.85 "PR-A" to STA. 892+50.88 "PR-A"



OUTSIDE WIDE DITCH DETAIL
 STA. 886+00.00 "PR-A" to STA. 889+00.00 "PR-A" NB

- A NB Bridge Paving Exception from STA. 873+15.76 "PR-A" to STA. 878+87.58 "PR-A"
- NB Bridge Paving Exception from STA. 892+50.88 "PR-A" to STA. 894+26.51 "PR-A"
- B Width varies at bridge approaches. Shoulder widening at 15:1 taper rate. See Construction Detail sheets for detailed dimensions.
- C See Construction Detail sheets for limits of acceleration and deceleration lanes and taper limits.

FOR INFORMATION ONLY
 SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

- Notes:**
1. See TS-00 for Legend.
 - * Max rollover between shoulder and travel lane not to exceed 8%. See Superelevation Detail for transition information
 - ** Slope varies. See Gore Details.

NOTE TO REVIEWER:
 High-side shoulder superelevation transition to meet Indiana Design Manual requirements. Under evaluation with on-going bridge projects. Details to be provided in future submittal.

NOTE TO REVIEWER:
 Pavement design assumed to be asphalt for estimating purposes. Pavement design to be approved in a future submittal.

NOTE TO REVIEWER:
 Safety Edge detail to be provided in a future submittal.

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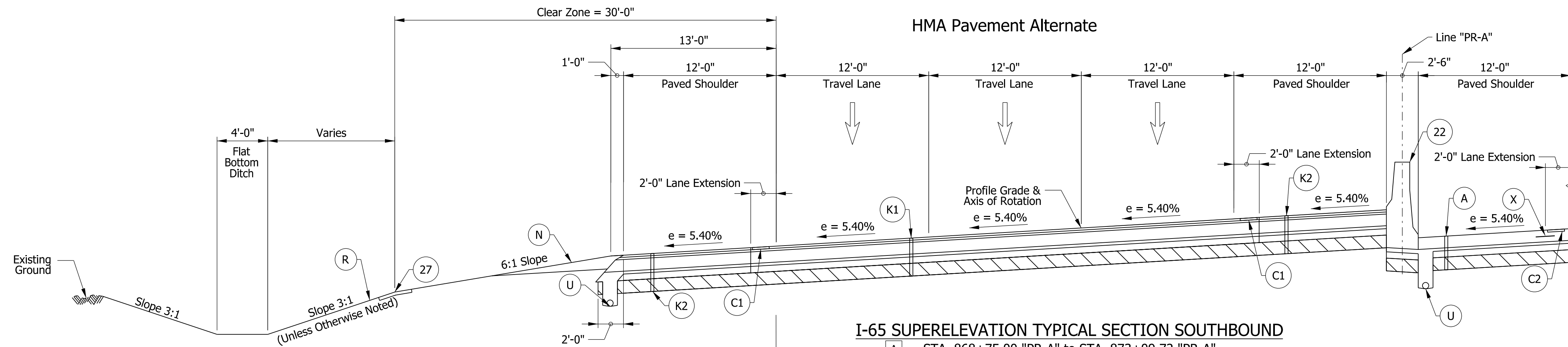
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DESIGNED: _____ DCK _____	DRAWN: _____ DCK _____	
CHECKED: _____ MKM _____	CHECKED: _____ MKM _____	

INDIANA DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS I-65 LINE "PR-A"

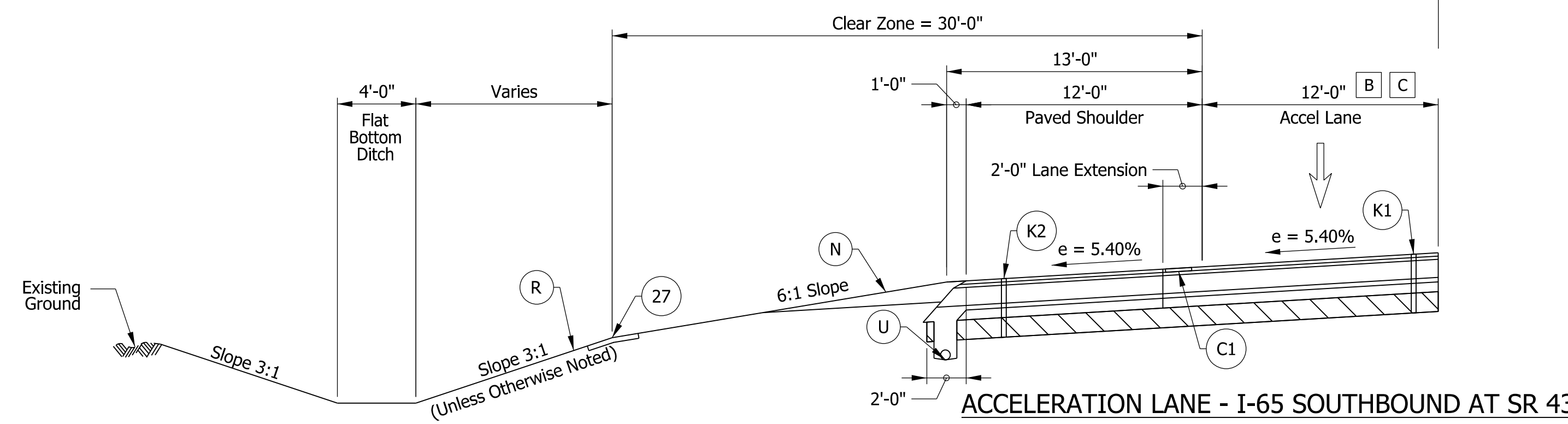
HORIZONTAL SCALE 3/16"=1'-0"	BRIDGE FILE 165-177-02402 CNBL & JCSBL
VERTICAL SCALE N/A	DESIGNATION 2001172
SURVEY BOOK ELECTRONIC	SHEETS 5 of 26
CONTRACT R-42909	PROJECT 2001172

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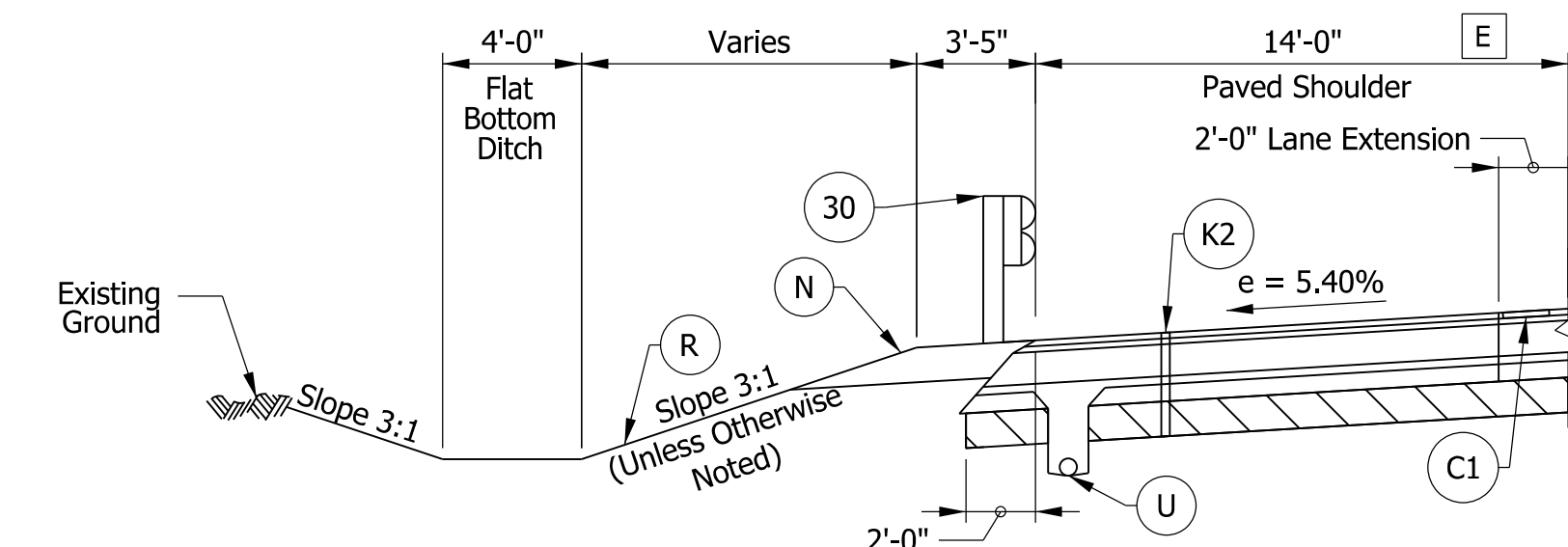
I-65 SUPERELEVATION TYPICAL SECTION SOUTHBOUND

- A STA. 868+75.00 "PR-A" to STA. 873+09.72 "PR-A"
- STA. 878+78.53 "PR-A" to STA. 892+68.37 "PR-A"
- STA. 894+48.55 "PR-A" to STA. 901+84.00 "PR-A"



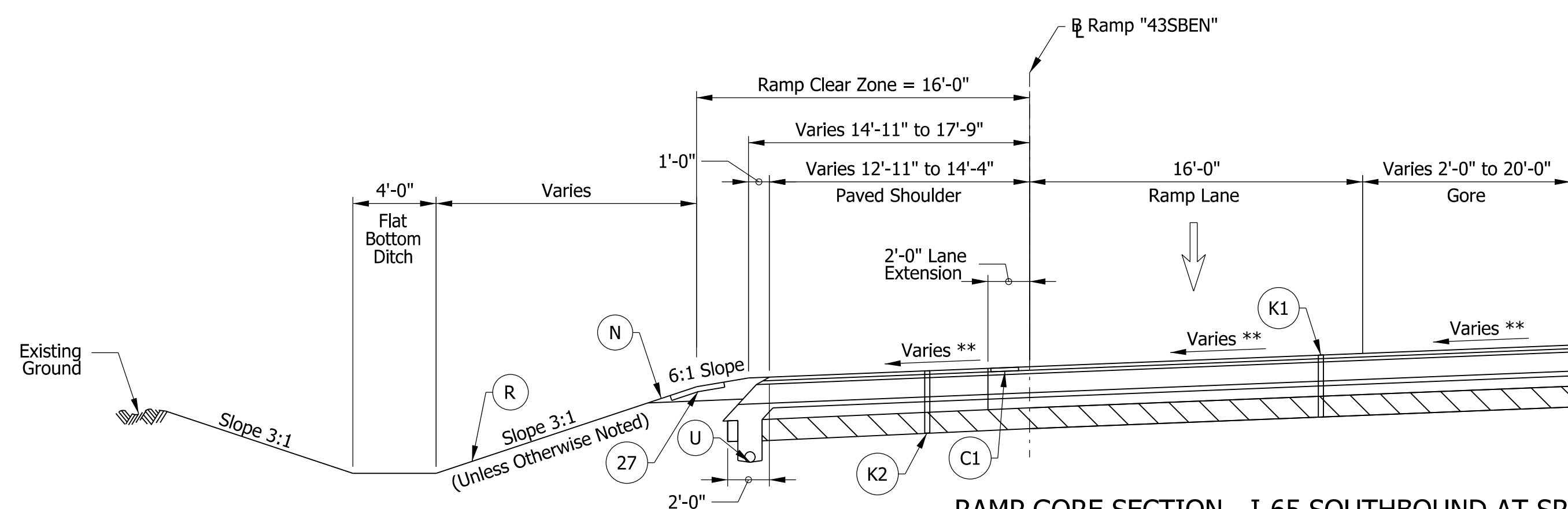
ACCELERATION LANE - I-65 SOUTHBOUND AT SR 43

- STA. 879+14.50 "PR-A" to STA. 892+68.37 "PR-A"
- STA. 894+48.55 "PR-A" to STA. 895+94.65 "PR-A"



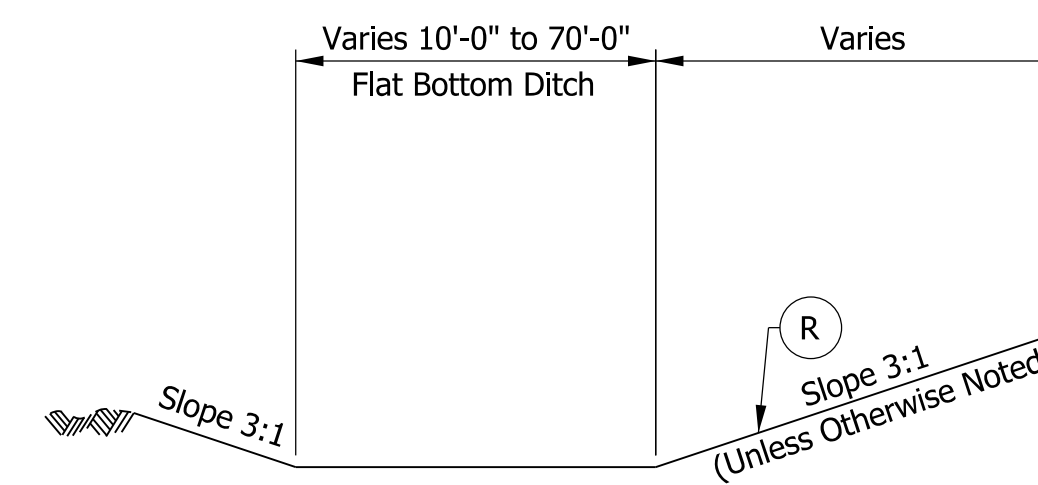
SB GUARDRAIL AND OUTSIDE SHOULDER DETAIL

- STA. 878+78.53 "PR-A" to STA. 881+35.00 "PR-A"
- STA. 894+48.55 "PR-A" to STA. 895+00.43 "PR-A"



RAMP GORE SECTION - I-65 SOUTHBOUND AT SR 43

- STA. 895+94.65 "PR-A" to STA. 896+87.97 "PR-A"



OUTSIDE WIDE DITCH DETAIL

- STA. 885+50.00 "PR-A" to STA. 891+50.00 "PR-A" SB

FOR INFORMATION ONLY
SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

- A SB Bridge Paving Exception from STA. 873+09.72 "PR-A" to STA. 878+78.53 "PR-A"
- SB Bridge Paving Exception from STA. 892+68.37 "PR-A" to STA. 894+48.55 "PR-A"
- B See Construction Detail sheets for limits of acceleration and deceleration lanes and taper limits.
- C Width varies from 0'-0" to 12'-0" from STA. 879+14.50 "PR-A" to STA. 886+64.50 "PR-A"
- Width varies from 12'-0" to 18'-2" from STA. 895+00.43 "PR-A" to STA. 895+94.65 "PR-A"
- E Width varies from 19'-8" to 19'-11" from STA. 894+46.33 "PR-A" to STA. 895+00.43 "PR-A"

Notes:

1. See TS-00 for Legend.
 2. Cross slope varies. See Superelevation Detail and Intersection Detail Sheets for cross slope transition locations.
- * Max rollover between shoulder and travel lane not to exceed 8%. See Superelevation Detail for transition information
- ** Slope varies. See Gore Details.

NOTE TO REVIEWER:

High-side shoulder superelevation transition to meet Indiana Design Manual requirements. Under evaluation with on-going bridge projects. Details to be provided in future submittal.

NOTE TO REVIEWER:

Pavement design assumed to be asphalt for estimating purposes. Pavement design to be approved in a future submittal.

NOTE TO REVIEWER:

Safety Edge detail to be provided in a future submittal.

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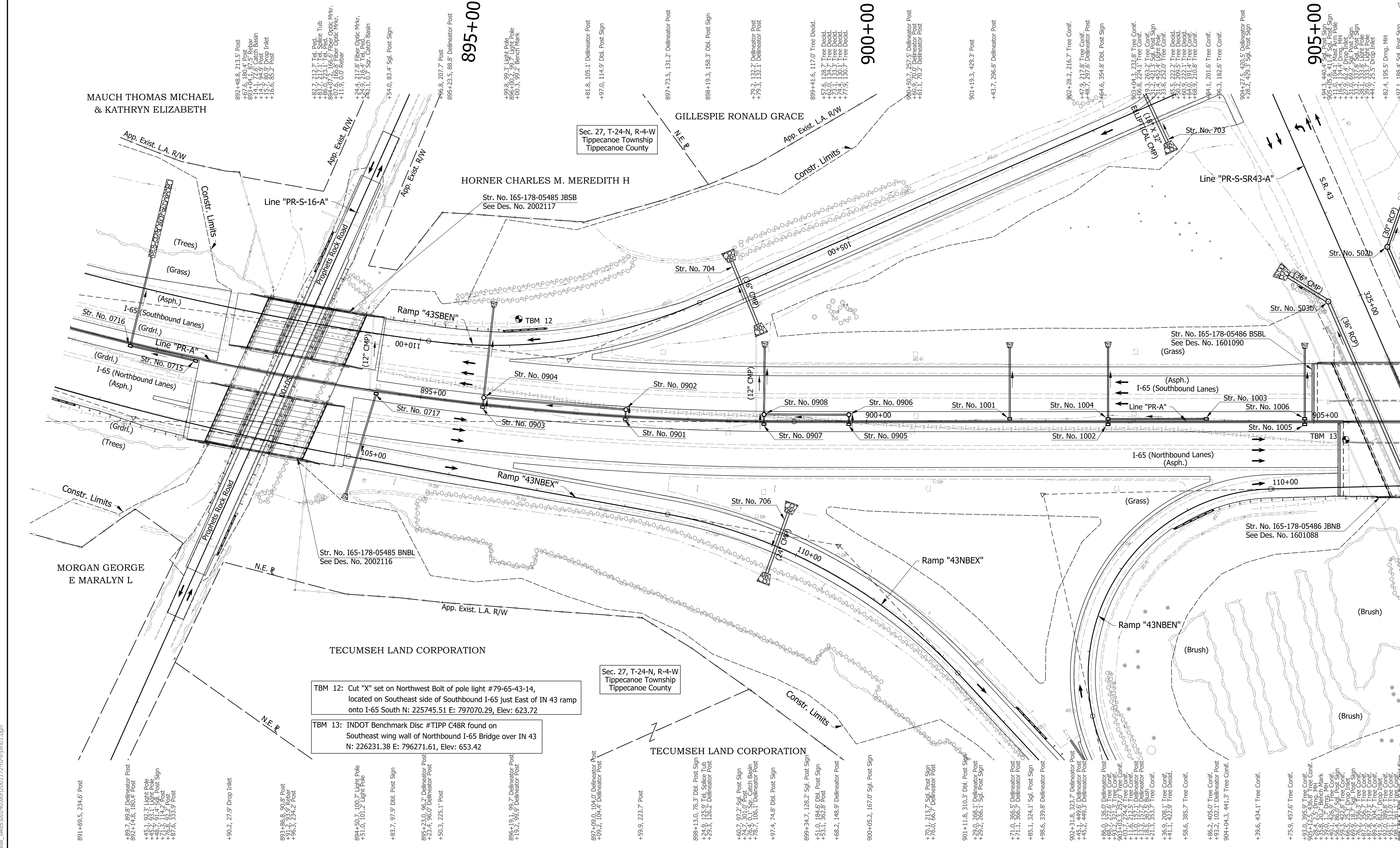
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ DCK _____	DRAWN: _____ DCK _____	
CHECKED: _____ MKM _____	CHECKED: _____ MKM _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTIONS
I-65 LINE "PR-A"

HORIZONTAL SCALE	BRIDGE FILE
3/16"=1'-0"	165-177-02402 CNBL & JCSBL
VERTICAL SCALE	DESIGNATION
N/A	2001172
SURVEY BOOK	SHEETS
ELECTRONIC	6 of 26
CONTRACT	PROJECT
R-42909	2001172

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- Notes:**
- Existing topography notes taken from Line "PR-A".
 - For geometric information, see Geometric Layout Sheets.
 - For control point information, see Reference Tie Sheets.
 - Line "PR-A" to be constructed.

TBM 12: Cut "X" set on Northwest Bolt of pole light #79-65-43-14, located on Southeast side of Southbound I-65 just East of IN 43 ramp onto I-65 South N: 225745.51 E: 797070.29, Elev: 623.72

TBM 13: INDOT Benchmark Disc #TIPP C48R found on Southeast wing wall of Northbound I-65 Bridge over IN 43 N: 226231.38 E: 796271.61, Elev: 653.42

FOR INFORMATION ONLY

SEE ROAD PLANS, GRADE REVIEW SUBMITTAL

DRAFT
NOT FOR CONSTRUCTION

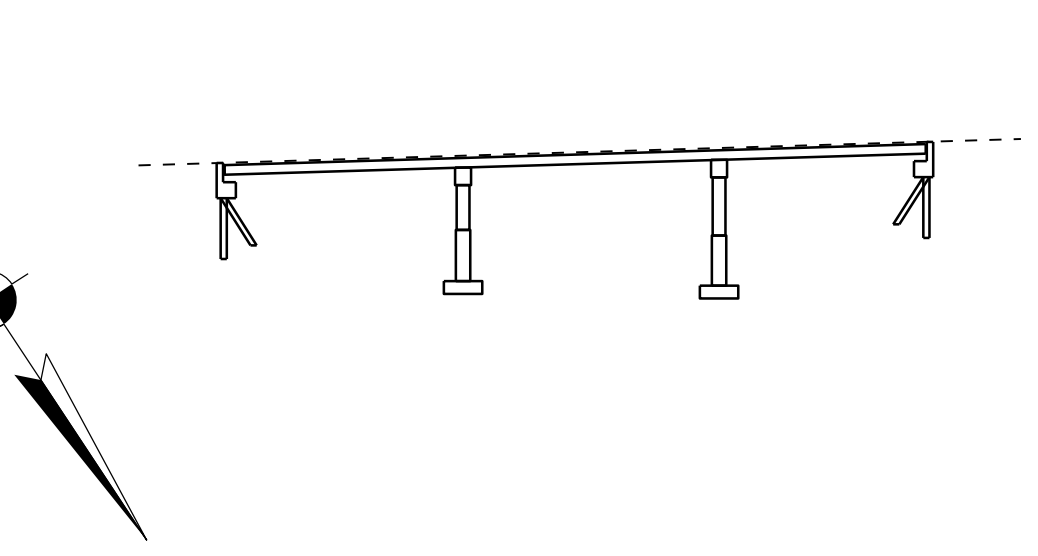
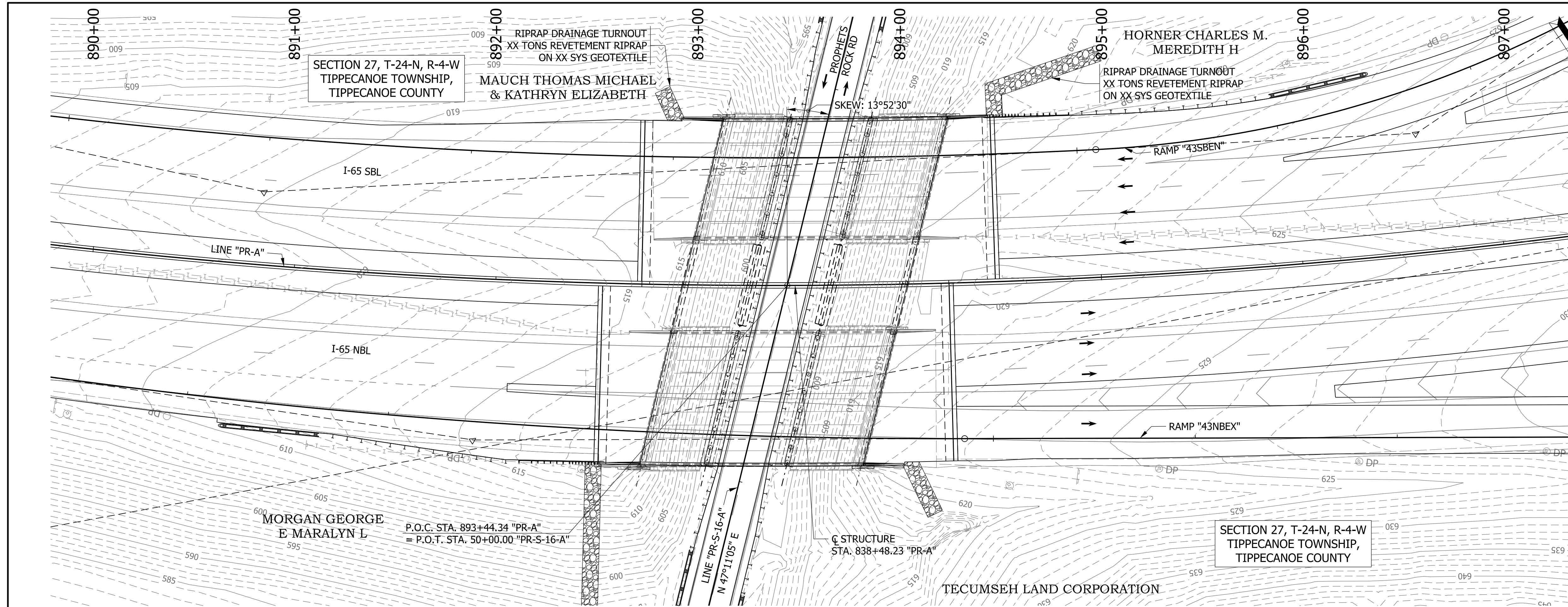
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DESIGNED: JMA	DRAWN: CEB	
CHECKED: MKM	CHECKED: MKM	

INDIANA DEPARTMENT OF TRANSPORTATION

PLAN SHEET
LINE "PR-A"
STA. 891+00 TO STA. 906+00

HORIZONTAL SCALE	BRIDGE FILE
1"=50'	165-178-05485 CNBL & JCSBL
VERTICAL SCALE	DESIGNATION
N/A	2001172
SURVEY BOOK	SHEETS
ELECTRONIC	11 of 26
CONTRACT	PROJECT
R-42909	2001172

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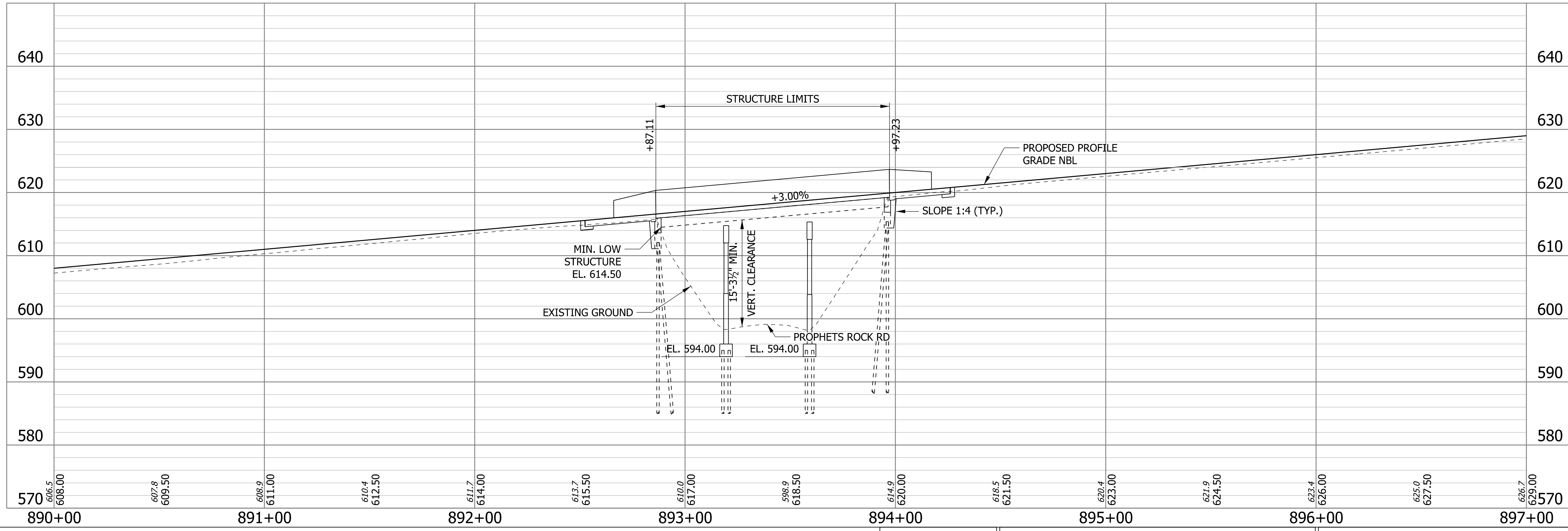
EXISTING STRUCTURE
 THE EXISTING NORTHBOUND STRUCTURE IS A THREE SPAN, ROLLED STEEL BEAM BRIDGE. THE EXISTING BRIDGE IS SKEWED 13°52'30" RT. TO TANGENT WITH SPANS OF 32'-0", 40'-0", 37'-0", AND A CLEAR ROADWAY WIDTH OF 64'-5". THE STRUCTURE WAS BUILT IN 1968 WITH A DECK OVERLAY IN 1979 AND A DECK OVERLAY AND WIDENING IN 1991. EXISTING PLANS ARE ON FILE WITH INDIANA DEPARTMENT OF TRANSPORTATION UNDER BRIDGE FILE 165-180-5485, 165-180-5485A, 165-180-5485B.

EARTHWORK TABULATION

FILL + 25%	XXX CYS
COMMON EXCAVATION	XXX CYS
BORROW	XXX CYS
EXCAVATION, FOUNDATION, UNCLASSIFIED	XXX CYS

HORIZONTAL CURVE DATA

P.I. STA. = 886+24.35
 $\Delta = 49^{\circ}55'06"$ LT.
 R = 3190.00'
 T = 1484.76'
 L = 2779.26'
 E = 328.61'
 SE = 5.40%



NOTE TO REVIEWER:
 Earthwork tabulations and quantities to be completed in future submittal.

CONTINUOUS COMPOSITE ROLLED STEEL BRIDGE
 3 SPANS: 32'-0", 40'-0", 37'-0"
 SKEW: 13°52'30" RT. TO TANGENT
 CLEAR ROADWAY 86'-8 1/2"
 I-65 NBL OVER PROPHETS ROCK ROAD
 TIPPECANOE COUNTY

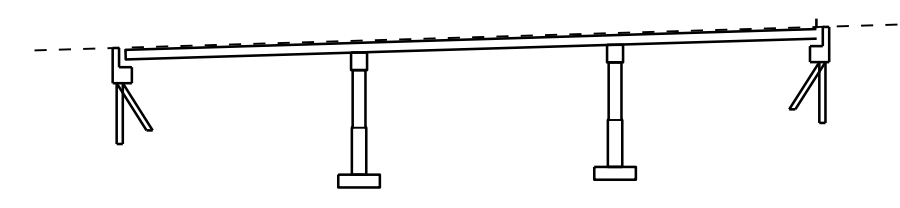
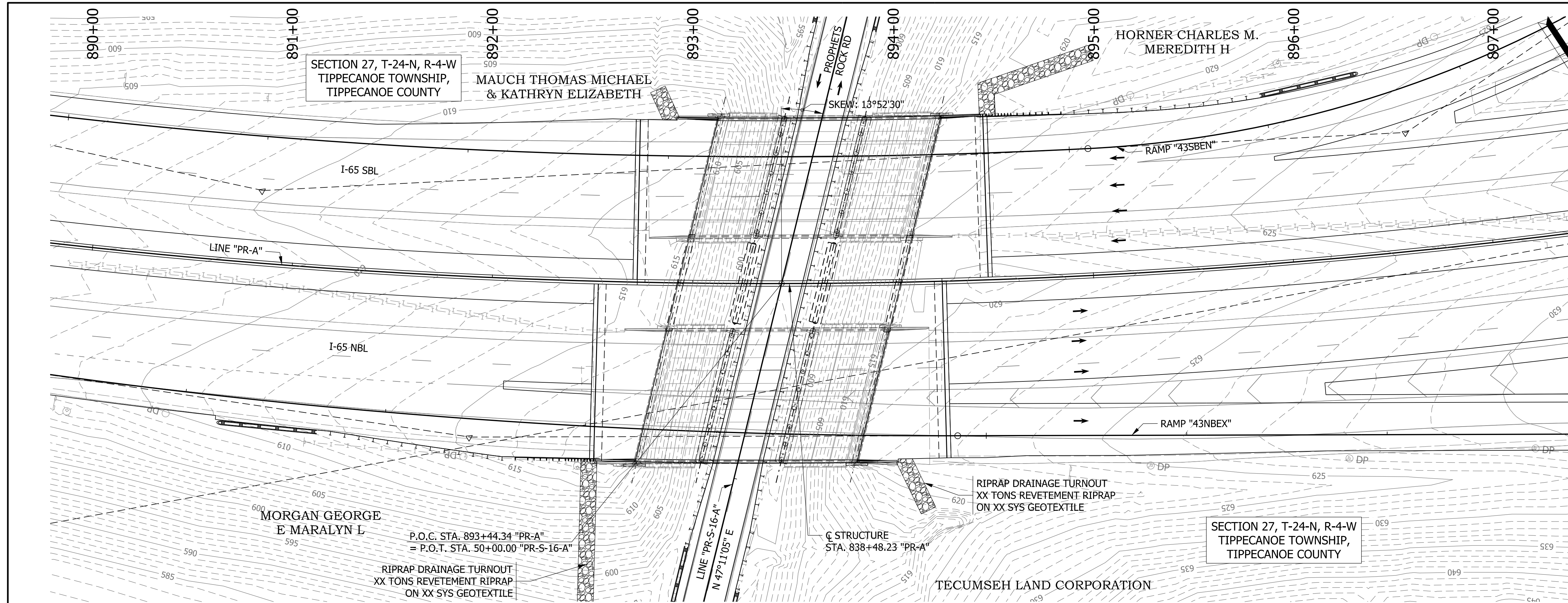
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: EAS	DRAWN: EAS	
CHECKED: DSB	CHECKED: DSB	

INDIANA DEPARTMENT OF TRANSPORTATION
 LAYOUT - NBL

SCALE	BRIDGE FILE
1"=30'	165-178-05485 CNBL & JCSBL
VERTICAL SCALE	DESIGNATION
1"=10'	2002116 & 2002117
SURVEY BOOK	SHEETS
ELECTRONIC	13 of 26
CONTRACT	PROJECT
R-42909	2001172



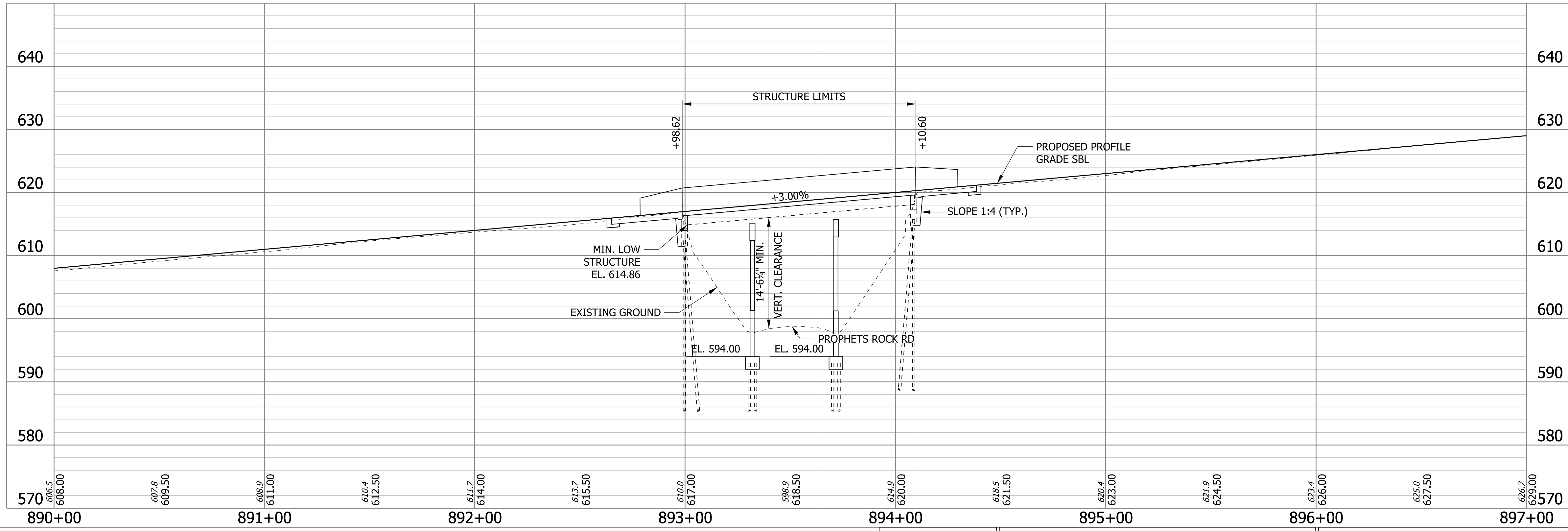
EXISTING STRUCTURE
 THE EXISTING SOUTHBOUND STRUCTURE IS A THREE SPAN, ROLLED STEEL BEAM BRIDGE. THE EXISTING BRIDGE IS SKEWED 13°52'30" RT. TO TANGENT WITH SPANS OF 32'-0", 40'-0", 37'-0", AND A CLEAR ROADWAY WIDTH OF 57'-11 1/4". THE STRUCTURE WAS BUILT IN 1968 WITH A DECK OVERLAY IN 1979 AND A DECK OVERLAY AND WIDENING IN 1991. EXISTING PLANS ARE ON FILE WITH INDIANA DEPARTMENT OF TRANSPORTATION UNDER BRIDGE FILE 165-180-5485J, 165-180-5485JA, AND 165-180-5485JB.

EARTHWORK TABULATION

FILL + 25%	XXX CYS
COMMON EXCAVATION	XXX CYS
BORROW	XXX CYS
EXCAVATION, FOUNDATION, UNCLASSIFIED	XXX CYS

HORIZONTAL CURVE DATA

P.I. STA. = 886+24.35
 $\Delta = 49^{\circ}55'06"$ LT.
 R = 3190.00'
 T = 1484.76'
 L = 2779.26'
 E = 328.61'
 SE = 5.40%



NOTE TO REVIEWER:
 Earthwork tabulations and quantities to be completed in future submittal.

CONTINUOUS COMPOSITE ROLLED STEEL BRIDGE
 3 SPANS: 32'-0", 40'-0", 37'-0"
 SKEW: 13°52'30" RT. TO TANGENT
 CLEAR ROADWAY 79'-9"
 I-65 SBL OVER PROPHETS ROCK ROAD
 TIPPECANOE COUNTY

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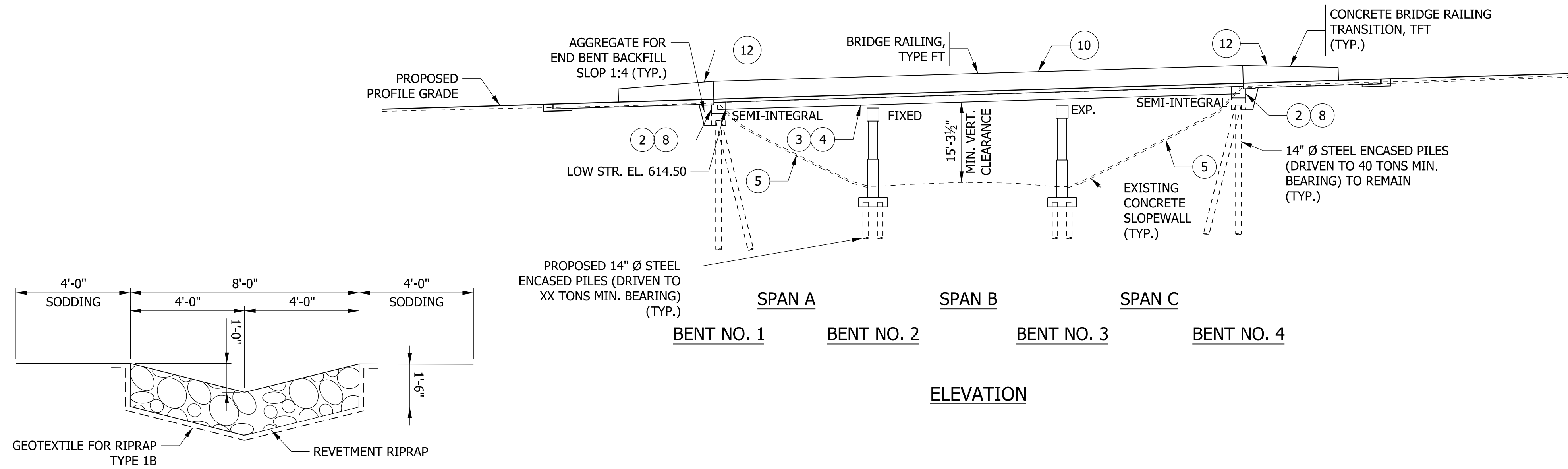
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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: EAS	DRAWN: EAS	
CHECKED: DSB	CHECKED: DSB	

INDIANA DEPARTMENT OF TRANSPORTATION
 LAYOUT - SBL

SCALE	BRIDGE FILE
1"=30'	165-178-05485 CNBL & JCSBL
VERTICAL SCALE	DESIGNATION
1"=10'	2002116 & 2002117
SURVEY BOOK	SHEETS
ELECTRONIC	14 of 26
CONTRACT	PROJECT
R-42909	2001172

STRUCTURE BUILT ON A +3.00% GRADE



ELEVATION

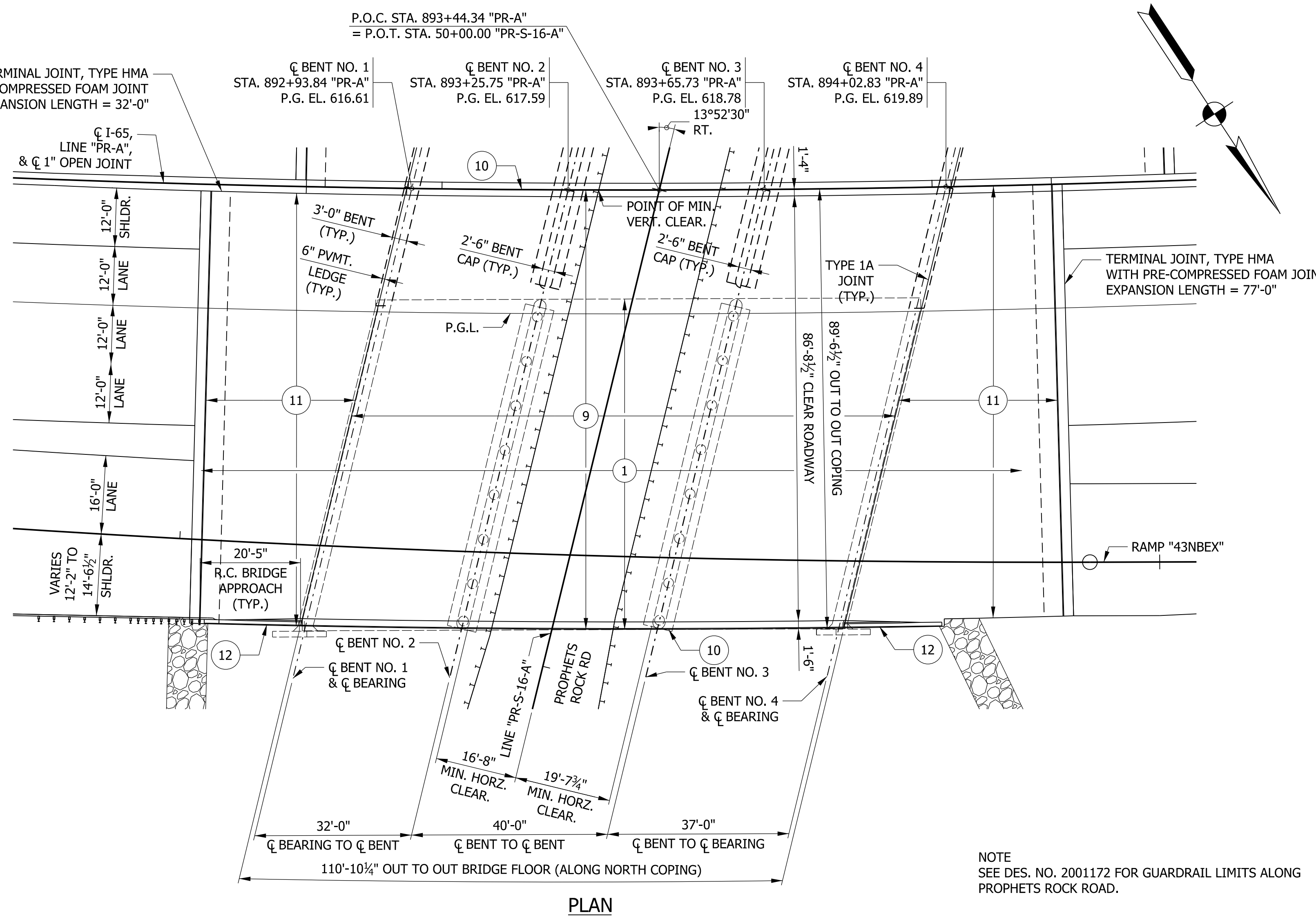
RIPRAP DRAINAGE TURNOUT
TYPICAL SECTION

SCALE: 1/4" = 1'-0"

TERMINAL JOINT, TYPE HMA WITH PRE-COMPRESSED FOAM JOINT EXPANSION LENGTH = 32'-0"

LEGEND

- 1 REMOVE EXISTING BRIDGE DECK, BRIDGE RAILING, REINFORCED CONCRETE APPROACH SLABS.
- 2 REMOVE PORTIONS OF THE MUDWALLS AND WINGWALL AND EXCAVATE TO THE DEPTH REQUIRED PER DETAILS.
- 3 REMOVE EXISTING DIAPHRAGMS AND CONNECTIONS AS REQUIRED PER DETAILS. CONSTRUCT NEW BOLTED DIAPHRAGMS.
- 4 JACK AND SUPPORT SUPERSTRUCTURE. TEMPORARY SHORING REQUIRED PRIOR TO ANY CONCRETE REMOVAL. SHORING SHALL BE DESIGNED FOR A DEAD LOAD OF XX KIPS PER GIRDER PLUS ANY CONSTRUCTION LOADS.
- 5 REMOVE PORTIONS OF SLOPEWALL AND CONSTRUCT NEW PORTIONS OF 4" SLOPEWALL.
- 6 CONSTRUCT NEW STEEL GIRDERS.
- 7 REMOVE ALL PAINT, GRIME, RUST, AND DIRT BY SCRAPING, WIRE BRUSH, SAND/SODA BLASTING, AND/OR MECHANICAL GRINDING ALL EXISTING STRUCTURAL STEEL AND THEN PAINT.
- 8 CONSTRUCT SEMI-INTEGRAL BENTS AT NO. 1 AND NO. 4.
- 9 CONSTRUCT NEW REINFORCED CONCRETE BRIDGE DECK.
- 10 CONSTRUCT NEW BRIDGE RAILING, TYPE FT.
- 11 CONSTRUCT NEW BRIDGE APPROACH SLABS.
- 12 CONSTRUCT NEW BRIDGE RAILING TRANSITIONS, TYPE TFT.
- 13 SURFACE SEAL THE BRIDGE DECK, COPINGS, BRIDGE RAILINGS, BRIDGE TRANSITIONS, AND REINFORCED BRIDGE APPROACH SLAB (APPROX. XX,XXX SFT).



PLAN

NOTE
SEE DES. NO. 2001172 FOR GUARDRAIL LIMITS ALONG PROPHETS ROCK ROAD.

GENERAL NOTES

PLANS FOR EXISTING STRUCTURE ARE ON FILE IN THE CENTRAL OFFICE AS BRIDGE FILE I-65-181-5486, I-65-181-5486A AND I-65-181-5487B AND ARE AVAILABLE UPON REQUEST.

WHERE WORK IS TO BE FITTED TO OLD WORK, THE CONTRACTOR SHALL CHECK ALL DIMENSIONS AND CONDITIONS IN THE FIELD AND REPORT ANY ERRORS TO THE ENGINEER AND ASSUME RESPONSIBILITY FOR THEIR CORRECTNESS AND THE FIT OF THE NEW PART TO THE OLD.

REINFORCING STEEL COVERING SHALL BE 2 1/2" IN THE TOP AND 1" MINIMUM IN THE BOTTOM OF THE FLOOR SLAB, 3" IN THE SUBSTRUCTURE EXCEPT THE BOTTOM STEEL WHICH SHALL BE 4" AND 2" IN ALL OTHER PARTS, UNLESS OTHERWISE NOTED.

CLEAN AND SURFACE SEAL ALL EXPOSED FACES OF THE CONCRETE RAILING, COPING, AND APPROACH SLABS.

ALL PAVEMENT MARKINGS AND SNOWPLOWABLE RAISED PAVEMENT MARKINGS DISTURBED DURING CONSTRUCTION SHALL BE REPLACED IN KIND.

DESIGN DATA

SUPERSTRUCTURE AND SUBSTRUCTURE DESIGN FOR HS-20 LOADING, IN ACCORDANCE WITH THE AASHTO BRIDGE DESIGN SPECIFICATIONS, SEVENTEENTH EDITION, 2002, AND ITS SUBSEQUENT INTERMS.

DEAD LOAD:

DESIGNED FOR ACTUAL DEAD LOAD PLUS 35 LBS/SFT FUTURE WEARING SURFACE SLAB DESIGNED WITH 1/2" MONOLITHIC WEARING SURFACE.

LIVE LOAD:

SUPERSTRUCTURE DESIGNED FOR HS20-44 LIVE LOAD WITH IMPACT.

MATERIALS:

CLASS "C" CONCRETE f_c = 4,000 psi
 CLASS "A" CONCRETE f_c = 3,500 psi
 CLASS "B" CONCRETE f_c = 3,000 psi
 REINFORCING BARS F_y = 60,000 psi

CONSTRUCTION LOADING

THE EXTERIOR GIRDER HAS BEEN CHECKED FOR STRENGTH, DEFLECTION AND OVERTURNING USING THE CONSTRUCTION LOADS SHOWN BELOW. CANTILEVER OVERHANG BRACKETS WERE ASSUMED FOR SUPPORT OF THE DECK OVERHANG PAST THE EDGE OF THE EXTERIOR GIRDER. THE FINISHING MACHINE WAS ASSUMED TO BE SUPPORTED 6" OUTSIDE THE VERTICAL COPING FORM. THE TOP OVERHANG BRACKETS WERE ASSUMED TO BE LOCATED 6" PAST THE EDGE OF THE VERTICAL COPING FORM. THE BOTTOM OVERHANG BRACKETS WERE ASSUMED TO BE BRACED AGAINST THE INTERSECTION OF THE GIRDER BOTTOM FLANGE AND WEB.

DECK FALSEWORK LOADS:

DESIGNED FOR 15 PSF FOR PERMANENT METAL STAY IN PLACE DECK FORMS, REMOVABLE DECK FORMS AND 2 FOOT WIDTH EXTERIOR WALKWAY.

CONSTRUCTION LIVE LOAD:

DESIGNED FOR 20 LBS/SFT EXTENDING 2'-0" PAST THE EDGE OF COPING AND 75 LBS/FOOT VERTICAL FORCE APPLIED AT A DISTANCE OF 6" OUTSIDE THE FACE OF COPING OVER A 30'-0" LENGTH OF THE DECK CENTERED WITH FINISHING MACHINE.

FINISHING MACHINE LOAD:

4500 LBS DISTRIBUTED OVER 10'-0" ALONG THE COPING.

WIND LOAD:

STRUCTURE DESIGNED FOR 70 MPH HORIZONTAL WIND LOADING IN ACCORDANCE WITH AASHTO LRFD 3.8.1.

CONTINUOUS COMPOSITE ROLLED STEEL BRIDGE
 3 SPANS: 32'-0", 40'-0", 37'-0"
 SKEW: 13°52'30" RT. TO TANGENT
 CLEAR ROADWAY 86'-8 1/2"
 I-65 NBL OVER PROPHETS ROCK ROAD
 TIPPECANOE COUNTY

NOTE TO REVIEWER
 This is a Pavement Alternate Bid Contract. Terminal joints will depend on the pavement type used.

NOTE TO REVIEWER
 Proposed pile tip elevations and capacity will be provided in a future submittal.

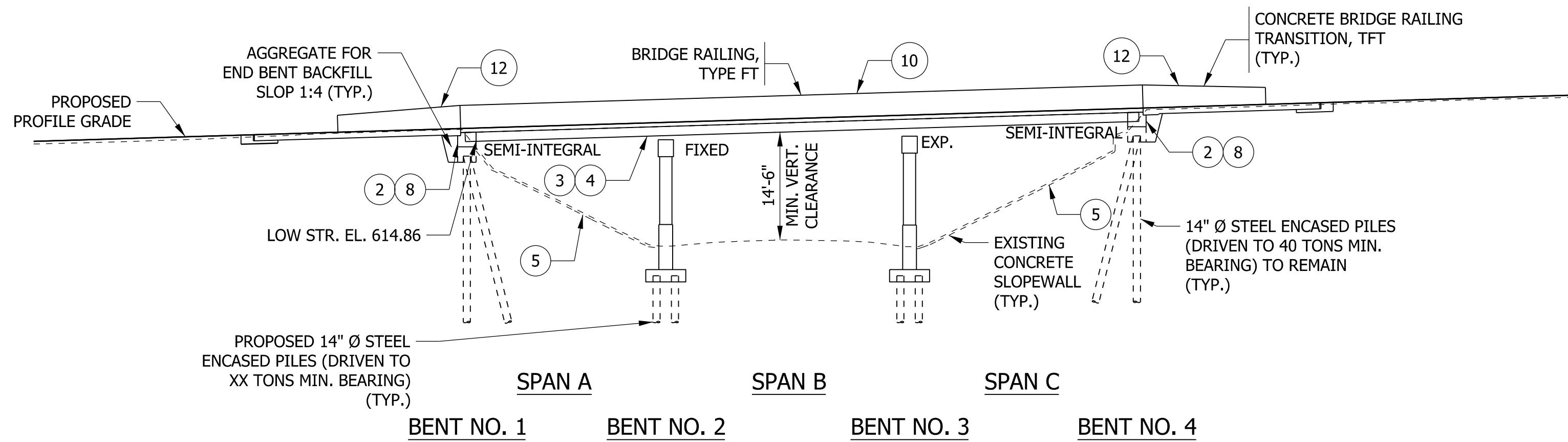
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 NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: EAS	DRAWN: EAS	
CHECKED: DSB	CHECKED: DSB	

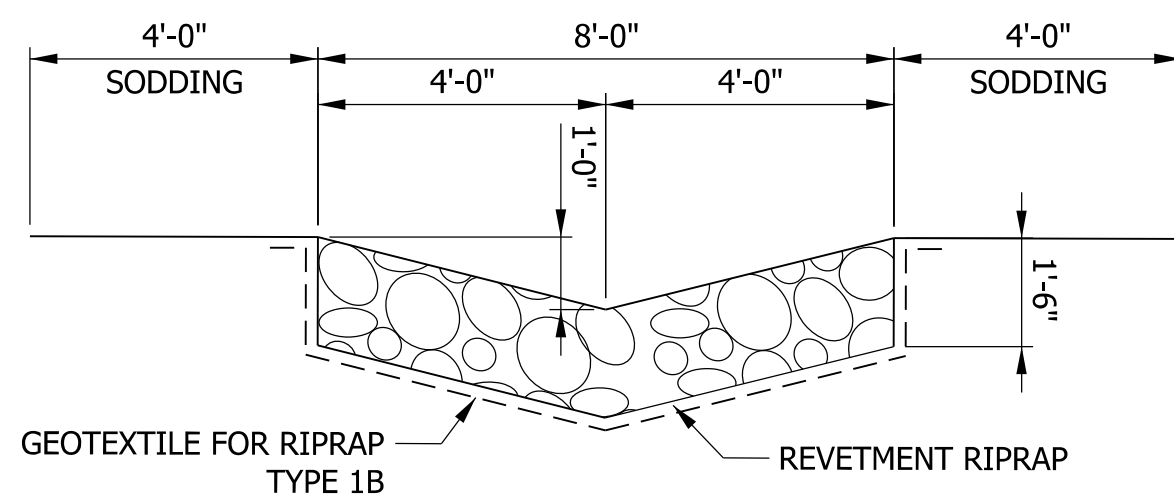
INDIANA
 DEPARTMENT OF TRANSPORTATION
 GENERAL PLAN - NBL

HORIZONTAL SCALE	BRIDGE FILE
1/16"=1'-0"	165-178-05485 CNBL & JCSBL
VERTICAL SCALE	DESIGNATION
1/16"=1'-0"	2002116 & 2002117
SURVEY BOOK	SHEETS
ELECTRONIC	15 of 26
CONTRACT	PROJECT
R-42909	2001172

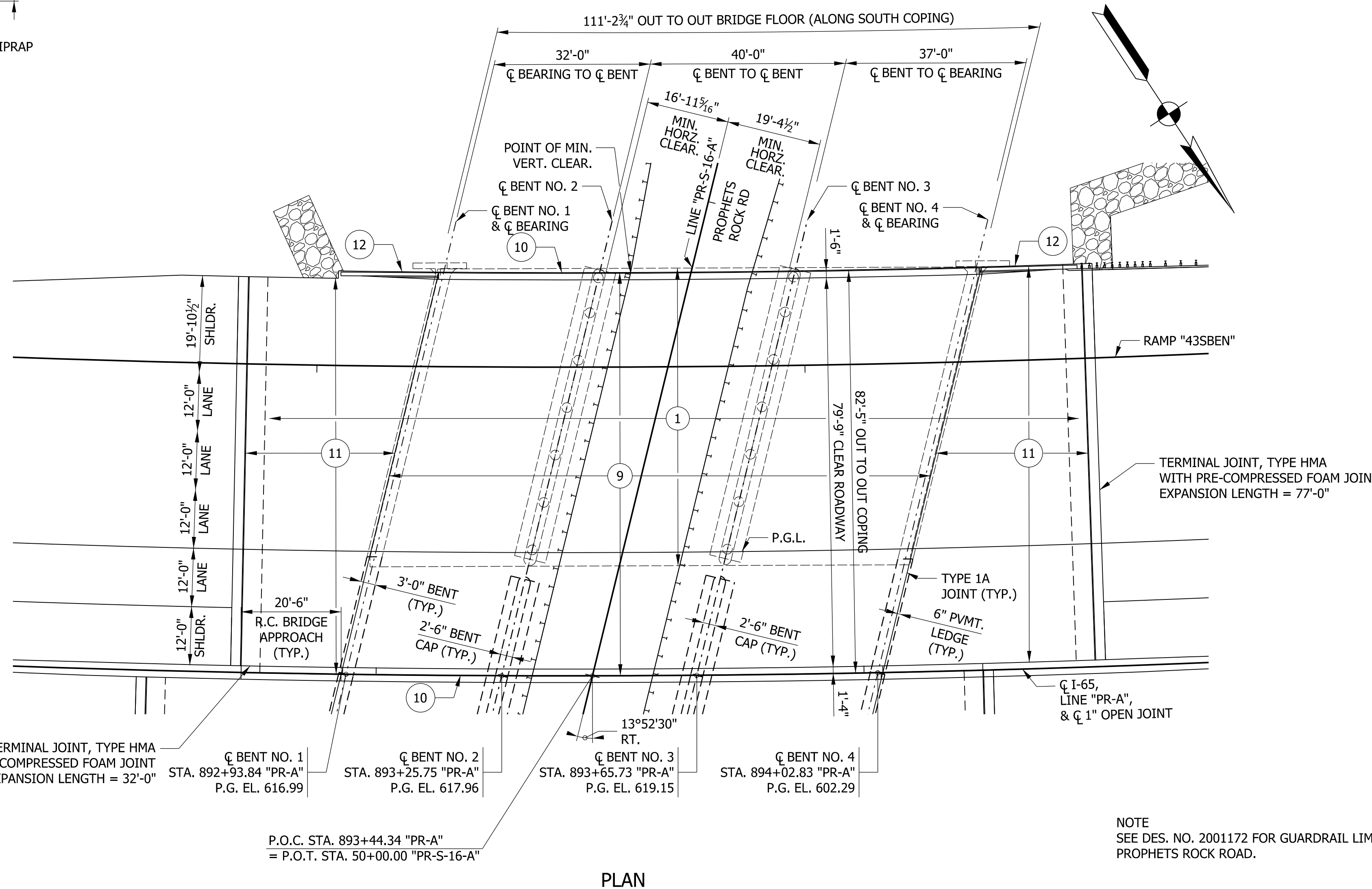
STRUCTURE BUILT ON A +3.00% GRADE



ELEVATION



RIPRAP DRAINAGE TURNOUT
TYPICAL SECTION
SCALE: 1/4" = 1'-0"



PLAN

LEGEND

- 1 REMOVE EXISTING BRIDGE DECK, BRIDGE RAILING, REINFORCED CONCRETE APPROACH SLABS.
- 2 REMOVE PORTIONS OF THE MUDWALLS AND WINGWALL AND EXCAVATE TO THE DEPTH REQUIRED PER DETAILS.
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- 10 CONSTRUCT NEW BRIDGE RAILING, TYPE FT.
- 11 CONSTRUCT NEW BRIDGE APPROACH SLABS.
- 12 CONSTRUCT NEW BRIDGE RAILING TRANSITIONS, TYPE TFT.
- 13 SURFACE SEAL THE BRIDGE DECK, COPINGS, BRIDGE RAILINGS, BRIDGE TRANSITIONS, AND REINFORCED BRIDGE APPROACH SLAB (APPROX. XX,XXX SFT).

NOTE TO REVIEWER

This is a Pavement Alternate Bid Contract. Terminal joints will depend on the pavement type used.

NOTE TO REVIEWER

Proposed pile tip elevations and capacity will be provided in a future submittal.

DRAFT
NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: EAS	DRAWN: EAS	
CHECKED: DSB	CHECKED: DSB	

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN - SBL

HORIZONTAL SCALE	BRIDGE FILE
1/16"=1'-0"	165-178-05485 CNBL & JCSBL
VERTICAL SCALE	DESIGNATION
1/16"=1'-0"	2002116 & 2002117
SURVEY BOOK	SHEETS
ELECTRONIC	16 of 26
CONTRACT	PROJECT
R-42909	2001172

GENERAL NOTES

PLANS FOR EXISTING STRUCTURE ARE ON FILE IN THE CENTRAL OFFICE AS BRIDGE FILE 165-180-5485J, 165-180-5485JA AND 165-180-5485JB AND ARE AVAILABLE UPON REQUEST.

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DESIGN DATA

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LIVE LOAD:

SUPERSTRUCTURE DESIGNED FOR HS20-44 LIVE LOAD WITH IMPACT.

MATERIALS:

CLASS "C" CONCRETE	f _c = 4,000 psi
CLASS "A" CONCRETE	f _c = 3,500 psi
CLASS "B" CONCRETE	f _c = 3,000 psi
REINFORCING BARS	F _y = 60,000 psi

CONSTRUCTION LOADING

THE EXTERIOR GIRDER HAS BEEN CHECKED FOR STRENGTH, DEFLECTION AND OVERTURNING USING THE CONSTRUCTION LOADS SHOWN BELOW. CANTILEVER OVERHANG BRACKETS WERE ASSUMED FOR SUPPORT OF THE DECK OVERHANG PAST THE EDGE OF THE EXTERIOR GIRDER. THE FINISHING MACHINE WAS ASSUMED TO BE SUPPORTED 6" OUTSIDE THE VERTICAL COPING FORM. THE TOP OVERHANG BRACKETS WERE ASSUMED TO BE LOCATED 6" PAST THE EDGE OF THE VERTICAL COPING FORM. THE BOTTOM OVERHANG BRACKETS WERE ASSUMED TO BE BRACED AGAINST THE INTERSECTION OF THE GIRDER BOTTOM FLANGE AND WEB.

DECK FALSEWORK LOADS:

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CONSTRUCTION LIVE LOAD:

DESIGNED FOR 20 LBS/SFT EXTENDING 2'-0" PAST THE EDGE OF COPING AND 75 LBS/FOOT VERTICAL FORCE APPLIED AT A DISTANCE OF 6" OUTSIDE THE FACE OF COPING OVER A 30'-0" LENGTH OF THE DECK CENTERED WITH FINISHING MACHINE.

FINISHING MACHINE LOAD:

4500 LBS DISTRIBUTED OVER 10'-0" ALONG THE COPING.

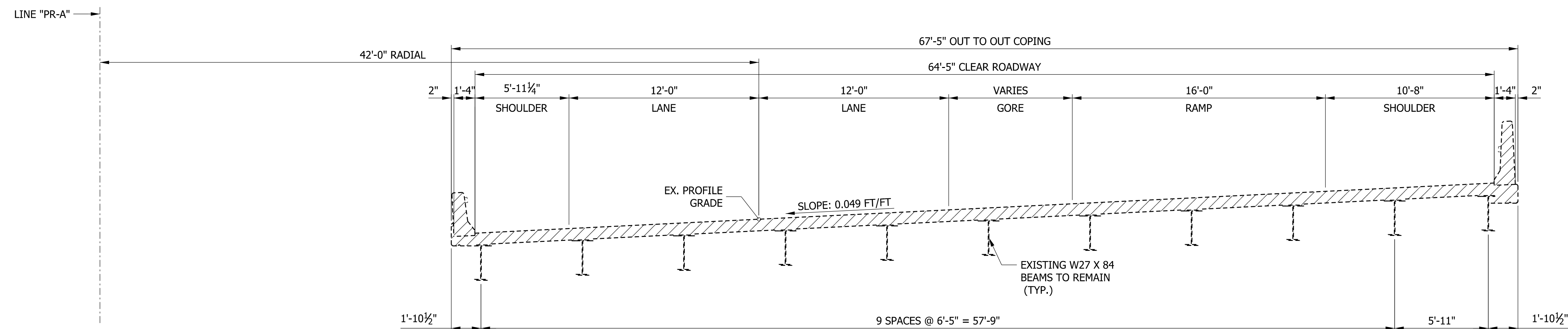
WIND LOAD:

STRUCTURE DESIGNED FOR 70 MPH HORIZONTAL WIND LOADING IN ACCORDANCE WITH AASHTO LRFD 3.8.1.

CONTINUOUS COMPOSITE ROLLED STEEL BRIDGE
3 SPANS: 32'-0", 40'-0", 37'-0"
SKEW: 13°52'30" RT. TO TANGENT
CLEAR ROADWAY 79'-9"
I-65 SBL OVER PROPHETS ROCK ROAD
TIPPECANOE COUNTY

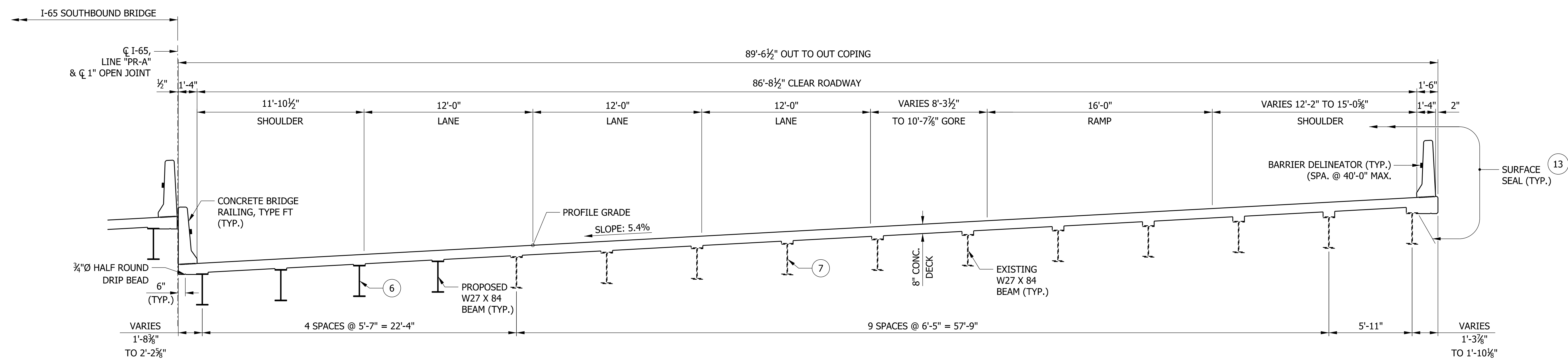
NOTE
SEE DES. NO. 2001172 FOR GUARDRAIL LIMITS ALONG
PROPHETS ROCK ROAD.

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EXISTING TYPICAL SECTION - NBL

INDICATES LIMITS OF REMOVAL



PROPOSED TYPICAL SECTION - NBL

CONTINUOUS COMPOSITE ROLLED STEEL BRIDGE
 3 SPANS: 32'-0", 40'-0", 37'-0"
 SKEW: 13°52'30" RT. TO TANGENT
 CLEAR ROADWAY 86'-8 1/2"
 I-65 NBL OVER PROPHETS ROCK ROAD
 TIPPECANOE COUNTY

NOTE:
See previous sheets for legend.

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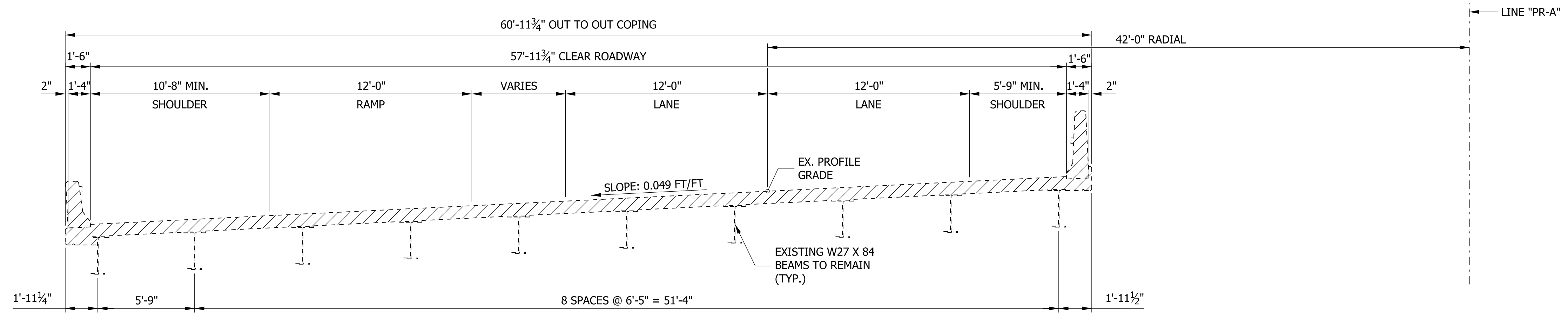
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RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ EAS _____	DRAWN: _____ EAS _____	
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INDIANA
DEPARTMENT OF TRANSPORTATION

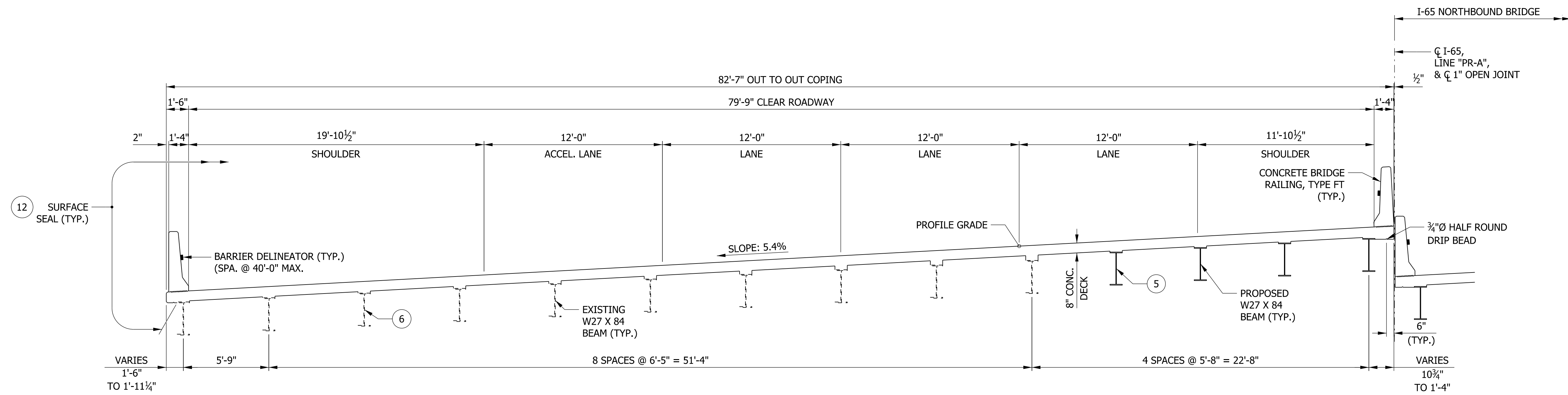
GENERAL PLAN - NBL

HORIZONTAL SCALE 1/4"=1'-0"	BRIDGE FILE 165-178-05485 CNBL & JCSBL
VERTICAL SCALE 1/4"=1'-0"	DESIGNATION 2002116 & 2002117
SURVEY BOOK ELECTRONIC	SHEETS 17 of 26
CONTRACT R-42909	PROJECT 2001172



EXISTING TYPICAL SECTION - SBL

INDICATES LIMITS OF REMOVAL



PROPOSED TYPICAL SECTION - SBL

CONTINUOUS COMPOSITE ROLLED STEEL BRIDGE
 3 SPANS: 32'-0", 40'-0", 37'-0"
 SKEW: 13°52'30" RT. TO TANGENT
 CLEAR ROADWAY 79'-9"
 I-65 SBL OVER PROPHETS ROCK ROAD
 TIPPECANOE COUNTY

NOTE:
See previous sheets for legend.

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 model-Sheet42
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 NOT FOR CONSTRUCTION

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: _____ EAS _____	DRAWN: _____ EAS _____	
CHECKED: _____ DSB _____	CHECKED: _____ DSB _____	

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN - SBL

HORIZONTAL SCALE	BRIDGE FILE
1/4"=1'-0"	165-178-05485 CNBL & JCSBL
VERTICAL SCALE	DESIGNATION
1/4"=1'-0"	2002116 & 2002117
SURVEY BOOK	SHEETS GP-04
ELECTRONIC	18 of 26
CONTRACT	PROJECT
R-42909	2001172

Appendix C: Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

June 12, 2020

Sample Early
Coordination Letter

Christie Stanifer
Environmental Coordinator
Indiana Department of Natural Resources
402 West Washington St., Room W273
Indianapolis, IN 46204

Re: Des. No. 2001172 (Lead)
I-65 Added Travel Lanes Project
Tippecanoe County, Indiana

Dear Ms. Stanifer:

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intend to proceed with an added travel lanes project along Interstate 65 (I-65). This letter is part of the early coordination phase of the environmental review process. We request comments from you within your area of expertise regarding any potential environmental or community effects associated with this proposed project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of the project's environmental effects.

Project Location: The project is located along I-65 from approximately 1.33 miles north of SR 25 to approximately 2.43 miles north of SR 43 near Lafayette and Battle Ground in Tippecanoe County, Indiana. Additional length north and south of these limits is included in the project area for median crossovers for maintenance of traffic. The project also includes the deck replacement of the CR 725 N. bridge over I-65, which is approximately 390 feet north of the added travel lane limits. More specifically, the project is located in Sections 17, 20, 21, 27, and 28 of Township 24 North, Range 4 West; and Burnett's Reserve in Tippecanoe Township. The adjacent land use includes residential, agricultural, recreational, and forest. Maps and photos of the project area are included as attachments.

Existing Conditions: This section of I-65 currently has two 12-foot travel lanes in each direction separated by a 40 to 50-foot wide grass median. It has a 4-foot paved inside shoulder and a 10-foot paved (12-foot usable) outside shoulder. I-65 has a posted speed limit of 65 miles per hour (mph) from the southern limit to SR 43 and 70 mph from SR 43 to the northern limit. I-65 is classified as a rural interstate, is part of the U.S. National Highway System (NHS), and is on the National Truck Network.

Purpose and Need: The following needs are identified for the project:

1. The pavement on I-65 is in poor for condition and nearing the end of its lifecycle. The existing pavement was originally constructed in 1966 as concrete, with multiple asphalt

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INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

overlays completed over the following decades. The last major pavement rehabilitation project was completed in 2000.

2. The condition of the I-65 bridges over 9th Street/CSX Railroad/Burnett Creek/Wabash Heritage Trail, I-65 bridges over Prophets Rock Road, and CR 725 N. over I-65 are deteriorating and require repair.
3. Level of Service (LOS) provides a common “grading” scale for describing traffic congestion on roadways, from LOS A (best) to LOS F (worst). LOS A represents near ideal traffic flow, while LOS F represents a breakdown of the traffic flow. Preliminary traffic analysis shows that I-65 currently performs at LOS C, which is considered acceptable. However, it is anticipated that I-65 from the Wabash River Bridge to SR 43 will perform at LOS D by 2038, which is considered the threshold for poor traffic performance.
4. INDOT’s Long Range Transportation Plan (2018-2045 Transportation Needs Report)⁵ identifies expanding I-65 to a 6-lane section (3 lanes in each direction) as a major corridor improvement that is critical to the mobility and economic activity throughout the state. I-65 is a Statewide Mobility Corridor and is a priority for automobile and freight travel.

The purpose of the project is to improve pavement quality, improve bridge condition, reduce congestion, and address INDOT long-term planning goals.

Proposed Project: The following activities are included in the project:

1. Travel lane and shoulder pavement replacement;
2. Reconstruction of the SR 43 entrance and exit ramps;
3. Addition of a travel lane in each direction in the median with traffic separated by a concrete barrier;
4. Bridge deck replacement and widening of I-65 northbound bridge and bridge deck overlay and widening of I-65 southbound bridge over 9th Street/CSX Railroad/Burnett Creek/Wabash Heritage Trail (Des. Nos. 1592725 and 1592726);
5. Bridge deck replacement and widening of I-65 northbound and southbound bridges over Prophets Rock Road (Des. Nos. 1592704 and 1592705);
6. Bridge deck replacement and widening of I-65 northbound and southbound bridges over SR 43 (Note: the environmental impacts of work to these bridges were previously

⁵ https://www.in.gov/indot/files/INDOT_LRTP_FINAL_FullDocWebPost.pdf



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

documented in a separate Categorical Exclusion (CE) document under Des. Nos. 1601088 and 1601090);

7. Bridge deck replacement of CR 725 N. bridge over I-65 (Des. No. 1500644);
8. Replacement of culverts crossing under I-65 and/or construction of median drains, culverts, and detention basins for roadway drainage.

The bridges over Burnett Creek, south of CR 600 N., will have a deck replacement and be widened as part of a separate project prior to this added travel lanes project. Environmental impacts of that bridge work will be documented in a separate CE document under Des Nos. 1601091 and 1601092.

Right-of-Way: No new right-of-way will be required for the project. All other work is anticipated to be within existing right-of-way.

Maintenance of Traffic: The maintenance of traffic plan will include maintaining two lanes of traffic in each direction through the majority of construction. Intermittent lane restrictions will be implemented on I-65. Access to and from the SR 43 interchange will be provided during the majority of construction; however, intermittent closures will be required to reconstruct the existing ramps. 9th Street, Prophets Rock Road, and CR 725 will be closed at I-65 for approximately three to six months and detours will be required for through traffic.

Surrounding Resources: Land use in the vicinity of the project is primarily residential, agricultural, recreational, and forest.

The Wabash Heritage Trail crosses under I-65 along the west side of Burnett Creek under the bridge over 9th Street and the CSX railroad. The trail is approximately 13 miles long, starting at Tippecanoe Battlefield in Battle Ground and ending at Fort Ouiatenon. There is a planned trail, the Prophetstown Bike Trail, mapped along Swisher Road over I-65. Prophetstown State Park, Tippecanoe Battlefield Memorial Park, and the Boehning Nature Reserve are mapped adjacent to the project area.

I-65 crosses two mapped streams within the project area. It crosses Burnett Creek at two locations, one south of the SR 43 interchange and one south of the CR 600 N. bridge over I-65. It also crosses an Unnamed Tributary (UNT) of the North Fork of Burnett Creek south of the CR 725 N. bridge over I-65. Floodplains are mapped with the Burnett Creek crossings and within the southern project limits as associated with the Wabash River. Impacts to Burnett Creek and the UNT of the North Fork of Burnett Creek, and floodplains are possible. A waters/wetland determination will be performed to determine the presence of jurisdictional waterways and wetlands within the project area. All applicable permits will be obtained before construction begins. Tree and brush clearing are anticipated to complete construction activities.



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Coordination will be performed to verify the location of surrounding utilities and determine any impacts.

Comments Request: You are asked to review this information and provide any comments you may have relative to the anticipated effects of the project on areas which you have jurisdiction or special expertise. Please send your comments to Kia Gillette, of HNTB Corporation, at kgillette@hntb.com or 317-917-5240. Should we not receive your response **within thirty (30) calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request.

If you have any questions regarding this matter, please feel free to contact Kia Gillette, of HNTB Corporation, at kgillette@hntb.com or 317-917-5240; or Arshad Ahmed, INDOT Project Manager, at arahmed@indot.in.gov or 765-361-5258. Thank you in advance for your input.

Sincerely,

Kia M. Gillette
HNTB CORPORATION

- Attachments: General Project Area Map
- Project Location Map
- USGS 7.5 Minute Topographic Quad Map
- Project Area Aerial Map Index
- Project Area Aerial/Photo Location Map Series
- Project Location Photographs

Attachments were removed to avoid duplication. Attachments can be found in Appendix B of this CE document.

Cc:

- Arshad Ahmed, INDOT Project Manager
- Austin Hastings, HNTB
- Christina Bowman, INDOT Crawfordsville District Environmental
- Ron Bales, INDOT Environmental Policy Office
- Rickie Clark, INDOT Public Hearings
- Kari Carmany-George, Federal Highway Administration
- Paul Lehmann, US Department of Housing and Urban Development
- Elizabeth McCloskey, US Fish and Wildlife Service



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Indiana Department of Environmental Management Online Form
Alisha Turnbow, Indiana Department of Environmental Management Groundwater Section
Sallie Fahey, Area Plan Commission of Tippecanoe County and Floodplain Administrator
Gregory McKay, US Army Corps of Engineers
Dale Brier, Indiana Department of Natural Resources, Division of Outdoor Recreation
Christie Stanifer, Indiana Department of Natural Resources, Division of Fish and Wildlife
Brian Royer, Indiana Department of Natural Resources, Division of Oil and Gas
National Park Service
Kim Riley, Indiana State Police
Allen Nail, Tippecanoe County Parks Department
Prophetstown State Park
Ron Holladay, Town of Battle Ground Town Supervisor and MS4 Coordinator
Zach Beasley, Tippecanoe County Surveyor, MS4 Coordinator
Robert Goldsmith, Tippecanoe County Sheriff
Stewart Kline, Tippecanoe County Highway Department
Wm. Smokey Anderson, Tippecanoe Emergency Management Agency
David Byers, Tippecanoe County Commissioner
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Tony Roswarski, Mayor City of Lafayette
John Dennis, Mayor City of West Lafayette
Mitch Daniels, Purdue University
West Lafayette Apostolic Christian Church
Immanuel Reformed Presbyterian Church
Battle Ground Bible Church
Indiana Geological Survey
Niches Land Trust
Heritage Aggregates LLC



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Eric Holcomb, Governor
Joe McGuinness, Commissioner

October 26, 2020

Early Coordination Letter to IDNR
State Parks & Reservoirs

Indiana Department of Natural Resources
State Parks & Reservoirs
402 West Washington St., Room W298
Indianapolis, IN 46204

Re: Des. No. 2001172 (Lead)
I-65 Added Travel Lanes Project
Tippecanoe County, Indiana

To Whom it May Concern:

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intend to proceed with an added travel lanes project along Interstate 65 (I-65). This letter is part of the early coordination phase of the environmental review process. We request comments from you within your area of expertise regarding any potential environmental or community effects associated with this proposed project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of the project’s environmental effects.

Project Location: The project is located along I-65 from approximately 1.33 miles north of SR 25 to approximately 2.43 miles north of SR 43 near Lafayette and Battle Ground in Tippecanoe County, Indiana. Additional length north and south of these limits is included in the project area for median crossovers for maintenance of traffic. The project also includes the deck replacement of the CR 725 N. bridge over I-65, which is approximately 390 feet north of the added travel lane limits. More specifically, the project is located in Sections 17, 20, 21, 27, and 28 of Township 24 North, Range 4 West; and Burnett’s Reserve in Tippecanoe Township. The adjacent land use includes residential, agricultural, recreational, and forest. Maps and photos of the project area are included as attachments.

Existing Conditions: This section of I-65 currently has two 12-foot travel lanes in each direction separated by a 40 to 50-foot wide grass median. It has a 4-foot paved inside shoulder and a 10-foot paved (12-foot usable) outside shoulder. I-65 has a posted speed limit of 65 miles per hour (mph) from the southern limit to SR 43 and 70 mph from SR 43 to the northern limit. I-65 is classified as a rural interstate, is part of the U.S. National Highway System (NHS), and is on the National Truck Network.

Purpose and Need: The following needs are identified for the project:

1. The pavement on I-65 is in poor for condition and nearing the end of its lifecycle. The existing pavement was originally constructed in 1966 as concrete, with multiple asphalt

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Sincerely,

Kia M. Gillette
HNTB CORPORATION

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Gregory McKay, US Army Corps of Engineers
Dale Brier, Indiana Department of Natural Resources, Division of Outdoor Recreation
Christie Stanifer, Indiana Department of Natural Resources, Division of Fish and Wildlife
Brian Royer, Indiana Department of Natural Resources, Division of Oil and Gas
National Park Service
Kim Riley, Indiana State Police
Allen Nail, Tippecanoe County Parks Department
Prophetstown State Park
Ron Holladay, Town of Battle Ground Town Supervisor and MS4 Coordinator
Zach Beasley, Tippecanoe County Surveyor, MS4 Coordinator
Robert Goldsmith, Tippecanoe County Sheriff
Stewart Kline, Tippecanoe County Highway Department
Wm. Smokey Anderson, Tippecanoe Emergency Management Agency
David Byers, Tippecanoe County Commissioner
Scott Hanback, Tippecanoe School Corporation
Tony Roswarski, Mayor City of Lafayette
John Dennis, Mayor City of West Lafayette
Mitch Daniels, Purdue University
West Lafayette Apostolic Christian Church
Immanuel Reformed Presbyterian Church
Battle Ground Bible Church
Indiana Geological Survey
Niches Land Trust
Heritage Aggregates LLC
IDNR State Parks & Reservoirs

From: [Tony Roswarski](#)
To: [Kia Gillette](#); [Website-Mayors Office](#)
Subject: RE: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination
Date: Friday, June 12, 2020 9:58:26 AM
Attachments: [image012.png](#)
[image013.png](#)
[image014.png](#)
[image015.png](#)

Thank-you

Tony Roswarski

Mayor, City of Lafayette

phone 765.807.1002 | hotline 765.807.1004

address 20 N. 6th St., Lafayette IN 47901



From: Kia Gillette <kgillette@HNTB.com>
Sent: Friday, June 12, 2020 9:31 AM
To: Website-Mayors Office <web-mayor@lafayette.in.gov>
Subject: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination

Mimecast Attachment Protection has deemed this file to be safe, but always exercise caution when opening files.

Dear Mayor Roswarski,

Attached is an early coordination letter for the I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172).

Please let me know if you have any questions.

Thank you,
Kia

Kia Gillette

Environmental Project Manager

Tel (317) 917-5240 Cell (317) 695-0825 Email kgillette@hntb.com

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From: [Allen Nail](#)
To: [Kia Gillette](#); [Wade Garriott](#)
Subject: Re: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination
Date: Friday, June 12, 2020 10:07:47 AM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)

Hello, Kia.

You may recall that our work trails crossed on the bridge pier repair/Heritage Trail matter on 231 just sw of downtown Lafayette, but if I recall correctly you have changed firms since that time. I hope that you are doing well.

I am copying Mr. Wade Garriott, who is our Wabash Heritage Trail Manager, and I'd like to include him in a conversation regarding the impact of this project on the Heritage Trail. It seems pretty clear that with the construction and the adjacent obstacles of Burnett Creek and the railroad, we would be looking at a full closure of the trail for some period of time. The overall trail impact of the project would depend on the design of the added lanes, particularly on the upstream side of the interstate corridor. The course of the creek is a limiting factor in that direction.

We would be happy to speak with you in the next couple of weeks if you would like to suggest a couple of preferred dates and times.

Best Regards and Be Well,
Allen

Please see Appendix I, pages 47-54 for additional coordination with the Tippecanoe County Parks and Recreation Department regarding the Wabash Heritage Trail.

From: Kia Gillette <kgillette@HNTB.com>
Sent: Friday, June 12, 2020 9:15 AM
To: ParksandRec <tcparks@tippecanoe.in.gov>
Subject: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination

Dear Mr. Nail,

Attached is an early coordination letter for the I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172).

We think it would helpful to have a call to discuss possible temporary impacts to the Wabash Heritage Trail under the I-65 bridge as a result of the project. Would you be open to a call to discuss this in the next couple of weeks?

From: [Hurst, Allen A](#)
To: [Kia Gillette](#)
Cc: [Brier, Dale](#); [Bronson, Bob](#)
Subject: FW: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination
Date: Tuesday, June 16, 2020 3:31:41 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[Early Coordination Letter - Des. No. 2001172 I65 Added Travel Lanes Brier.pdf](#)

Early Coordination Review

Designation #: 2001172

Description: I-65 Added Travel Lanes Project

Hello,

The project as submitted will not impact a LWCF site or a Designated Natural, Scenic, or Recreational River. Should the location or scope of the project change that alters the nearby LWCF sites (Prophetstown State Park and Tippecanoe Battlefield Memorial Park) in any way that is not intended for outdoor recreation use, the DNR, Division of Outdoor Recreation must be contacted at 317-232-4075 or bbronson@dnr.in.gov.

For our reference- the nearby LWCF site project numbers are:

1800275

1800532

Thank you,

ALLEN HURST
TRAILS COORDINATOR
DIVISION OF OUTDOOR RECREATION
DEPARTMENT OF NATURAL RESOURCES | DNR.IN.GOV
402 W. WASHINGTON ST. ROOM W271, INDIANAPOLIS, IN 46204
P: (317)232-4070 | AHURST@DNR.IN.GOV

* Please let us know about the quality of our service by taking this brief [customer survey](#).

From: Brier, Dale
Sent: Friday, June 12, 2020 9:11 AM
To: Hurst, Allen A <AHurst@dnr.IN.gov>
Subject: FW: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) ŸV Early Coordination

From: Kia Gillette <kgillette@HNTB.com>

From: [Scott Hanback](#)
To: [Kia Gillette](#)
Subject: Re: [EXTERNAL] I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination
Date: Tuesday, June 16, 2020 4:12:52 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)

Received. Thank you for the information.

Scott Hanback
Tippecanoe School Corporation

On Fri, Jun 12, 2020 at 9:29 AM Kia Gillette <kgillette@hntb.com> wrote:

Dear Dr. Hanback,

Attached is an early coordination letter for the I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172).

Please let me know if you have any questions.

Thank you,

Kia

Kia Gillette

Environmental Project Manager

Tel (317) 917-5240 Cell (317) 695-0825 Email kgillette@hntb.com

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Eric J. Holcomb
Governor

Bruno Pigott
Commissioner

June 18, 2020

66-33
HNTB
Attention: Kia Gillette
111 Monument Circle, Suite 1200
Indianapolis, Indiana 46204

Dear Kia Gillette,

RE: Wellhead Protection Area
Proximity Determination
Des No 2001172
I-65 Added Travel Lanes Project
Tippecanoe County, Indiana

Upon review of the above referenced project site, it has been determined that the proposed project area **is located within** a Wellhead Protection Area. If the contact information is needed for the WHPA, please contact the reference located at the bottom of the letter for the appropriate information. The information is accurate to the best of our knowledge; however, there are in some cases a few factors that could impact the accuracy of this determination. Some Wellhead Protection Area Delineations have not been submitted, and many have not been approved by this office. In these cases we use a 3,000 foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's (PWSS's) Wellhead Protection Area Delineation please visit our tracking database at <http://www.in.gov/idem/cleanwater/2456.htm> and scroll to the bottom of the page.

The project area **is no located within** a Source Water Assessment Area for a PWSS's surface water intake. The Source Water Assessment Area relates to the surface water drainage area that water could potentially flow and influence water quality for a PWSS's source of drinking water.

Note: the Drinking Water Branch has a self service feature which allows one to determine wellhead proximity without submitting the application form. Use the following instructions:

1. Go to <http://idemmaps.idem.in.gov/whpa2/>
2. Use the search tool located in the upper left hand corner of the application to zoom to your site of interest by way of city, county, or address; or use the mouse to click on the site of interest displayed on the map.
3. Once the site of interest has been located and selected, use the print tool to create a .pdf of a wellhead protection area proximity determination response.

In the future please consider using this self service feature if it suits your needs.

If you have any additional questions please feel free to contact me at the address above or at (317) 233-9158 and aturnbow@idem.in.gov.

Sincerely,

Alisha Turnbow,
Environmental Manager
Ground Water Section
Drinking Water Branch
Office of Water Quality



Please Reduce, Reuse, Recycle

From: [Turnbow, Alisha](#)
To: [Kia Gillette](#)
Cc: [Caroline Tegeler](#)
Subject: RE: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination
Date: Thursday, November 5, 2020 5:50:19 PM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image011.png](#)
[image012.png](#)
[image013.png](#)
[image014.png](#)
[image015.png](#)
[image016.png](#)

Hi Kia,

The project Des No 2001172 is located in Indiana American Water – West Lafayette’s Wellhead Protection Area. The contact for Indiana American Water – West Lafayette is Christina Gosnell and she can be reached at christina.gosnell@amwater.com and 317-885-2408. Let me know what questions you have.

Sincerely,

COVID-19 Resources:

- **Indiana State Dept. of Health (ISDH) COVID-19 Call Center:** Call 877-826-0011 (available 8:00 am-5:00 pm daily).
- **Anthem NurseLine:** Call 800-337-4770 or visit the [Anthem NurseLine](#) online for a FREE symptom screening. Available to anyone with an Anthem health plan (this includes State of IN employees)
- **Anthem Employee Assistance Program (EAP):** Available to full-time state employees and their household members regardless of health plan participation. Call 800-223-7723 or visit anthemeap.com (enter State of Indiana) for crisis counseling, help finding child/elder care, legal/financial consultation and much more.



Alisha Turnbow
Environmental Manager
Office of Water Quality
Drinking Water Branch, Groundwater Section
(317) 233-9158 • aturnbow@idem.IN.gov

Indiana Department of Environmental Management



IDEM values your feedback.

Please take two minutes and complete this brief survey.



From: Kia Gillette <kgillette@HNTB.com>
Sent: Monday, November 02, 2020 1:56 PM
To: Turnbow, Alisha <ATurnbow@idem.IN.gov>
Cc: Caroline Tegeler <ctegeler@HNTB.com>

June 17, 2020

Kia M. Gillette
HNTB Corporation
111 Monument Circle, Suite 1200
Indianapolis, Indiana 46204

Dear Ms. Gillette:

The proposed project to move forward with added travel lanes along I-65 in Tippecanoe County, Indiana, (Des No 2001172), as referred to in your letter received June 12, 2020, will not cause a conversion of primes farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

RICHARD Digitally signed by
RICHARD NEILSON
NEILSON Date: 2020.06.17
15:06:24 -04'00'

RICK NEILSON
State Soil Scientist

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20 NORTH 3RD STREET
LAFAYETTE, INDIANA 47901-1209

(765) 423-9242
(765) 423-9154 [FAX]
www.tippecanoe.in.gov/apc

SALLIE DELL FAHEY
EXECUTIVE DIRECTOR

June 23, 2020
Ref. No. 2020-146

Kia Gillette, Environmental Project Manager
HNTB Corporation
111 Monument Cir, STE 1200
Indianapolis, IN 46204

Dear Ms. Gillette:

Thank you for including the Area Plan Commission as part of the early coordination phase of the I-65 Added Travel Lanes project, des #2001172. We have reviewed the project information and offer the following comments.

In reviewing the Tippecanoe County Interim Report we have identified six sites within a ½ miles radius of the project. The sites are:

- 1) Miller House on 800 N (rated as "Contributing")
- 2) Burnett Creek Cemetery on E 600 N (rated as "Contributing")
- 3) Peterson Farm on 650 N (rated as "Contributing")
- 4) Burkhardt Farms on SR 43 (rated as "Notable")
- 5) Tippecanoe Battleground Marker on Prophet's Rock Rd (rated as "Contributing")
- 6) Hurtz Cemetery on Swisher Rd (rated as "Contributing")

According to Area Plan Commission data, there are two cemeteries within a ½ mile radius of the project site.

- 1) Burnett's Creek Cemetery, east of the project site on E 600 N
- 2) Hurst Cemetery, east of the project site on Swisher Rd

Traffic for three nearby schools -- Burnett Creek Elementary, Harrison Senior High, and Battleground Middle -- may be impacted by the project. Additionally, one recreation facility, Edwood Glen Country Club, may be impacted. Two churches -- Apostolic Christian Church of West Lafayette, and Apostolic Christian Fellowship -- are within the ½ mile radius. In addition to the three parks noted in the early coordination letter, there are two more managed lands within the ½ mile radius, Edwood Glen Golf Course and Coyote Crossing Golf Course. Four Indiana Gas Co. pipelines are within the ½ mile radius. There are six USTs, two LUSTs, one NPDES pipe, and eight NPDES facilities within the ½ mile radius.

The Area Plan Commission is unable to determine the presence or absence of rusty patch bumblebees, Indiana bats, or northern long-eared bats.

We have also researched various other GIS data sets and informational sources and found no other churches, libraries, schools, parks, trails, nature preserves, NICHES Land Trust areas,

THE

Area Plan Commission

of TIPPECANOE COUNTY

20 NORTH 3RD STREET
LAFAYETTE, INDIANA 47901-1209

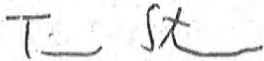
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SALLIE DELL FAHEY
EXECUTIVE DIRECTOR

round barns, historical markers, oil or gas wells, active mining, and underground storage tanks within a ½ mile radius of the project that were not mentioned in the early coordination letter.

Please call or email if you have any questions.

Sincerely,



Tim Stroshine
Assistant Director

cc: Sallie Fahey, Area Plan Commission of Tippecanoe County Executive Director

From: [Jim Butcher](#)
To: [Kia Gillette](#)
Cc: [Zachariah Beasley](#); [Brenda Garrison](#)
Subject: RE: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination
Date: Wednesday, June 24, 2020 9:58:00 AM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[20200624093820326.pdf](#)
[AA-15.pdf](#)
[V-7.pdf](#)
[Y-10.pdf](#)

Kia,

There are several section corners within the proposed construction area. A map and section corner dossiers are attached. Also, please be aware that stormwater detention will be required to mitigate increased runoff due to the increase in impervious surface.

Jim

James A. Butcher PLS
Project Manager
Tippecanoe County Surveyor's Office
20 North Third St.
Lafayette, IN 47901
Phone: 765.423.9228
Fax: 765.423.9241
jbutcher@tippecanoe.in.gov

From: Kia Gillette <kgillette@HNTB.com>
Sent: Friday, June 12, 2020 9:20 AM
To: Surveyor <Surveyor@tippecanoe.in.gov>
Subject: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination

Dear Mr. Beasley,

Attached is an early coordination letter for the I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172).

Please let me know if you have any questions.

Thank you,
Kia

Kia Gillette
Environmental Project Manager
Tel (317) 917-5240 Cell (317) 695-0825 Email kgillette@hntb.com

From: [McCloskey, Elizabeth](#)
To: [Kia Gillette](#)
Subject: Re: [EXTERNAL] I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination
Date: Thursday, June 25, 2020 11:40:46 AM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)

Good morning, because the proposed project will have minor impacts on natural resources, and no Federally endangered species are known to be present, the U.S. Fish and Wildlife Service will not be providing a comment letter.

Elizabeth McCloskey
U.S. Fish and Wildlife Service
Northern Indiana Suboffice
Ecological Services
Chesterton, Indiana

From: Kia Gillette <kgillette@HNTB.com>
Sent: Friday, June 12, 2020 7:54 AM
To: McCloskey, Elizabeth <elizabeth_mccloskey@fws.gov>
Subject: [EXTERNAL] I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination

Dear Ms. McCloskey,

Attached is an early coordination letter for the I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172).

Please let me know if you have any questions.

Thank you,
Kia

Kia Gillette
Environmental Project Manager
Tel (317) 917-5240 Cell (317) 695-0825 Email kgillette@hntb.com

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IDEM > Proposed Roadway Letter

Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

INDOT
Arshad Ahmed
41 W. 300 N.
Crawfordsville , IN 47933

HNTB Corporation
Kia Gillette
111 Monument Circle
Suite 1200
Indianapolis , IN 46204

July 10, 2020

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: INDOT and FHWA intend to proceed with an added travel lanes project along I-65. The project is located 1.33 miles north of SR 25 to 2.43 miles north of SR 43 near Lafayette. The project includes: travel lane/shoulder pavement replacement, reconstruction of SR 43 ramps, addition of a travel lane in the median, widening of the I-65 bridges over 9th St./CSX RR/Burnett Creek, widening of the I-65 bridges over Prophets Rock Road, widening of the I-65 bridges over SR 43, deck replacement of the CR 725 N. bridge over I-65, and construction of median drains, culverts, and detention basins.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at:

<http://www.in.gov/idem/5283.htm>.

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE [Permits and Public Notices \(http://www.irl.usace.army.mil/orf/default.asp\)](http://www.irl.usace.army.mil/orf/default.asp) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm>. IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/4384.htm>.
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the

OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/idem/4384.htm> for the appropriate staff contact to further discuss your project.

5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
 - IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - IC 14-28-1 Flood Control Act 310 IAC 6-1
 - IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> . Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm>

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq>), and as described in 327 IAC 15-5-6.5 ([http://www.in.gov/legislative/iac/T03270/A00150 \[PDF\]](http://www.in.gov/legislative/iac/T03270/A00150), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html>).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm>.

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/idem/4148.htm>) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/idem/4145.htm>.)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf.) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit:

<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>, <http://www.in.gov/idem/4145.htm>, or <http://www.epa.gov/radon/index.html>.

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>.

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit:

<http://www.in.gov/idem/4983.htm>.

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/isdh/19131.htm>.

5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2 , Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf.) New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/idem/4223.htm>, or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm>.
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/4999.htm>.

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/idem/5284.htm>, is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

INDOT and FHWA intend to proceed with an added travel lanes project along I-65. The project is located 1.33 miles north of SR 25 to 2.43 miles north of SR 43 near Lafayette. The project includes: travel lane/shoulder pavement replacement, reconstruction of SR 43 ramps, addition of a travel lane in the median, widening of the I-65 bridges over 9th St./CSX RR/Burnett Creek, widening of the I-65 bridges over Prophets Rock Road, widening of the I-65 bridges over SR 43, deck replacement of the CR 725 N. bridge over I-65, and construction of median drains, culverts, and detention basins.

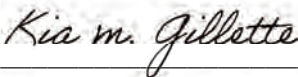
With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: 07/10/2020

Signature of the INDOT
Project Engineer or Other Responsible Agent 

Arshad Ahmed

Date: July 10, 2020

Signature of the
For Hire Consultant 

Kia M. Gillette

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State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-22703

Request Received: June 12, 2020

Requestor: HNTB Corporation
Kia Gillette
111 Monument Circle, Suite 1200
Indianapolis, IN 46204-5178

Project: I-65 added travel lanes from 1.33 miles north of SR 25 to 2.43 miles north of SR 43 (Lead Des #2001172) with bridge deck replacements and widening over 9th Street/CSX Railroad/Burnett Creek/Wabash Heritage Trail (Des #1592725 & 1592726), Prophets Rock Road (Des #1592704 & 1592705), and SR 43 (Des #1601088 & 1601090), and also CR 725 North bridge deck replacement over I-65 (Des #1500644)

County/Site info: Tippecanoe

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: Portions of this proposal may require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1) for any proposal to construct, excavate, or fill in or on the floodway of a stream or other flowing waterbody which has a drainage area greater than one square mile, unless it qualifies for a bridge exemption (see enclosure). Please include a copy of this letter with the permit application, if required.

Natural Heritage Database: The Natural Heritage Program's data have been checked. The managed lands and animal species below have been documented within 1/2 mile of the project area. The Division of Nature Preserves recommends that the project footprint be minimized as much as possible near the nature preserve. Also, no equipment or vehicles should be staged within the preserve boundaries, all right-of-way should be maintained, and the property should be clearly marked to prevent potential negative impacts.

A) MANAGED LANDS:

1. Boehning Nature Preserve (NICHES Land Trust)
2. Prophetstown State Park
3. Tippecanoe Battlefield Memorial Park (Tippecanoe County Parks & Rec)

B) MUSSELS (in the Wabash River):

1. Snuffbox (*Epioblasma triquetra*); federal & state endangered
2. Rayed Bean (*Villosa fabalis*); federal & state endangered
3. Fanshell (*Cyprogenia stegaria*); federal & state endangered
4. Clubshell (*Pleurobema clava*); federal & state endangered
5. Rough Pigtoe (*Pleurobema plenum*); federal & state endangered
6. Rabbitsfoot (*Theliderma cylindrica*); federally threatened & state endangered
7. Round Hickorynut (*Obovaria subrotunda*); state endangered
8. Ohio Pigtoe (*Pleurobema cordatum*); state special concern
9. Kidneyshell (*Ptychobranhus fasciolaris*); state special concern

C) MAMMAL: American Badger (*Taxidea taxus*); state special concern

Attachments: A - Bridge Exemption Criteria

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

Fish & Wildlife Comments: As long as erosion control measures are implemented, we do not foresee any impacts to the mussel species above as a result of this project. Also, badgers are a wide ranging species that prefer an open, prairie-type habitat, with Indiana being at the eastern edge of their natural range. The range of the badger continues to expand as a result of land-use changes from forest to farmland and open pastureland. Impacts to the American badger or its preferred habitat are unlikely as a result of this project.

Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. The following are recommendations that address potential impacts identified in the proposed project area:

1) Stream Crossing Design:

For purposes of maintaining fish and wildlife passage through a crossing structure, the Environmental Unit recommends bridges rather than culverts and bottomless culverts rather than box or pipe culverts. Wide culverts are better than narrow culverts, and culverts with shorter through lengths are better than culverts with longer through lengths. If box or pipe culverts are used, the bottoms should be buried a minimum of 6" (or 20% of the culvert height/pipe diameter, whichever is greater up to a maximum of 2') below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should: span the entire channel width (a minimum of 1.2 times the OHWM width); maintain the natural stream substrate within the structure; have a minimum openness ratio (height x width / length) of 0.25; and have stream depth, channel width, and water velocities during low-flow conditions that are approximate to those in the natural stream channel. Banklines should be restored within box and pipe structures to allow for wildlife passage above the ordinary highwater mark.

The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. The Division of Fish and Wildlife would like to emphasize the importance of wildlife passage issues and transportation infrastructure projects. The following is a good place to start in terms of resources to consider in the design of stream crossing structures:
<http://www.fs.fed.us/wildlifecrossings/library/>.

The following are recommended resources for designing and constructing stream crossings for maintenance of instream habitat and aquatic organism passage:
https://www.fs.fed.us/biology/nsaec/fishxing/aop_pdfs.html and
<https://www.fhwa.dot.gov/engineering/hydraulics/pubs/11008/hif11008.pdf>.

2) Bank Stabilization:

Some form of bank and/or streambed stabilization is almost always needed with the construction, repair, replacement, or modification of a stream channel or crossing structure. For streambank stabilization and erosion control, regrading to a stable slope (2:1 or shallower) and establishing native vegetation along the banks are typically the most effective techniques. A variety of methods to accomplish this include: planting plugs, whips, container stock, seeding, and live stakes. In addition to vegetation establishment, some additional level of bioengineered bank stabilization may be needed under certain circumstances (inability to regrade to a stable slope, flow velocities that exceed the limits of vegetation alone, etc.). Combining vegetation with any of the following bank stabilization methods can provide additional bank protection while not compromising benefits to fish, wildlife, and botanical resources: geotextiles (erosion control blankets and/or turf reinforcement mats that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles), vegetated geogrids or soil lifts, fiber rolls, glacial stone, or riprap. Information about bioengineering techniques can

Attachments: A - Bridge Exemption Criteria

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

be found at <http://www.in.gov/legislative/iac/20120404-IR-312120154NRA.xml.pdf>. Additionally, the following is a link to a USDA/NRCS document that outlines many different bioengineering techniques for streambank stabilization: <http://directives.sc.egov.usda.gov/17553.wba>.

Riprap or other hard bank stabilization materials should be used only at the toe of the sideslopes up to the ordinary high water mark (OHWM) with the exception of areas directly under bridges for instance. The banks above the OHWM should be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion. For streambed stabilization or scour protection, riprap or other stabilization materials should not be placed in the active stream channel above the existing streambed or flowline elevation unless specifically designed and installed for grade control and aquatic organism passage. This is to prevent obstructions to the movement of aquatic organisms upstream and downstream.

3) Riparian Habitat:

We recommend a mitigation plan be developed (and submitted with the permit application, if required) for any unavoidable habitat impacts that will occur. The DNR's Habitat Mitigation guidelines (and plant lists) can be found online at: <http://iac.iga.in.gov/iac/20200527-IR-312200284NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acre in an urban area may still involve the replacement of large diameter trees but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas. There are exceptions for high quality habitat sites however.

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana and specifically for stream bank/floodway stabilization purposes as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only.
2. Minimize and contain within the project limits inchannel disturbance and the clearing of trees and brush.
3. Do not work in the waterway from April 1 through June 30 without the prior written approval of the Division of Fish and Wildlife.
4. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
5. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumpharounds.
6. Use minimum average 6 inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids.

Attachments: A - Bridge Exemption Criteria

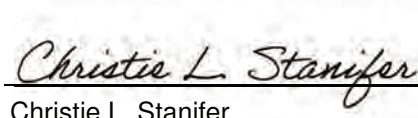
THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

- 7. Do not use broken concrete as riprap.
- 8. Underlay the riprap with a bedding layer of well graded aggregate or a geotextile to prevent piping of soil underneath the riprap.
- 9. Minimize the movement of resuspended bottom sediment from the immediate project area.
- 10. Do not deposit or allow demolition/construction materials or debris to fall or otherwise enter the waterway.
- 11. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
- 12. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: July 10, 2020

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

From: [Kia Gillette](#)
To: "Getz, Jason"
Cc: [Shapley, Jake B](#)
Subject: RE: Des. No. 2001172 Comments Request
Date: Wednesday, October 14, 2020 11:12:00 AM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)

Hi Jason,

I apologize for the delay in this response. Please see responses to your questions/comments in green below.

Exit 178 is a primary access location for Prophetstown State Park. During this construction phase, visitors will be impacted by the construction processes. I would need notification with as much as advance notice as possible to redirect arriving campers. At least a one weeks' notice would be the minimum to be able to send out traffic alerts. - **The exit/entrance ramps will be closed for some duration of time during construction. We will coordinate on timing and will give as much notice as possible (1 week minimum) so you can notify visitors.**

Will the present fence that serves as a physical barrier between State and private property be replaced during this time? It's my understanding that no boundaries will change as a result of this process. – **There are no anticipated impacts to the fence. It will remain in place. No boundaries will change as a result of the project.**

It is also my understanding that when the bridge decking was replaced on the Swisher Road Bridge that the road would be lowered beneath the bridge to meet the height requirements. How will the drainage be directed so water does not enter on state or private property? **The mainline will be lowered to meet vertical clearance criteria at Swisher Rd. Drainage and hydraulics will be evaluated and all roadway drainage will be detained within INDOT right-of-way.**

There is presently a population of crown vetch on the state right away that has entered into State Park property. Can this population of crown vetch be sprayed/removed if it is not impacted by construction? All seed mix should not contain any state listed invasive species or species of concern. – **Could you send a map or GoogleEarth pin of the location of concern? If it is within the construction limits, it can be removed. If they are outside the limits, that is something we can bring up with INDOT and may require further discussion. We anticipate reseeding disturbed areas with INDOT Seed Mix R which includes tall fescue, perennial ryegrass, creeping red fescue, and white dutch clover. Crown vetch is not included and I'm not seeing these species on the Terrestrial Invasive Species – Plant list (<https://www.in.gov/dnr/6351.htm>)**

Please let me know if you have any questions.

Thanks,
Kia

From: [Kia Gillette](#)
To: "Getz, Jason"
Subject: RE: Des. No. 2001172 I-65 Added Trave Lanes
Date: Thursday, January 21, 2021 7:16:00 AM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)

Hi Jason,

We discussed this with INDOT and they do not wish to add the tent symbols to the exit signs as adding those symbols to INDOT signs is not a standard process. They feel the notification signs are appropriate and those will be reinstalled.

We do have some flexibility with the temporary signage during the SR 43 ramp detour during construction. Is there anything that would be helpful to include in the temporary construction signage for the park?

Thanks,
Kia

Kia Gillette
Environmental Project Manager
Email kgillette@hntb.com

From: Getz, Jason <JGetz@dnr.IN.gov>
Sent: Monday, December 7, 2020 3:13 PM
To: Kia Gillette <kgillette@HNTB.com>
Subject: Des. No. 2001172 I-65 Added Trave Lanes

Good afternoon Kia. I do not have any concerns, but was wondering if we can get the camping (tent) symbol added to the exit 178 signs? Those have not been instated yet and I thought that this would be a good time. There should be exit notification signs on the north and south bound exits already along with the directional signs for Prophetstown State Park. Are there any other signage improvements that could be made for the state park?

Thank you,

Jason

Jason Getz
Property Manager
Prophetstown State Park
5545 Swisher Road [GPS park entrance]
4112 State Road 225 E [mailing only]
West Lafayette, IN 47906

From: [Grant Heinold](#)
To: [Kia Gillette](#)
Subject: Re: Comment regarding Des. No. 2001172
Date: Monday, July 27, 2020 10:01:06 AM

Thanks, Kia.

Yes, we would be interested in knowing the results once they are available.

Best regards,
Grant

On Mon, Jul 27, 2020 at 6:49 AM Kia Gillette <kgillette@hntb.com> wrote:

Hi Grant,

It's ok that the 30 days has passed, your comments will still be considered.

At this stage in the project, we do not know if a noise barrier will be constructed. Per INDOT's Noise Policy, a noise analysis will be completed for the project and that will determine if any noise barriers will be constructed. INDOT's Noise Policy is available online if you are interested in reviewing it:
<https://www.in.gov/indot/files/2017%20INDOT%20Noise%20Policy.pdf>

I anticipate the noise analysis will start later this fall and will likely be complete early next year. I can follow up with you once the noise analysis is complete to let you know the results.

Please let me know if you have additional questions.

Thanks,

Kia

Kia Gillette

Environmental Project Manager

Email kgillette@hntb.com

From: Grant Heinold <gheinold@gmail.com>
Sent: Wednesday, July 22, 2020 10:10 AM
To: Kia Gillette <kgillette@HNTB.com>; arahmed@indot.in.gov
Subject: Comment regarding Des. No. 2001172

Hello Kia and Arshad,

I am contacting you on behalf of the West Lafayette Apostolic Christian Church. We are located on a property adjacent to I-65 NW of the I65 / CR 600 N overpass. Our actual address is 6101 N CR 75 East West Lafayette, IN 47906.

We received the packet of information requesting comment in the mail, and last night was the 1st time since receiving it that our church's Stewardship Committee met where it could be discussed. We realize we have missed the 30 day window that was stated for feedback, but would appreciate it if our input could still be considered.

The comments that were made in the meeting were around the additional traffic volume and speed that the proposed expansion would allow for. It was asked whether a sound barrier wall is being considered for installation on any sections where there are buildings such as ours relatively close to the road.

Can you comment on whether this has been consideration of this?

Respectfully submitted,

Grant Heinold

This e-mail and any files transmitted with it are confidential and are intended solely for the use of the individual or entity to whom they are addressed. If you are NOT the intended recipient and receive this communication, please delete this message and any attachments. Thank you.

Organization and Project Information

Project ID:
Des. ID: Des. No. 2001172
Project Title: I-65 Added Travel Lanes
Name of Organization: HNTB Corporation
Requested by: Caroline Tegeler

Environmental Assessment Report

1. Geological Hazards:
 - High liquefaction potential
 - Floodway
2. Mineral Resources:
 - Bedrock Resource: High Potential
 - Sand and Gravel Resource: High Potential
3. Active or abandoned mineral resources extraction sites:
 - None documented in the area

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

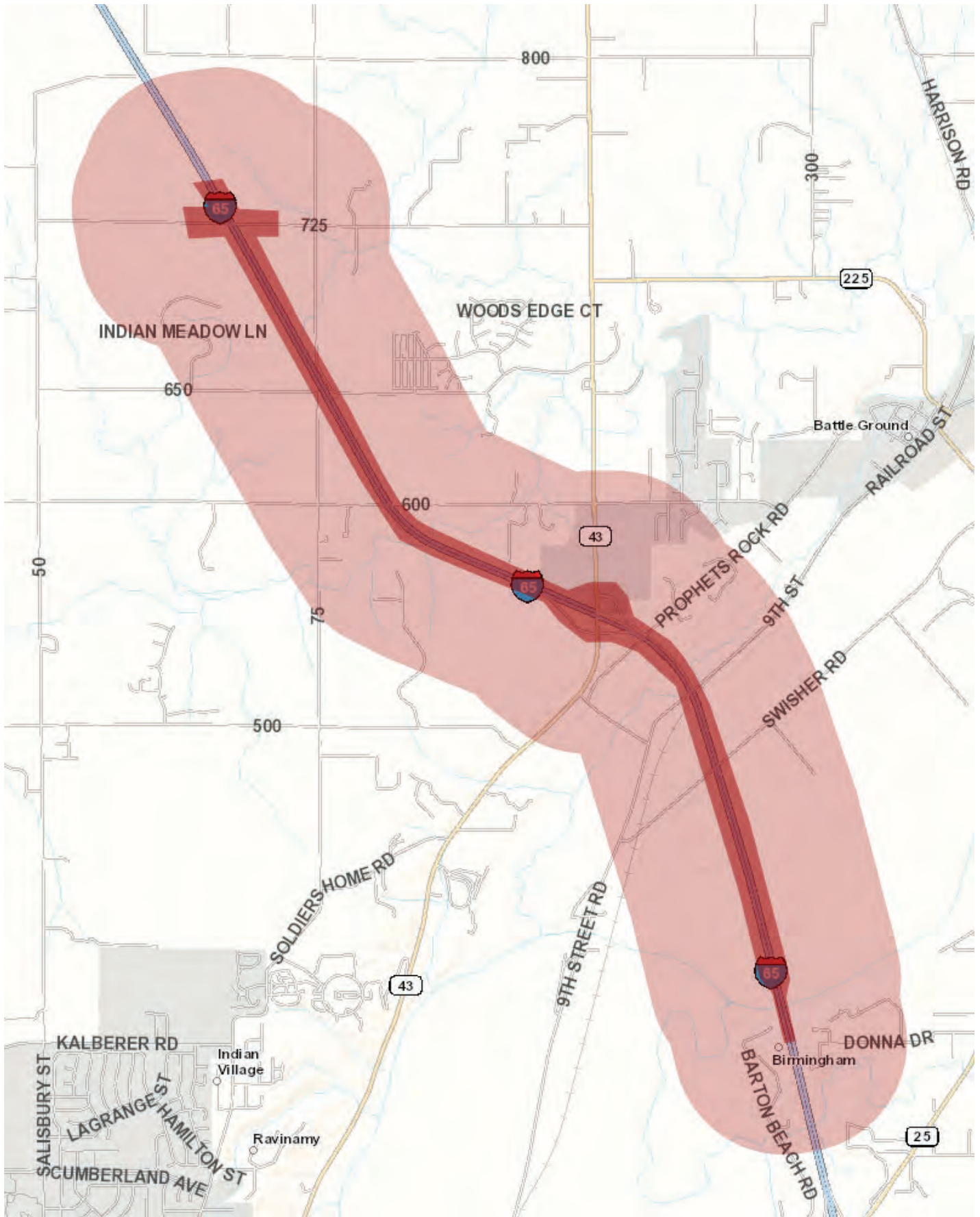
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: October 20, 2020



From: [Kia Gillette](#)
To: ["christina.gosnell@amwater.com"](mailto:christina.gosnell@amwater.com)
Cc: [Caroline Tegeler](#)
Subject: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination for Wellhead Protection Area
Date: Friday, November 6, 2020 6:45:00 AM
Attachments: [Early Coordination Letter - Des. No. 2001172 I65 Added Travel Lanes Indiana American Water.pdf](#)
[image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)

Dear Ms. Gosnell,

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intend to proceed with an added travel lanes project along I-65 in Tippecanoe County. Additional information for the project is included in the attached letter.

As part of the environmental review process, INDOT investigates potential impacts to resources including wellhead protection areas. Coordination with the Indiana Department of Environmental Management (IDEM) indicates the project is located in the Indiana American Water – West Lafayette’s wellhead protection area.

We are coordinating with you to gather more information about the wellhead protection area, specifically:

1. Do you have any concerns with the I-65 added travel lanes project or foresee any potential impacts to the wellhead protection area as a result of this project?
2. Are there any best management practices or measures that INDOT should consider to avoid or minimize impacts to the wellhead protection area?
3. Is there a wellhead protection management plan available for this wellhead protection area that you would be willing to share with the I-65 project team?

Please let me know if you need any additional information or if there is someone else we should contact regarding the wellhead protection area.

Thank you,
Kia

Kia Gillette

Environmental Project Manager

Tel (317) 917-5240 Cell (317) 695-0825 Email kgillette@hntb.com

HNTB CORPORATION

111 Monument Circle, Suite 1200 | Indianapolis, IN 46204 | www.hntb.com

 **100+ YEARS OF INFRASTRUCTURE SOLUTIONS**



From: [Kia Gillette](#)
To: "Carrie E Lowe"
Subject: RE: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination for Wellhead Protection Area
Date: Monday, November 16, 2020 8:11:00 AM

Hi Carrie,

At this point we do not know where the contractor will store fuel or if there will be secondary containment. We can tell them where not to store it as part of our project commitments. Is there a specific area you would prefer them not to store fuel?

The erosion control plans are still under development. We anticipate them being ready at about the end of the year. I will send them to you at that point.

We will include a commitment about notifying you immediately in the event of a spill.

Thanks,
Kia

Kia Gillette
Environmental Project Manager
Email kgillette@hntb.com

From: Carrie E Lowe <Carrie.Lowe@amwater.com>
Sent: Friday, November 13, 2020 1:40 PM
To: Kia Gillette <kgillette@HNTB.com>
Subject: I-65 Added Travel Lanes Project in Tippecanoe County (Des. No. 2001172) – Early Coordination for Wellhead Protection Area

Hello Kia,

I do not think the area you were speaking about with Chris Gosnell is in our Wellhead Protection area, but my local operations had a few questions. Where will fuel be stored and will there be secondary containment involved. Also is there an erosion plan we could look at? We do request that if there is a known spill that we be notified immediately so we may monitor the spill in relation to our treatment plants.

Thank you,

Carrie Lowe
Water Quality Supervisor
Carrie.lowe@amwater.com
260.557.2694

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United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

January 15, 2021

Consultation Code: 03E12000-2021-SLI-0175

Event Code: 03E12000-2021-E-02552

Project Name: I-65 Added Travel Lanes Project (Des. No. 2001172 & 2100049)

Subject: Updated list of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service’s Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2021-SLI-0175

Event Code: 03E12000-2021-E-02552

Project Name: I-65 Added Travel Lanes Project (Des. No. 2001172 & 2100049)

Project Type: TRANSPORTATION

Project Description: FHWA and the INDOT Crawfordsville District are planning to proceed with an added travel lanes project on I-65 from approximately 1.33 miles north of SR 25 to approximately 2.43 miles north of SR 43 near Lafayette and Battle Ground in Tippecanoe County, Indiana. Additional length north and south of these limits is included in the project area for median crossovers for maintenance of traffic. Project activities would include pavement and shoulder replacement, the addition of a travel lane in each direction in the median, addition of lanes/extension of turn lanes on I-65/SR 43 interchange ramps, bridge deck replacement and widening of bridges over 9th Street/CSX Railroad/Burnett Creek/Wabash Heritage Trail (Bridge Nos. I65-177-02402 BNBL & I65-177-02402 JCSB), bridge deck replacement and widening of bridges over Prophets Rock Road (Bridge Nos. I65-178-05485 BNBL & I65-178-05485 JBSB), bridge deck replacement and widening of bridges over SR 43 (Bridge Nos. I65-178-05486 JNBN & I65-178-05486 BSBL), bridge deck replacement and elevation of CR 725 N. bridge over I-65 (Bridge No. I65-180-05489), and replacement of culvert crossings under I-65 and construction of median drains, culverts, and detention basins for roadway drainage. Construction is anticipated to start in March 2022 and end in November 2023.

No temporary or permanent right-of-way acquisition will be necessary for this project. A maximum of 9.0 acres of tree clearing may occur within the existing right-of-way. Of this, 7.7 acres will be within 100 feet of the road, and 1.3 acres will be removed 100-300 feet from road. Dominant tree species included white mulberry (*Morus alba*), autumn olive (*Elaeagnus umbellata*), black locust (*Robinia pseudoacacia*), Eastern red cedar (*Juniperous virginiana*). Construction activities are anticipated to elevate noise levels above existing levels and the project involves permanent and temporary lighting modifications. A query of the USFWS Bat Database by INDOT Environmental Services Division staff on June 15, 2020, did not identify any documented sites within 0.5 mile of the project area. Bridge and culvert inspections completed in 2020 by HNTB staff did not indicate any evidence of bats. Reports for these inspections have been uploaded into IPaC.

Mitigation for tree clearing will be required. The mitigation calculation for the project is $(1.3 \text{ ac}) \times (1.75) \times \$9,354 = \$21,280.35$.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@40.496121070543694,-86.86759355699122,14z>



Counties: Tippecanoe County, Indiana

Endangered Species Act Species

There is a total of 5 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Clams

NAME	STATUS
Fanshell <i>Cyprogenia stegaria</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/4822	Endangered
Rabbitsfoot <i>Quadrula cylindrica cylindrica</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5165	Threatened
Sheepnose Mussel <i>Plethobasus cyphus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6903	Endangered

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 2:15 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482338.36	Contract Number: 1592725 & 1592726	Anticipated Start Date for Construction: March 2022
Easting: 512025.24		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: Burnett Creek, 9th St., CSX RR	Station:
Bridge/Culvert number: I65-177-052402 BNBL & JCSB	Number of Spans: 6
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input checked="" type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 1:50 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4481803.64	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 512025.24		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: I-65	Station:
Bridge/Culvert number: I65-177-05484 A	Number of Spans: 2
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input checked="" type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input checked="" type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 8/7/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 3:00 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4485722.66	Contract Number: 1500644	Anticipated Start Date for Construction: March 2022
Easting: 511151.53		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: I-65	Station:
Bridge/Culvert number: I65-180-05489	Number of Spans: 2
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input checked="" type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 8/7/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 2:20 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482759.67	Contract Number:	Anticipated Start Date for
Easting: 511519.50	1592704 & 1592705	Construction:
UTM Zone: 16		March 2022

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: Prophets Rock Rd.	Station:
Bridge/Culvert number: I65-178-05485 BNBL & JBSB	Number of Spans: 3
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input checked="" type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 8/7/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 5:20 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482894.53	Contract Number:	Anticipated Start Date for
Easting: 511151.53	TBD	Construction:
UTM Zone: 16		March 2022

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: SR 43	Station:
Bridge/Culvert number: I65-178-05486 JBNB & BSBL	Number of Spans: 2
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input checked="" type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 2:00pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4481437.61	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 542137.66		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P501	Number of Spans: 36"x183'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 7:10 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482899.82	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 511137.45		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P502	Number of Spans: 36"x437
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input checked="" type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input checked="" type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 3:30 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482900.94	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 511163.34		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P503	Number of Spans: 36"x383'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input checked="" type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input checked="" type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 3:10 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4483067.21	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 510763.93		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P504	Number of Spans: 36"x220'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/28/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 7:15pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4483285.62	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 510269.64		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P505	Number of Spans: 36"x319'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/28/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 7:50 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4483375.82	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 510057.44		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P506	Number of Spans: 42"x241'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/28/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 8:35 am	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4483688.28	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 509678.37		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P507	Number of Spans: 36"x139'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/28/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 8:45 am	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: K. Williams, L. Little		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4483694.81	Contract Number:	Anticipated Start Date for
Easting: 509613.68	2001172	Construction:
UTM Zone: 16		March 2022

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P508	Number of Spans: 36"x123'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/28/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 6:40 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Tegeler, C. Meador		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4484127.36	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 509396.40		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P509	Number of Spans: 36"x183'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/28/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 9:42pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Tegeler, C. Meador		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4484559.79	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 509155.36		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P510	Number of Spans: 36"x226'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 6:20pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Tegeler, C. Meador		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4484743.27	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 509052.05		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P511	Number of Spans: 36"x206'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/28/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 4:00pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: K. Lucier		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4484908.25	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 508960.19		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: North Branch Burnett Creek	Station:
Bridge/Culvert number: P512	Number of Spans: 42"x247'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 8/7/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 6:24 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4483039.50	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 511205.42		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P515	Number of Spans: 36"x100'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 7:30 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482773.88	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 511091.78		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P702	Number of Spans: 36"x145'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 8:00 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482776.33	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 511193.62		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P703	Number of Spans: 36"x76"
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 4:20 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482778.98	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 511345.85		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P704	Number of Spans: 36"x77"
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 2:30 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4482861.89	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 511368.79		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P706	Number of Spans: 36"x72'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 2:30 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4481822.77	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 512043.85		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P707	Number of Spans: 36"x140'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input checked="" type="checkbox"/> Corrugated steel pipe <input type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input checked="" type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input checked="" type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A

INDOT Bridge/Small Structure Bat Inspection Data Sheet (Rev 4/29/2016)

General Information		
Date of Inspection: 7/30/2020	Initial Inspection <input checked="" type="checkbox"/>	Temp: 81 °C
Time of Inspection: 5:20 pm	Follow-up Inspection <input type="checkbox"/>	Wind: 12 mph
County: Tippecanoe	Construction <input type="checkbox"/>	Precip: 0
Inspected by: C. Meador, C. Tegeler		Sunrise: 7:41 Sunset: 7:26
GPS Northing: 4481822.77	Contract Number: 2001172	Anticipated Start Date for Construction: March 2022
Easting: 512043.85		
UTM Zone: 16		

Bridge or Culvert	Bridge or Culvert
Stream or Road Crossed: N/A	Station:
Bridge/Culvert number: P707B	Number of Spans: 36"x175'
Type of Structure: <input type="checkbox"/> Concrete box beam <input type="checkbox"/> Steel beam <input type="checkbox"/> Concrete I-beam <input type="checkbox"/> Steel girder <input type="checkbox"/> Concrete bulb tee beam <input type="checkbox"/> Steel pony truss <input type="checkbox"/> Concrete arch <input type="checkbox"/> Welded steel thru girder <input type="checkbox"/> Concrete girder <input type="checkbox"/> Concrete box culvert <input type="checkbox"/> Concrete slab <input type="checkbox"/> Concrete pipe <input type="checkbox"/> Multi-plate arch <input type="checkbox"/> Corrugated steel pipe <input checked="" type="checkbox"/> Other (list):	Material: <input type="checkbox"/> Concrete <input type="checkbox"/> Steel <input type="checkbox"/> Other (describe): Shape: <input type="checkbox"/> Box Culvert <input type="checkbox"/> Pipe <input type="checkbox"/> Arch <input type="checkbox"/> Slab <input type="checkbox"/> Other (describe)
Searched entire structure? If not, why not? Yes	Location of bats or signs of use (w/drawing and photos): N/A
Bats Present? <input type="checkbox"/> Seen? <input type="checkbox"/> Heard? No	
In Clusters? Number of clusters: N/A	
Number of bats in largest cluster: N/A	
Approximate total number of bats found: N/A	
Signs of previous bat use? <input type="checkbox"/> Guano <input type="checkbox"/> Staining No	

If Bats Present
Date and Time Project Supervisor was notified: N/A
Name of Project Supervisor notified: N/A



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

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<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

IPaC Record Locator: 869-98435061

January 15, 2021

Subject: Consistency letter for the 'I-65 Added Travel Lanes Project (Des. No. 2001172 & 2100049)' project (no current TAILS record) under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **I-65 Added Travel Lanes Project (Des. No. 2001172 & 2100049)** (Proposed Action) may rely on the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, and is likely to adversely affect the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*). Consultation with the Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) is required.

This "may affect - likely to adversely affect" determination becomes effective when the lead Federal action agency or designated non-federal representative requests the Service rely on the PBO to satisfy the agency's consultation requirements for this project. Please provide this consistency letter to the lead Federal action agency or its designated non-federal representative for review, and as the agency deems appropriate, transmit to this Service Office for verification that the project is consistent with the PBO.

This Service Office will respond by letter to the requesting Federal action agency or designated non-federal representative within 30 calendar days to:

- verify that the Proposed Action is consistent with the scope of actions covered under the PBO;
- verify that all applicable avoidance, minimization, and compensation measures are included in the action proposal;
- identify any action-specific monitoring and reporting requirements, consistent with the monitoring and reporting requirements of the PBO, and
- identify anticipated incidental take.

ESA Section 7 compliance for this Proposed Action is not complete until the Federal action agency or its designated non-federal representative receives a verification letter from the Service.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please advise the lead Federal action agency accordingly.

The following species may occur in your project area and **are not** covered by this determination:

- Fanshell *Cyprogenia stegaria* Endangered
- Rabbitsfoot *Quadrula cylindrica cylindrica* Threatened
- Sheepnose Mussel *Plethobasus cyphus* Endangered

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

I-65 Added Travel Lanes Project (Des. No. 2001172 & 2100049)

Description

FHWA and the INDOT Crawfordsville District are planning to proceed with an added travel lanes project on I-65 from approximately 1.33 miles north of SR 25 to approximately 2.43 miles north of SR 43 near Lafayette and Battle Ground in Tippecanoe County, Indiana. Additional length north and south of these limits is included in the project area for median crossovers for maintenance of traffic. Project activities would include pavement and shoulder replacement, the addition of a travel lane in each direction in the median, addition of lanes/extension of turn lanes on I-65/SR 43 interchange ramps, bridge deck replacement and widening of bridges over 9th Street/CSX Railroad/Burnett Creek/Wabash Heritage Trail (Bridge Nos. I65-177-02402 BNBL & I65-177-02402 JCSB), bridge deck replacement and widening of bridges over Prophets Rock Road (Bridge Nos. I65-178-05485 BNBL & I65-178-05485 JBSB), bridge deck replacement and widening of bridges over SR 43 (Bridge Nos. I65-178-05486 JNBN & I65-178-05486 BSBL), bridge deck replacement and elevation of CR 725 N. bridge over I-65 (Bridge No. I65-180-05489), and replacement of culvert crossings under I-65 and construction of median drains, culverts, and detention basins for roadway drainage. Construction is anticipated to start in March 2022 and end in November 2023.

No temporary or permanent right-of-way acquisition will be necessary for this project. A maximum of 9.0 acres of tree clearing may occur within the existing right-of-way. Of this, 7.7 acres will be within 100 feet of the road, and 1.3 acres will be removed 100-300 feet from road. Dominant tree species included white mulberry (*Morus alba*), autumn olive (*Elaeagnus umbellata*), black locust (*Robinia pseudoacacia*), Eastern red cedar (*Juniperus virginiana*). Construction activities are anticipated to elevate noise levels above existing levels and the project involves permanent and temporary lighting modifications. A query of the USFWS Bat Database by INDOT Environmental Services Division staff on June 15, 2020, did not identify any documented sites within 0.5 mile of the project area. Bridge and culvert inspections completed in 2020 by HNTB staff did not indicate any evidence of bats. Reports for these inspections have been uploaded into IPaC.

Mitigation for tree clearing will be required. The mitigation calculation for the project is $(1.3 \text{ ac}) \times (1.75) \times \$9,354 = \$21,280.35$.

Determination Key Result

Based on your answers provided, this project is likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the conclusion and Incidental Take Statement provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?
No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

Yes

20. Are *all* trees that are being removed clearly demarcated?

Yes

21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?

Yes

22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

23. Does the project include slash pile burning?

No

24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

25. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

26. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *INDOT_Bridge Inspection Form I65-177-05484 A Swisher Rdpdf.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246654>
- *INDOT_Bridge Inspection Form I65-180-05489 CR 725 N.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246655>
- *INDOT_Bridge Inspection Form I65-187-05485 BNBL JBSB Prophets Rock Rdpdf.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246656>
- *INDOT_Bridge Inspection Form I65-187-05486 JBNB BSBL SR 43.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246658>
- *INDOT_Bridge Inspection Form P502.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246659>
- *INDOT_Bridge Inspection Form P501.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246660>
- *INDOT_Bridge Inspection Form P503.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246661>
- *INDOT_Bridge Inspection Form P504.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246662>
- *INDOT_Bridge Inspection Form P505.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246663>
- *INDOT_Bridge Inspection Form P506.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246664>

- *INDOT_Bridge Inspection Form P507.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246665>
- *INDOT_Bridge Inspection Form P508.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246666>
- *INDOT_Bridge Inspection Form P510.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246641>
- *INDOT_Bridge Inspection Form P511.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246642>
- *INDOT_Bridge Inspection Form P509.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246643>
- *INDOT_Bridge Inspection Form P512.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246645>
- *INDOT_Bridge Inspection Form P515.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246646>
- *INDOT_Bridge Inspection Form P702.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246647>
- *INDOT_Bridge Inspection Form P703.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246648>
- *INDOT_Bridge Inspection Form P704.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246649>
- *INDOT_Bridge Inspection Form P706.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246650>
- *INDOT_Bridge Inspection Form P707B.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246651>
- *INDOT_Bridge Inspection Form P707.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246652>
- *INDOT_Bridge Inspection Form I65-177-02402 BNBL JCSB Burnett Creek 9th St.pdf* <https://ecos.fws.gov/ipac/project/LA3KGH4EXVBVZJFCHH4U3X3W3Q/projectDocuments/24246653>

27. Did the bridge assessment detect *any* signs of Indiana bats and/or NLEBs roosting in/under the bridge (bats, guano, etc.)^[1]?

[1] If bridge assessment detects signs of *any* species of bats, coordination with the local FWS office is needed to identify potential threatened or endangered bat species. Additional studies may be undertaken to try to identify which bat species may be utilizing the bridge prior to allowing *any* work to proceed.

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

28. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

Yes

29. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

30. Will the project involve the use of **temporary** lighting *during* the active season?

Yes

31. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?

Yes

32. Will the project install *any* new or replace any existing **permanent** lighting in addition to the lighting already indicated for habitat removal (including the removal or trimming of trees) or bridge/structure removal, replacement or maintenance activities?

Yes

33. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **permanent** lighting (other than the lighting already indicated for habitat removal (including the removal or trimming of trees) or bridge/structure removal, replacement or maintenance activities) will be installed or replaced?

Yes

34. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

Yes

35. Will the activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the active season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

36. Will *any* activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the inactive season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

37. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

38. Will the project raise the road profile **above the tree canopy**?

No

39. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the active season within undocumented habitat.

40. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) and/or increase noise levels above existing traffic/background levels consistent with a No Effect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the inactive season

41. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

42. Is the habitat removal portion of this project consistent with a Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal that occurs outside the Indiana bat's active season is 100-300 feet from the existing road/rail surface, and is not in documented roosting/foraging habitat or travel corridors.

43. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

44. Is the habitat removal portion of this project consistent with a Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal that occurs outside the NLEB's active season is 100-300 feet from the existing road/rail surface, and is not in documented roosting/foraging habitat or travel corridors.

45. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

46. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

47. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word "trees" as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS' current summer survey guidance for our latest definitions of suitable habitat.

Yes

48. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

49. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^{[1][2]} to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

50. **Lighting AMM 2**

Will the **permanent** lighting used during removal of suitable habitat and/or the removal/trimming of trees within suitable habitat be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

51. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

52. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^{[1][2]} to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

53. **Lighting AMM 2**

Will the **permanent** lighting (other than any lighting already indicated for tree clearing or bridge/structure removal, replacement or maintenance activities) be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

54. For Indiana bat, if applicable, compensatory mitigation measures are required to offset adverse effects on the species (see Section 2.10 of the BA). Please select the mechanism in which compensatory mitigation will be implemented:

1. *Range-wide In Lieu Fee Program, The Conservation Fund*

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

Yes

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

No

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

7.7

4. How many acres^[1] of trees are proposed for removal between 100-300 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

1.3

5. **Please verify:**

All tree removal will occur greater than 0.5 mile from any hibernaculum.

Yes, I verify that all tree removal will occur greater than 0.5 miles from any hibernaculum.

6. Is the project location 0-100 feet from the edge of existing road/rail surface?

Yes

7. Is the project location 100-300 feet from the edge of existing road/rail surface?

Yes

8. **Please verify:**

No documented Indiana bat roosts or surrounding summer habitat within 0.25 mile of documented roosts will be impacted between May 1 and July 31.

Yes, I verify that no documented Indiana bat roosts or surrounding summer habitat within 0.25 mile of documented roosts will be impacted during this period.

9. **Please verify:**

No documented NLEB roosts or surrounding summer habitat within 150 feet of documented roosts will be impacted between June 1 and July 31.

Yes, I verify that no documented NLEB roosts or surrounding summer habitat within 150 feet of documented roosts will be impacted during this period.

10. Please describe the proposed bridge work:

FHWA and the INDOT Crawfordsville District are planning to proceed with an added travel lanes project on I-65 from approximately 1.33 miles north of SR 25 to approximately 2.43 miles north of SR 43 near Lafayette and Battle Ground in Tippecanoe County, Indiana. Additional length north and south of these limits is included in the project area for median crossovers for maintenance of traffic. Project activities would include pavement and shoulder replacement, the addition of a travel lane in each direction in the

median, addition of lanes/extension of turn lanes on I-65/SR 43 interchange ramps, bridge deck replacement and widening of bridges over 9th Street/CSX Railroad/Burnett Creek/Wabash Heritage Trail (Bridge Nos. I65-177-02402 BNBL & I65-177-02402 JCSB), bridge deck replacement and widening of bridges over Prophets Rock Road (Bridge Nos. I65-178-05485 BNBL & I65-178-05485 JBSB), bridge deck replacement and widening of bridges over SR 43 (Bridge Nos. I65-178-05486 JNBN & I65-178-05486 BSBL), bridge deck replacement and elevation of CR 725 N. bridge over I-65 (Bridge No. I65-180-05489), and replacement of culvert crossings under I-65 and construction of median drains, culverts, and detention basins for roadway drainage.

11. Please state the timing of all proposed bridge work:

March 2022-November 2023

12. Please enter the date of the bridge assessment:

July 28-August 7, 2020

13. You have indicated that the following Avoidance and Minimization Measures (AMMs) will be implemented as part of the proposed project:

- *Tree Removal AMM 1*
- *Lighting AMM 1*
- *Lighting AMM 2*
- *Tree Removal AMM 3*
- *General AMM 1*

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 29, 2020. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

From: [Hinkle, Meghan](#)
To: [Kia Gillette](#)
Cc: [Miller, Brandon](#); [Laymon, Makinna](#)
Subject: RE: LAA response Des. No. 2001172 - I-65 Added Travel Lanes - IPaC Review
Date: Wednesday, January 20, 2021 1:05:56 PM
Attachments: [image001.png](#)

Good Afternoon Kia,

This has been sent to USFWS for their review. Once I receive the concurrence letter I will forward it to you.

Thanks,

Meghan Hinkle

Major Projects / LPA Review Liaison
Environmental Services Division
Indiana Department of Transportation
100 N Senate Ave N758-ES
Indianapolis, IN 46204-2216
New Work Cell: 317-416-6649
Email: MHinkle@indot.IN.gov



From: Kia Gillette <kgillette@HNTB.com>
Sent: Friday, January 15, 2021 11:03 AM
To: Hinkle, Meghan <MHinkle@indot.IN.gov>
Cc: Miller, Brandon <BraMiller1@indot.IN.gov>; Laymon, Makinna <MLaymon2@indot.IN.gov>
Subject: RE: LAA response Des. No. 2001172 - I-65 Added Travel Lanes - IPaC Review

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Meghan,

Thanks for your help. The project description, species list, determination key, and consistency letter have been updated in IPaC for this project.

Please let me know if you have any questions or need any additional information.

Thanks,
Kia

Kia Gillette



United States Department of the Interior Fish and Wildlife Service



Indiana Field Office (ES)
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

January 20, 2021

Karstin Carmany-George
Federal Highway Administration
575 N. Pennsylvania St. Room 254
Indianapolis, Indiana 46204
(sent via email)

TAILS: 03E12000-2021-SLI-0175

RE: REVISED I-65 Added Travel Lanes Project (Des. No. 2001172 & 2100049), Tippecanoe County

Dear Ms. Carmany-George:

The U.S. Fish and Wildlife Service (Service) is responding to your request dated January 15, 2021 to verify that the proposed I-65 Added Travel Lanes Project (Des. No. 2001172 & 2100049) (the Project) may rely on the February 5, 2018, Programmatic Biological Opinion (BO) for federally funded or approved transportation projects that may affect the federally listed endangered Indiana bat (*Myotis sodalis*) and/or federally listed threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). We received your original request and the associated LAA Consistency Letter on November 23, 2020. Since that time, changes in the project scope have resulted in reduced impacts and compensatory mitigation fees; these updates are reflected in this revised letter.

This letter provides the Service's response as to whether the Federal Highway Administration may rely on the BO to comply with Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C. 1531 *et seq.*) for the Project's effects to the Indiana bat and/or NLEB.

The Federal Highway Administration has determined that the Project is *likely to adversely affect* the Indiana bat and/or the NLEB.

Conclusion

The Service has reviewed the effects of the proposed Project, which includes the Federal Highway Administration's commitment to implement any applicable mitigation measures as

indicated on the LAA Consistency Letter. We confirm that the proposed Project's effects are consistent with those analyzed in the BO. The Service has determined that projects consistent with the conservation measures and scope of the program analyzed in the BO are not likely to jeopardize the continued existence of the Indiana bat and/or the NLEB. In coordination with your agency and the other sponsoring Federal Transportation Agencies, the Service will reevaluate this conclusion annually in light of any new pertinent information under the adaptive management provisions of the BO.

Incidental Take

Indiana Bat

The Service anticipates that tree removal associated with the proposed Project will cause incidental take of Indiana bats. As described in the Incidental Take Statement (ITS) of the BO, such taking will be difficult to detect. The Service determined that it is appropriate to measure the amount or extent of incidental taking resulting from BO projects using the proposed acreage of tree removal from Indiana bat suitable habitat as a surrogate for the numbers of individuals taken.

The proposed Project will remove **9.0 acre(s)** of trees from habitat that is suitable for the Indiana bat. All tree removal will occur in winter (October 1 – March 30) and comply with all other conservation measures in the BO. Based on the BO, **7.7 acre(s)** of the removal are within 100 feet of the edge of pavement and therefore not anticipated to result in any adverse effects; **1.3 acre(s)** are within 100-300 feet and expected to result in adverse effects.

The Federal Highway Administration uses the mitigation ratio of **1.75** from Table 3 of the BO¹ to calculate the compensatory mitigation required to offset these adverse impacts for a total of **2.275 acres²** of trees that is suitable for the Indiana bat.

Based on the mitigation identified above² and the information provided in Table 2 of Exhibit E in The Conservation Fund's (TCF) In Lieu Fee (ILF) Instrument³, the Federal Highway Administration will contribute **\$21,280.35** to TCF prior to the start of construction in order to comply with the mitigation requirements of the program of transportation projects reviewed in the BO. These calculations are based on the 2020-2021 Land Use Values in Table 2 of Exhibit E in TCF's ILF Instrument, which are applicable even if the project construction should occur in a different calendar year. At the time of payment, the Federal Highway Administration or designated non-federal representative shall notify the Service of compliance with the compensatory mitigation requirements as described above.

The purchase of species conservation credits and/or in-lieu fee contributions shall occur prior to construction of a transportation project covered under this programmatic consultation. Exceptions to this program stipulation include emergency projects that do not require a letting

¹ https://www.fws.gov/midwest/endangered/section7/fhwa/pdf/IBAT_ILF_ratios_transportation_agencies.pdf

² XX acres * XX ratio

³ https://www.fws.gov/midwest/endangered/section7/fhwa/pdf/IBAT_ExhibitE_Table2_FeeSchedule_LandValues.pdf

prior to construction. In these cases, purchase of credits and/or in-lieu fee contributions shall occur within three months of completion of the project. This timeframe allows for measuring the acres of habitat affected by the emergency project and for financial processing.

In addition (if applicable), the Project may take up to 5 Indiana bats that were not detected during bridge/culvert bat assessments conducted prior to implementing the proposed work on the various structures. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service (refer to User Guide Appendix E - Post Assessment Discovery of Bats at Bridge/Structure Form). Although such take is reasonably certain to occur at up to 10 bridge projects per year as included in the scope of the BO, it is a remote possibility for any individual project that is implemented consistent with the conservation measures of the BO.

The Service will add the acreage of Project-related tree removal to the annual total acreage attributed to the BO as a surrogate measure of Indiana bat incidental take and exempted from the prohibitions of Section 9 of the ESA. Such exemption is effective as long as your agency implements the reasonable and prudent measure (RPM) and accompanying terms and conditions of the BO's ITS.

The sole RPM of the BO's ITS requires the Federal Transportation Agencies to ensure that State/Local transportation agencies, who choose to include eligible projects under the programmatic action, incorporate all applicable conservation measures in the project proposals submitted to the Service for ESA section 7 compliance using the BO. The implementing terms and conditions for this RPM require the Federal Transportation Agencies to offer training to appropriate personnel about using the BO, and promptly report sick, injured, or dead bats (regardless of species) or any other federally listed species located in project action areas.

Northern Long-eared Bat

The Service anticipates that tree removal associated with the Project will cause incidental take of NLEBs. However, the Project is consistent with the BO, and such projects will not cause take of NLEB that is prohibited under the ESA section 4(d) rule for this species (50 CFR §17.40(o)). Therefore, the incidental take of NLEBs resulting from the Project does not require exemption from the Service.

Reporting Dead or Injured Bats

The Federal Highway Administration, its State/Local cooperators, and any contractors must take care when handling dead or injured Indiana bats and/or NLEBs, or any other federally listed species that are found at the Project site to preserve biological material in the best possible condition and to protect the handler from exposure to diseases, such as rabies. Project personnel are responsible for ensuring that any evidence about determining the cause of death or injury is not unnecessarily disturbed. Reporting the discovery of dead or injured listed species is required in all cases to enable the Service to determine whether the level of incidental take exempted by this BO is exceeded, and to ensure that the terms and conditions are appropriate and effective. Parties finding a dead, injured, or sick specimen of any endangered or threatened species must promptly notify this Service Office.

Reinitiation Notice

This letter concludes consultation for the Project, which qualifies for inclusion in the BO issued to the Federal Transportation Agencies. To maintain this inclusion, a reinitiation of this Project-level consultation is required where the Federal Highway Administration discretionary involvement or control over the Project has been retained (or is authorized by law) and if:

1. the amount or extent of incidental take of Indiana bat is exceeded;
2. new information reveals that the Project may affect listed species or critical habitat in a manner or to an extent not considered in the BO;
3. the Project is subsequently modified in a manner that causes an effect to listed species or designated critical habitat not considered in the BO; or
4. a new species is listed or critical habitat designated that the Project may affect.

Per condition #1 above, the anticipated incidental take is exceeded when:

- the Project removes trees of more than **1.3 acre(s)** of habitat suitable between 100 and 300 feet from the edge of pavement) for the Indiana bat; or
- the Project takes more than 5 Indiana bats resulting from work on the bridge/structure.

In instances where the amount or extent of incidental take is exceeded, the Federal Highway Administration is required to immediately request a reinitiation of this Project-level consultation.

We appreciate your continued efforts to ensure that this Project is fully consistent with all applicable provisions of the BO. If you have any questions regarding our response or if you need additional information, please contact Robin McWilliams Munson at Robin_Mcwilliams@fws.gov.

Sincerely,

SCOTT PRUITT Digitally signed by SCOTT PRUITT
Date: 2021.01.22 10:07:34 -05'00'

Scott Pruitt
Field Supervisor

Cc: (via email)

Meghan Hinkle, INDOT, Indianapolis, IN
Makinna Laymon, INDOT, Indianapolis, IN
Ibat ILF coordinator – to be sent by INDOT at later date

Appendix D: Section 106 of the NHPA

Minor Projects PA Project Assessment Form

Date: 9/22/20 (Revised 1/19/21)

Project Designation Number: 2001172 and 2100049

Route Number: I-65

Project Description: Added Travel Lanes

The proposed project involves the addition of travel lanes along I-65. The project is located along I-65 from approximately 1.33 miles north of SR 25 to approximately 2.43 miles north of SR 43 near Lafayette and Battle Ground in Tippecanoe County, Indiana. Additional length north and south of these limits is included in the project area for median crossovers for maintenance of traffic. The project also includes the deck replacement of the CR 725 N. bridge over I-65, which is approximately 390 feet north of the added travel lane limits.

The project includes the following elements:

1. Travel lane and shoulder pavement replacement;
2. Reconstruction of the SR 43 entrance and exit ramps;
3. Addition of a travel lane in each direction in the median with traffic separated by a concrete barrier;
4. Bridge deck replacement and widening of I-65 northbound bridge and bridge deck overlay and widening of I-65 southbound bridge over 9th Street/CSX Railroad/Burnett Creek/Wabash Heritage Trail (Des. Nos. 1592725 and 1592726). Riprap for scour protection will also be installed at this bridge;
5. Raising the elevation of the Wabash Heritage Trail by a maximum of approximately 6 inches within the existing INDOT right-of-way. The trail is an earthen path under the I-65 bridges over 9th Street/CSX Railroad/Burnett Creek;
6. Bridge deck replacement and widening of I-65 northbound and southbound bridges over Prophets Rock Road (Des. Nos. 1592704 and 1592705);
7. Lowering the elevation of Prophets Rock Road by 3 inches for a length of approximately 600 feet to obtain proper vertical clearance for the I-65 bridges over it;
8. Bridge deck replacement and widening of I-65 northbound and southbound bridges over SR 43 (Note: the environmental impacts of work to these bridges were previously documented in a separate Categorical Exclusion (CE) document under Des. Nos. 1601088 and 1601090);
9. Bridge deck replacement of CR 725 N. bridge over I-65 (Des. No. 1500644); the elevation of CR 725 N. will be raised by a maximum of approximately 3 inches for a length of approximately 500 feet along the road east and west of the bridge to obtain proper vertical clearance over I-65;
10. Replacement of culverts crossing under I-65 and/or construction of median drains, culverts, and detention basins for roadway drainage;

Minor Projects PA Project Assessment Form

11. Guardrail will be installed as needed intermittently along I-65 and Prophets Rock Road;
12. Existing signs are being replaced in kind. Additional signage will be warranted including but not limited to three concrete median mounted overhead cantilever signs at the north limits for the lane ending;
13. Additional lighting will be installed along the SR 43 entrance and exit ramps; and
14. A noise analysis will be completed for the project. As part of this analysis, possible noise barriers could be identified and analyzed to determine if they are reasonable and feasible in accordance with INDOT's Traffic Noise Analysis Procedure (2017).

On, January 19, 2021, INDOT, Cultural Resources was made aware of the following changes to the project scope:

1. Turn lanes will now be lengthened on the exit ramps and an acceleration lane added to the SR 43 to I-65 SB entrance ramp at the I-65/SR 43 interchange;
2. Originally the I-65 SB bridge over 9th Street/CSX RR/Burnett Creek/Wabash Heritage Trail was to receive a deck overlay. That bridge will now receive a deck replacement similar to the NB bridge at that location;
3. The overall project has been split into 2 Des #s for potential funding reasons. Des # 2001172 will include I-65 from 1.33 miles north of SR 25 to 0.8 mile north of SR 43. Des # 2100049 has been created and will include 0.8 mile north of SR 43 to 2.43 miles north of SR 43 (informational, not really a scope change);
4. Des #s for bridges have been updated (informational, not really a scope change)
 - a. 9th Street/CSX RR/Burnett Creek/Wabash Heritage Trail – Des #s. 2002114/2002115
 - b. Prophets Rock Rd. – Des #s 2002116/2002117
 - c. CR 725 over I-65 – Des # 2002364

Feature crossed (if applicable): N/A

City/Township: Tippecanoe Township

County: Tippecanoe County

Information reviewed (please check all that apply):

- General project location map USGS map Aerial photograph Interim Report
 Written description of project area General project area photos Soil survey data
 Previously completed historic property reports Previously completed archaeology reports
 Bridge Inspection Information SHAARD SHAARD GIS Streetview Imagery

Other (please specify):

Please specify all applicable categories and condition(s) (conditions that are applicable are highlighted):

- A-1. Any work on bridges limited to substructure or superstructure elements without replacing, widening, or elevating the superstructure under the conditions listed below (***BOTH Conditions A and B must be met***). This category **does not** include bridge replacement projects (when both superstructure and substructure are removed):

Minor Projects PA Project Assessment Form

- A. The project takes place in previously disturbed soils; *AND*
 - B. With regard to the bridges, at least one of the conditions (i, ii or iii) listed below must be satisfied:
 - i. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
 - ii. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect *AND* the considerations listed in Section IV of the Program Comment do not apply;
 - iii. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.
- A-2. All work within interchanges and within medians of divided highways in previously disturbed soils.
- A-3. Replacement, repair, lining, or extension of culverts and other drainage structures that do not exhibit wood, stone or brick structures or parts therein and are in previously disturbed soils.
- A-4. Roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required.
- A-5. Repair, in-kind replacement or upgrade of existing lighting, signals, signage, and other traffic control devices in previously disturbed soils.
- A-6. Repair, replacement, or upgrade of existing safety appurtenances such as guardrails, barriers, glare screens, and crash attenuators in previously disturbed soils.
- B-3. Construction of added travel, turning, or auxiliary lanes (e.g., bicycle, truck climbing, acceleration and deceleration lanes) and shoulder widening under the following conditions [***BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied***]:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. **Work occurs in previously disturbed soils**; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Minor Projects PA Project Assessment Form

Condition B (Above-Ground Resources)

Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

- B-8. Construction of pedestrian facilities including trails, multi-use paths, greenways, and associated minor activities defined below, under the following conditions [***BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied***]:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs within areas previously disturbed by vertical and horizontal construction activities, including existing roadway, sidewalk, or rail bed, and is not on, within or adjacent to a National Register listed or eligible site; *OR*
- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource.

Activities associated with this category include the following:

- Pavement surface installation, replacement, rehabilitation, resurfacing, and reconstruction work, including widening, laying down of crushed stone or gravel, shoulder treatments, pavement repair, seal coating, pavement grinding, pavement marking, etc.;
- Installation of new signals, signage, and other traffic control devices;
- Installation of new safety appurtenances such as guardrails and barriers;
- Installation of plant materials and hardscape landscaping elements, including, but not limited to bike racks, benches, trash cans, lighting, and other amenities;
- Trail heads and parking lots;
- Installation of pipes, culverts, and pedestrian bridges.

- B-12. Replacement, widening, or raising the elevation of the superstructure on existing bridges, and bridge replacement projects (when both the superstructure and substructure are removed), under the following conditions [***BOTH Condition A, which pertains to Archaeological Resources, and Condition B, which pertains to Above-Ground Resources, must be satisfied***]:

Condition A (Archaeological Resources)

One of the two conditions listed below must be met (*EITHER Condition i or Condition ii must be satisfied*):

- i. Work occurs in previously disturbed soils; *OR*

Minor Projects PA Project Assessment Form

- ii. Work occurs in undisturbed soils and an archaeological investigation conducted by the applicant and reviewed by INDOT Cultural Resources Office determines that no National Register-listed or potentially National Register-eligible archaeological resources are present within the project area. If the archaeological investigation locates National Register-listed or potentially National Register-eligible archaeological resources, then full Section 106 review will be required. Copies of any archaeological reports prepared for the project will be provided to the DHPA and any archaeological site form information will be entered directly into the SHAARD by the applicant. The archaeological reports will also be available for viewing (by Tribes only) on INSCOPE.

Condition B (Above-Ground Resources)

The conditions listed below must be met (***BOTH Condition i and Condition ii must be satisfied***)

- i. Work does not occur adjacent to or within a National Register-listed or National Register-eligible district or individual above-ground resource; ***AND***
- ii. With regard to the subject bridge, at least one of the conditions listed below is satisfied (***AT LEAST one of the conditions a, b or c, must be fulfilled***):
 - a. The latest Historic Bridge Inventory identified the bridge as non-historic (see <http://www.in.gov/indot/2531.htm>);
 - b. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect ***AND*** the considerations listed in Section IV of the Program Comment do not apply;
 - c. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.

Are there any commitments associated with this project? If yes, please explain and include in the Additional Comments Section below. yes no

Does the project result in a de minimis impact to a Section 4(f) protected historic resource? If yes, please explain in the Additional Comments Section below. yes no

Additional Comments:

Above-ground Resources

An INDOT Cultural Resources Office (CRO) historian, who met the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61 performed a desktop review, checking the Indiana Register of Historic Sites and Structures (State Register) and National Register of Historic Places (National Register) lists for Tippecanoe County. No listed properties are located adjacent to the project area.

The Indiana Historic Sites and Structures Inventory (IHSSI) was consulted via the Indiana State Historic Architectural and Archaeological Research Database (SHAARD), and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM). There are no surveyed properties in the project area rated at least "notable" or "outstanding."

The subject bridges were not determined eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005.

Minor Projects PA Project Assessment Form

The project area passes through primarily wooded and agricultural lands. From a review of aerial imagery and streetview, no properties adjacent to the project area appear to have the requisite integrity or significance to be considered eligible for the National Register.

Based on the available information, as summarized above, no above-ground concerns exist.

Archaeological Resources

An INDOT Cultural Resources Office (CRO) archaeologist, who meets the Secretary of the Interior's Professional Qualification Standards as per 36 CFR Part 61, performed a desktop review of the project area and completed an archaeological assessment. The proposed project is limited to adding northbound and southbound travel lanes in median of I-65 from the bridge over Wabash River to 0.8 miles north of SR 43 and improvements to the bridge decks, culverts, signage, lighting, and guardrail within the corridor. Approximately half the project area from SR 43 interchange to the Wabash River Bridge consists of soils classified loamy Udorthents which are fill soils used in the construction of interstates and interchanges. Other soil disturbances included roadside and median ditches, grade separations, road cuts, and utility easements. According to SHAARD GIS, there are no archaeological sites recorded in or adjacent to the proposed project area. Given these factors, there are no archaeological concerns.

The scope changes described on January 19, 2021 were reviewed and determined to fall under Categories A-2 and B-12.

Accidental Discovery: If any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, construction within 100 feet of the discovery will be stopped, and the INDOT Cultural Resources Office and the Division of Historic Preservation and Archaeology will be notified immediately.

INDOT Cultural Resources staff reviewer(s): Shaun Miller and Patrick Carpenter

****Be sure to attach this form to the National Environmental Policy Act documentation for this project. Also, the NEPA documentation shall reference and include the description of the specific stipulation in the PA that qualifies the project as exempt from further Section 106 review.*

From: [Carpenter, Patrick A](#)
To: [Kia Gillette](#)
Cc: [Miller, Shaun \(INDOT\)](#); [Ahmed, Arshad](#); [Khan, Asfahan](#); [Branigin, Susan](#)
Subject: RE: I-65 Added Travel Lanes, Tippecanoe County (Lead Des. No. 2001172) - Section 106 MPPA Documentation
Date: Tuesday, January 19, 2021 1:23:26 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[Minor Project PA Determination Form B-3andB-8 2001172and2100049 updated.pdf](#)

Hi Kia,

Please see attached an updated MPPA form that includes the updated information outlined below.

Please let us know if any questions.

Thank you,

Patrick Carpenter
Section 106 Specialist, Cultural Resources Office
Environmental Services
Indiana Department of Transportation
100 N Senate Ave., IGCN- Room N758-ES
Indianapolis, IN 46204-2216
317-416-7960*

*Please note new phone number.

From: Kia Gillette <kgillette@HNTB.com>
Sent: Tuesday, January 19, 2021 6:54 AM
To: Carpenter, Patrick A <PACarpenter@indot.IN.gov>
Cc: Miller, Shaun (INDOT) <smiller@indot.IN.gov>; Ahmed, Arshad <ArAhmed@indot.IN.gov>; Khan, Asfahan <akhan@indot.IN.gov>; Branigin, Susan <SBranigin@indot.IN.gov>
Subject: RE: I-65 Added Travel Lanes, Tippecanoe County (Lead Des. No. 2001172) - Section 106 MPPA Documentation

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Patrick,

We wanted to notify you of some minor changes in the scope of the I-65 Added Travel Lanes, Tippecanoe County (Des # 2001172). We believe these changes should not affect the Section 106 MPPA applicability: however, we did want to run them by you to be sure.

The following minor changes have been made to the project:

1. Turn lanes will now be lengthened on the exit ramps and an acceleration lane added to the SR 43 to I-65 SB entrance ramp at the I-65/SR 43 interchange (Category A-2 or B-3).
2. Originally the I-65 SB bridge over 9th Street/CSX RR/Burnett Creek/Wabash Heritage Trail was to receive a deck overlay. That bridge will now receive a deck replacement similar to the NB bridge at that location (Category A-1).
3. The overall project has been split into 2 Des #s for potential funding reasons. Des # 2001172 will include I-65 from 1.33 miles north of SR 25 to 0.8 mile north of SR 43. Des # 2100049 has been created and will include 0.8 mile north of SR 43 to 2.43 miles north of SR 43 (informational, not really a scope change).
4. Des #s for bridges have been updated (informational, not really a scope change):
 - a. 9th Street/CSX RR/Burnett Creek/Wabash Heritage Trail – Des #s. 2002114/2002115
 - b. Prophets Rock Rd. – Des #s 2002116/2002117
 - c. CR 725 over I-65 – Des # 2002364

Please let me know if you have any questions or need any additional information for the MPPA determination.

Thanks,
Kia

Kia Gillette

Environmental Project Manager

Email kgillette@hntb.com

From: Carpenter, Patrick A <[PACarpenter@indot.IN.gov](mailto:PA_Carpenter@indot.IN.gov)>

Sent: Tuesday, September 22, 2020 1:22 PM

To: Kia Gillette <kgillette@HNTB.com>

Cc: Miller, Shaun (INDOT) <smiller@indot.IN.gov>; Ahmed, Arshad <ArAhmed@indot.IN.gov>; Khan, Asfahan <akhan@indot.IN.gov>; Branigin, Susan <SBranigin@indot.IN.gov>

Subject: RE: I-65 Added Travel Lanes, Tippecanoe County (Lead Des. No. 2001172) - Section 106 MPPA Documentation

Kia,

Please find attached the completed Minor Projects PA form for the above-referenced project. With this MPPA form, Section 106 is considered complete.

Please note that if there are any project changes or modifications we should be consulted to determine if the MPPA still applies.

Do not hesitate to contact us if you have any questions.

Thank you,

Patrick Carpenter
Section 106 Specialist, Cultural Resources Office

Appendix E: Red Flag and Hazardous Materials



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 232-5113
FAX: (317) 233-4929

Eric Holcomb, Governor
Joe McGuinness,
Commissioner

Date: October 12, 2020

To: Site Assessment & Management
Environmental Policy Office - Environmental Services Division
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

From: Caroline Tegeler
HNTB Corporation
111 Monument Circle, Suite 1200
Indianapolis, IN, 46204
ctegeler@hntb.com

Re: RED FLAG INVESTIGATION
DES # 2001172, State Project
Added Travel Lanes Project
I-65 - from 1.33 Miles North of SR 25 to 2.43 Miles North of SR 43
Tippecanoe County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: The Federal Highway Administration (FHWA) and the Indiana Department of Transportation (INDOT), Crawfordsville District are planning to proceed with an added travel lanes project on I-65 from approximately 1.33 miles north of SR 25 to approximately 2.43 miles north of SR 43 near Lafayette and Battle Ground in Tippecanoe County, Indiana. Additional length north and south of these limits is included in the project area for median crossovers for maintenance of traffic. This project also includes the bridge deck replacement of the CR 725 N. bridge over I-65 which is approximately 390 feet north of the added travel lane limits. Specific project activities would include the following:

1. Travel lane and shoulder pavement replacement;
2. Reconstruction of the SR 43 entrance and exit ramps;
3. Addition of a travel lane in each direction in the median with traffic separated by a concrete barrier;
4. Bridge deck replacement and widening of I-65 northbound bridge and bridge deck overlay and widening of I-65 southbound bridge over 9th Street/CSX Railroad/Burnett Creek/Wabash Heritage Trail (Des. Nos. 1592725 and 1592726, Bridge Nos. I65-177-02402 BNBL, I65-177-02402 JCSB):

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5. Bridge deck replacement and widening of I-65 northbound and southbound bridges over Prophets Rock Road (Des. Nos. 1592704 and 1592705, Bridge Nos. I65-178-05485 BNBL, I65-178-05485 JBSB);
6. Bridge deck replacement and widening of I-65 northbound and southbound bridges over SR 43 (Note: the environmental impacts of work to these bridges were previously documented in a separate Categorical Exclusion (CE) document under Des. Nos. 1601088 and 1601090, Bridge Nos. I65-178-05486 JBNB, I65-178-05486 BSBL);
7. Bridge deck replacement of CR 725 N. bridge over I-65 (Des. No. 1500644, Bridge No. I65-180-05489);
8. Replacement of culverts crossing under I-65 and/or construction of median drains, culverts, and detention basins for roadway drainage.

Bridge and/or Culvert Project: Yes No Structure # I65-177-02402 BNBL, I65-177-02402 JCSB, I65-178-05485 BNBL, I65-178-05485 JBSB, I65-178-05486 JBNB, I65-178-05486 BSBL, I65-180-05489

If this is a bridge project, is the bridge Historical? Yes No , Select Non-Select

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary # Acres___ Permanent # Acres___ Not Applicable

Type of excavation: Excavation for construction of the project will be approximately four feet below the ground surface (bgs) for the mainline excavation, up to 6 feet bgs in the median for drainage, up to 6 feet bgs at bridge piers and end bents, up to 8 feet bgs for replacement of lighting, 20 feet bgs for installation of three cantilevered signs at the north end of the project area, and up to 12 feet bgs for installation of ground mounted signs.

Maintenance of traffic: The maintenance of traffic plan will include maintaining two lanes of traffic in each direction through the majority of construction. Intermittent lane restrictions will be implemented on I-65. Access to and from the SR 43 interchange will be provided during the majority of construction; however, intermittent closures will be required to reconstruct the existing ramps. 9th Street, Prophets Rock Road, and CR 725 will be closed at I-65 for approximately three to six months and detours will be required for through traffic.

Work in waterway: Yes No Below ordinary high water mark: Yes No

State Project: LPA:

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	2*	Recreational Facilities	N/A
Airports ¹	N/A	Pipelines	4
Cemeteries	4	Railroads	1
Hospitals	N/A	Trails	2
Schools	N/A	Managed Lands	5

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

Explanation:

Religious Facilities*: Two (2) religious facilities are located within the 0.5 mile search radius. Although not mapped on GIS, Immanuel Reformed Presbyterian Church is located approximately 0.34 mile northwest of the SR 43 ramp reconstruction project area. Coordination with Immanuel Reformed Presbyterian Church will occur. No impact is expected.

Cemeteries: Four (4) cemeteries are located within the 0.5 mile search radius. The nearest cemetery, Burnett’s Creek Cemetery, is located approximately 0.07 mile east of the central portion of the project area. Coordination with INDOT Cultural Resources will occur.

Pipelines: Four (4) pipeline segments are located within the 0.5 mile search radius. The nearest segment, associated with Indiana Gas Co. Inc., crosses the project area running parallel to SR 43. Coordination with INDOT Utilities and Railroads should occur.

Railroads: One (1) railroad segment is located within the 0.5 mile search radius. One railroad segment, associated with CSX RR, crosses the project area. Coordination with INDOT Utilities and Railroads should occur.

Trails: Two (2) trail segments are located within the 0.5 mile search radius. One (1) open trail segment, Wabash Heritage Trail, crosses under I-65 along the west side of Burnett Creek under the bridge over 9th Street and the CSX railroad. One (1) planned trail segment, Prophetstown Bike Trail Swisher Road Corridor, is located along Swisher Road crossing over I-65. Coordination with State Parks and Reservoirs and Tippecanoe County Parks and Recreation Department will occur.

Managed Lands: Five (5) managed lands are located within the 0.5 mile search radius. Boehing Nature Reserve, Tippecanoe Battlefield Memorial Park, and Prophetstown State Park are located adjacent to the project area. Coordination with Tippecanoe County Parks and Recreation, Niches Land Trust, and Prophetstown State Park is recommended.

WATER RESOURCES TABLE AND SUMMARY

Water Resources			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	1
Karst Springs	N/A	NWI - Wetlands	117
Canal Structures – Historic	N/A	Lakes	19
NPS NRI Listed	N/A	Floodplain - DFIRM	21
NWI-Lines	21	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	14	Sinkhole Areas	N/A
Rivers and Streams	35	Sinking-Stream Basins	N/A

Explanation:

NWI-Lines: Twenty-one (21) NWI-Lines are located within the 0.5 mile search radius. Two NWI-Lines, both associated with Burnett Creek, are located within the project area. A Waters of the US Report will be prepared and coordination with INDOT Ecology and Waterway Permitting will occur.

IDEM 303d Listed Streams and Lakes (Impaired): Fourteen (14) IDEM 303d Listed Streams and Lakes are located within the 0.5 mile search radius. Burnett Creek is located within the project area and is listed as impaired for Impaired Biotic Communities (IBC) and E. coli. Wabash River is located within the project area and is listed as impaired for IBC, E. coli, and nutrients. Concerning IBC and nutrients, Best Management Practices (BMPs) will be used to avoid further degradation to the stream. Regarding E. coli, workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

Rivers and Streams: Thirty-five (35) river and stream segments are located within the 0.5 mile search radius. Four (4) stream segments, one representing Wabash River, two representing Burnett Creek, and one representing an Unnamed Tributary (UNT), are located within the project area. A Waters of the U.S. Report will be prepared and coordination with INDOT Ecology and Waterway Permitting will occur.

Canal Routes – Historic: One (1) Canal Route is located within the 0.5 mile search radius. The Wabash-Erie Canal is located within the project area where only maintenance of traffic will occur. No impact is expected.

NWI – Wetlands: One hundred and seventeen (117) wetlands are located within the 0.5 mile search radius. Eight (8) wetlands are located within the project area. A Waters of the U.S. Report will be prepared and coordination with INDOT Ecology and Waterway Permitting will occur.

Lakes: Nineteen (19) lakes are located within the 0.5 mile search radius. The nearest lake is located approximately 0.08 mile east of the northern portion of the project area. No impact is expected.

Floodplain – DFIRM: Twenty-one (21) floodplain polygons are located within the 0.5 mile search radius. The project area is located within seven (7) of the floodplain polygons. Coordination with INDOT ESD Ecology and Waterway Permitting will occur.

URBANIZED AREA BOUNDARY SUMMARY

Explanation: This project lies within the Tippecanoe County UAB. Post construction Storm Water Quality BMPs may need to be considered. Coordination with appropriate MS4s will be necessary.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	N/A	Mineral Resources	1*
Mines – Surface	N/A	Mines – Underground	N/A

Explanation:

Mineral Resources: Although the symbol is mapped outside of the half mile search radius on the GIS layer, one (1) mineral resource was identified within the 0.5 mile search radius. The US Aggregates, Inc. Swisher Plant Sand and Gravel Pit is located 0.11 mile east of the project area. No impact is expected.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	1	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	N/A	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	5	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	12
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	1
Leaking Underground Storage (LUST) Sites	2	Notice of Contamination Sites	N/A

*Indicates site was incorrectly categorized in database.

Explanation:

RCRA Generator/TSD: One (1) RCRA Generator/TSD site is located within the 0.5 mile search radius. The RCRA Generator/TSD, Flaconte Inc. (AI ID# 56399), is located adjacent to the project area at the corner of I-65 and Swisher Road within the southern portion of the search radius at 4602 Swisher Road, West Lafayette, Indiana 47201. According to an IDEM Office of Land Quality authorization document published on January 16, 2004, the facility is no longer in need of a RCRA ID number and the facility is out of business. No impact is expected.

Underground Storage Tank (UST) Sites: Five (5) UST Sites are located within the 0.5 mile search radius. The nearest UST site, Gas America 73, is located approximately 0.20 mile north of the northern portion of the SR 43 ramp reconstruction area at 5829 N. SR 43, West Lafayette, IN 47906 (AI ID# 55866). IDEM conducted an UST Inspection on March 16, 2016, and the facility was found to be in compliance with equipment, operating, and maintenance requirements set forth in Indiana's UST Rule 329 IAC 9. No impact is expected.

Leaking Underground Storage Tank (LUST) Sites: Two (2) LUST sites are located within the 0.5 mile search radius.

- McCoy Enterprises, Inc. (AI ID No. 56480 and FID No. 11578) is located approximately 0.25 mile north of the project area at 5850-5851 SR 43 North, West Lafayette, Indiana 47906. This site continues to be monitored for a petroleum release with contamination in the area surrounding the site and extends into the ROW. If excavation occurs in this area, proper handling removal and disposal of soil and/or groundwater will be necessary.
- Lafayette Interstate Shell (AI ID No. 54836 and FID No. 16649) is located approximately 0.28 mile north of the project area at SR 43 North, West Lafayette, Indiana 47906. IDEM issued a Site Closure Letter on May 28, 2020, that stated there is existing petroleum contamination in the subsurface of the site and within the east ROW of SR 43. If excavation occurs in this area, proper handling, removal and disposal of soil and/or groundwater will be necessary.

NPDES Facilities: Twelve (12) NPDES Facilities are located within the 0.5 mile search radius. The I-65 over Wabash River Clearing Contract is located within the southern portion of the project area at I-65 mile marker 176, Lafayette, IM 47905.

This permit for this site is terminated. No impact is expected. Another NPDES facility (Speedway LLC Store #8064) is mapped within the SR 43 interchange; however, this facility is incorrectly mapped and no impact is expected.

NPDES Pipes: One (1) NPDES pipe is located within the 0.5 mile search radius. The NPDES pipe is located approximately 0.24 mile north of the SR 43 ramp reconstruction area. This NPDES pipe is associated with McCoy Enterprises Inc. J & Battle Service. This pipe outlets to an adjacent unnamed tributary and has an active permit. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The Tippecanoe County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did indicate the presence of ETR species within the 0.5 mile search radius. Coordination with USFWS and IDNR will occur.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5-mile of the project area. The project area is located in a residential, agricultural, recreational, and forested area. The August 26, 2019, inspection report for bridge # I65-177-02402 JCSB states that no evidence of bats was seen or heard under the bridge. The August 26, 2019, inspection report for bridge #I65-177-02402 BNBL states that no evidence of bats was seen or heard under the bridge. The August 26, 2019, inspection reports for bridge # I65-178-05485 BNBL and I65-178-05485 JBSB states that no evidence of bats was seen or heard under the bridges. The August 22, 2019 inspection reports for bridge # I65-178-05486 BSBL and I65-178-05486 JBNB state that no evidence of bats was seen or heard under the bridges. The August 21, 2019, inspection report for bridge # I65-180-05489 states that no evidence of bats was seen or heard under the bridge. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects."

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Religious Facilities: Immanuel Reformed Presbyterian Church is located approximately 0.34 mile northwest of the SR 43 ramp reconstruction project area. Coordination with Immanuel Reformed Presbyterian Church will occur.

Cemeteries: Burnett's Creek Cemetery is located approximately 0.07 mile east of the central portion of the project area. Coordination with INDOT Cultural Resources will occur.

Pipelines: The Indiana Gas Co. Inc. pipeline crosses the project area. Coordination with INDOT Utilities and Railroads should occur.

Railroads: One railroad segment, associated with CSX RR, crosses the project area. Coordination with INDOT Utilities and Railroads should occur.

Trails: One (1) open trail segment, Wabash Heritage Trail, crosses under I-65 along the west side of Burnett Creek under the bridge over 9th Street and the CSX railroad. One (1) planned trail segment, Prophetstown Bike Trail Swisher Road Corridor, is located along Swisher Road crossing over I-65. Coordination with State Parks and Reservoirs and Tippecanoe County Parks and Recreation Department will occur.

Managed Lands: Boehing Nature Reserve, Tippecanoe Battlefield Memorial Park, and Prophetstown State Park are located adjacent to the project area. Coordination with Tippecanoe County Parks and Recreation, Niches Land Trust, and Prophetstown State Park is recommended.

WATER RESOURCES:

The presence of following water resources will require the preparation of a Waters of the U.S. Report and coordination with INDOT ES Ecology and Waterway Permitting:

- Two NWI-Lines, both associated with Burnett Creek, are located within the project area.
- Four (4) stream segments, one representing Wabash River, two representing Burnett Creek, and one representing an Unnamed Tributary (UNT), are located within the project area.
- Eight (8) wetlands are located within the project area.

Floodplain – DFIRM: The project area is located within seven (7) floodplain polygons. Coordination with INDOT ESD Ecology and Waterway Permitting will occur.

IDEM 303d Listed Streams and Lakes (Impaired): Burnett Creek is located within the project area and is listed as impaired for IBC and E. coli. Wabash River is located within the project area and is listed as impaired for IBC, E. coli, and nutrients. Concerning IBC and nutrients, Best Management Practices (BMPs) will be used to avoid further degradation to the stream. Regarding E. coli, workers who are working in or near water with E. coli should take care to wear appropriate PPE, observe proper hygiene procedures, including regular hand washing, and limit personal exposure.

URBANIZED AREA BOUNDARY:

This project lies within the Tippecanoe County UAB. Post construction Storm Water Quality BMPs may need to be considered. Coordination with the appropriate MS4s will be necessary.

HAZARDOUS MATERIAL CONCERNS:

Leaking Underground Storage Tank (LUST) Sites: Two (2) LUST sites are located within the 0.5 mile search radius.

- McCoy Enterprises, Inc. (AI ID No. 56480 and FID No. 11578) is located approximately 0.25 mile north of the project area at 5850-5851 SR 43 North, West Lafayette, Indiana 47906. This site continues to be monitored for a petroleum release with contamination in the area surrounding the site and extends into the ROW. If excavation occurs in this area, proper handling removal and disposal of soil and/or groundwater will be necessary.
- Lafayette Interstate Shell (AI ID No. 54836 and FID No. 16649) is located approximately 0.28 mile north of the project area at SR 43 North, West Lafayette, Indiana 47906. IDEM issued a Site Closure Letter on May 28, 2020, that stated there is existing petroleum contamination in the subsurface of the site and within the east ROW of SR 43. If excavation occurs in this area, proper handling, removal and disposal of soil and/or groundwater will be necessary.

ECOLOGICAL INFORMATION:

Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to “Using the USFWS’s IPaC System for Listed Bat Consultation for INDOT Projects.”

Nicole Fohey Breting Digitally signed by Nicole Fohey-Breting
Date: 2020.10.14 12:39:50 -04'00' (Signature)

INDOT Environmental Services concurrence:

Prepared by:
Caroline Tegeler
Scientist
HNTB Corporation

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

INFRASTRUCTURE: YES

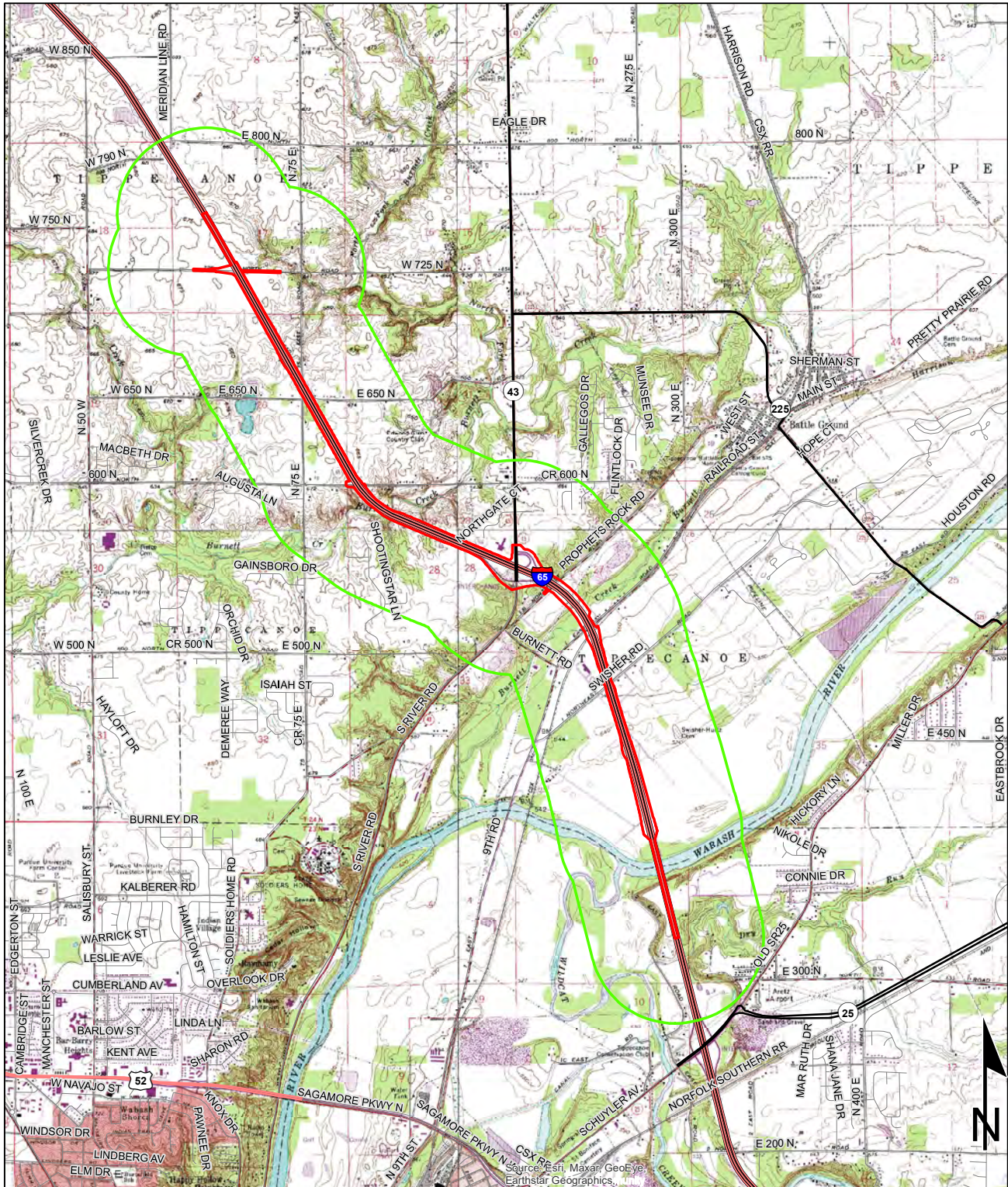
WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: YES

MINING/MINERAL EXPLORATION: YES

HAZARDOUS MATERIAL CONCERNS: YES

Red Flag Investigation - Site Location
I-65, 1.33 Miles North of SR 25 to 2.43 Miles North of SR 43
Des. No. 2001172, Added Travel Lanes
Tippecanoe County, Indiana



Sources: 0.75 0.375 0 0.75 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

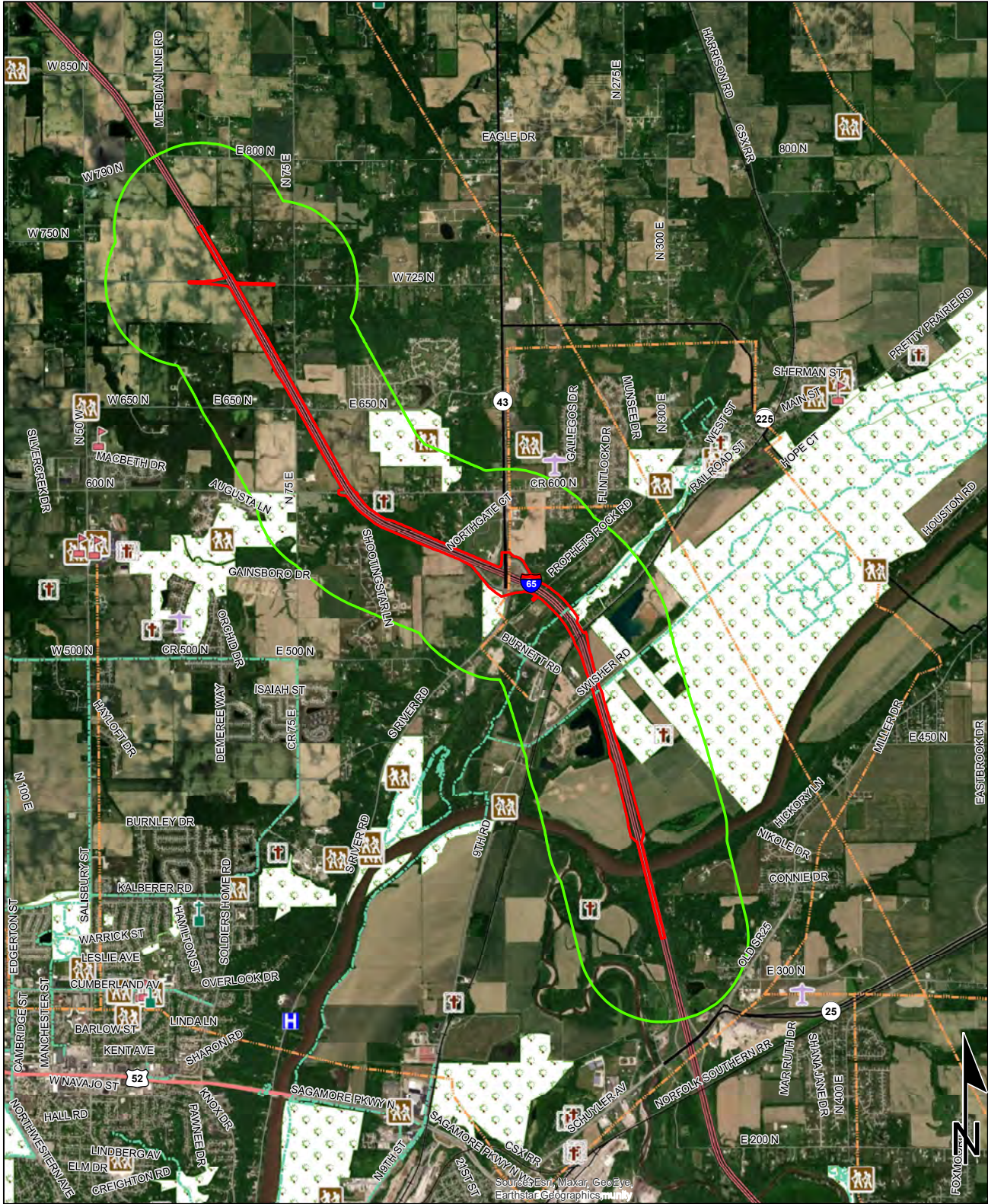
**LAFAYETTE EAST & WEST,
 BROOKSTON & BROOKSTON SW
 QUADRANGLE INDIANA
 7.5 MINUTE SERIES
 (TOPOGRAPHIC)**

Red Flag Investigation - Infrastructure

I-65, 1.33 Miles North of SR 25 to 2.43 Miles North of SR 43

Des. No. 2001172, Added Travel Lanes

Tippecanoe County, Indiana



Sources:

Non Orthophotography

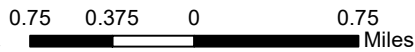
Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data

(www.indianamap.org)

Map Projection: UTM Zone 16 N Map Datum: NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



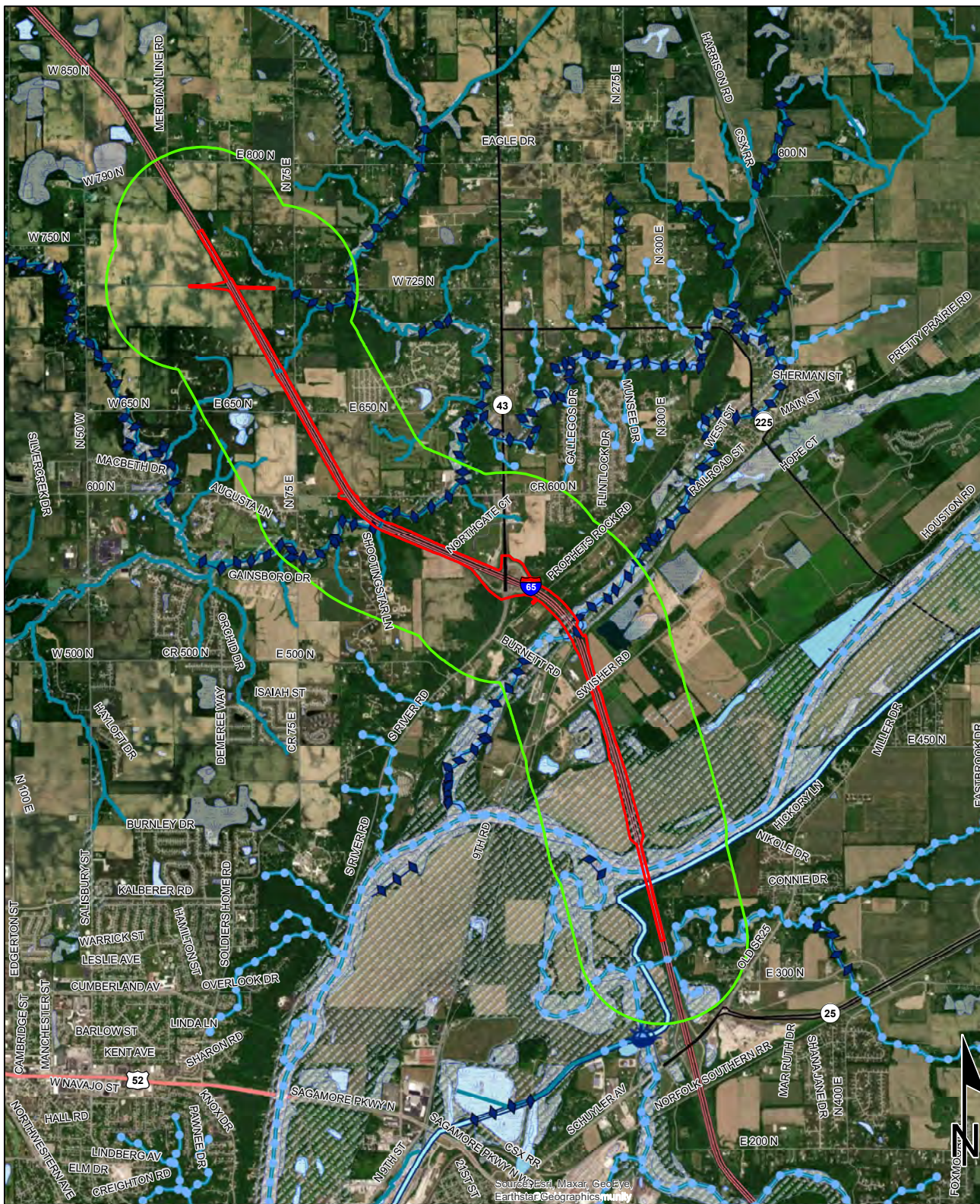
	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

Red Flag Investigation - Water Resources

I-65, 1.33 Miles North of SR 25 to 2.43 Miles North of SR 43

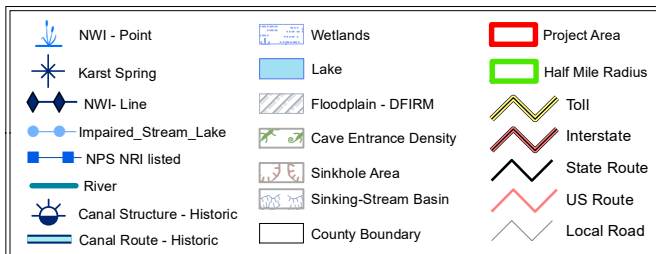
Des. No. 2001172, Added Travel Lanes

Tippecanoe County, Indiana



Sources: 0.75 0.375 0 0.75 Miles
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

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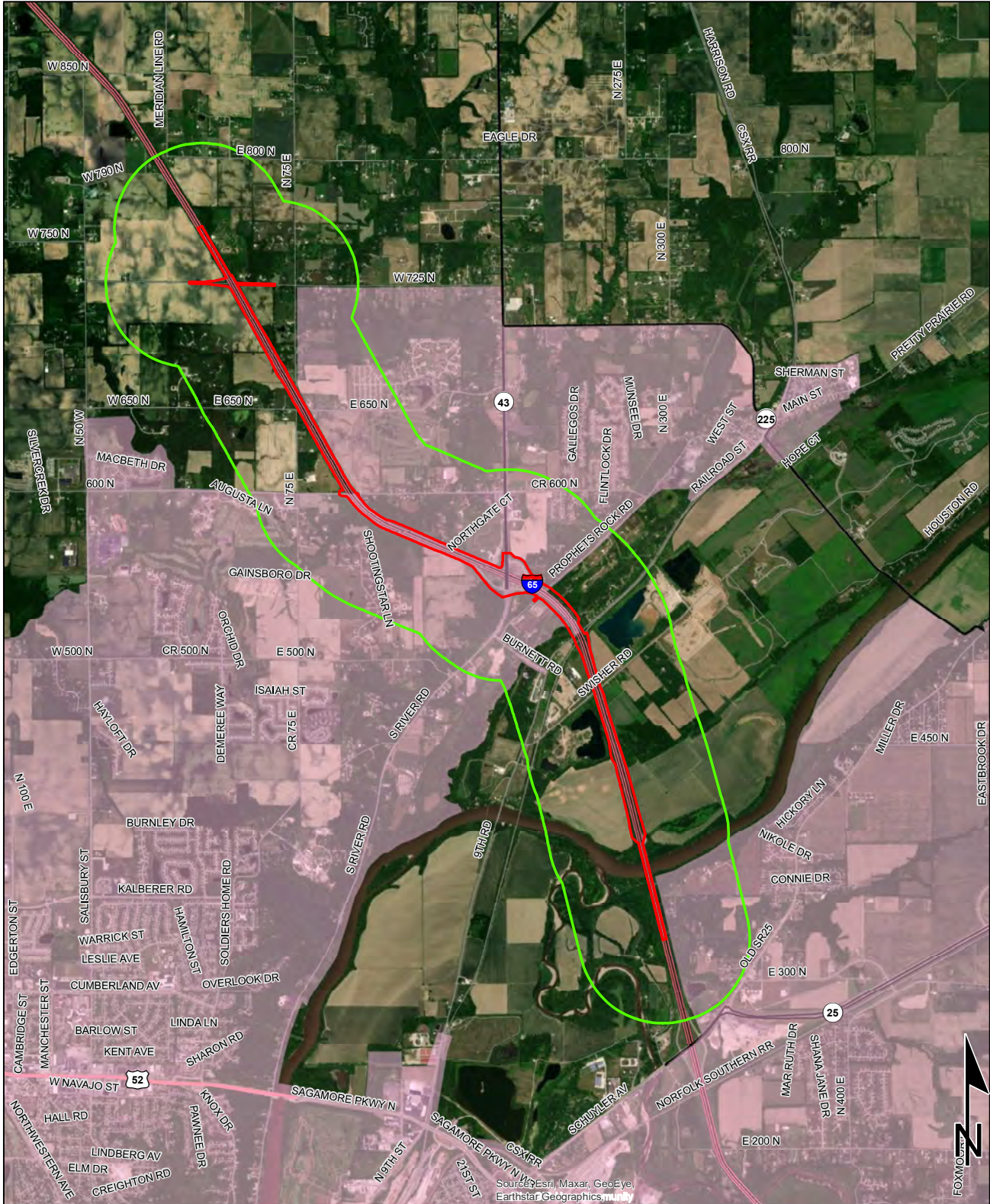


Red Flag Investigation - Urbanized Area Boundary

I-65, 1.33 Miles North of SR 25 to 2.43 Miles North of SR 43

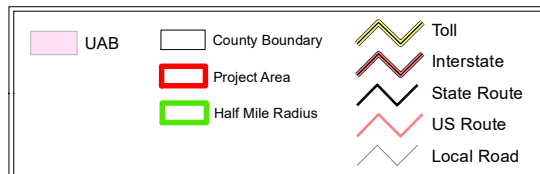
Des. No. 2001172, Added Travel Lanes

Tippecanoe County, Indiana



Sources:
Non Orthophotography Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

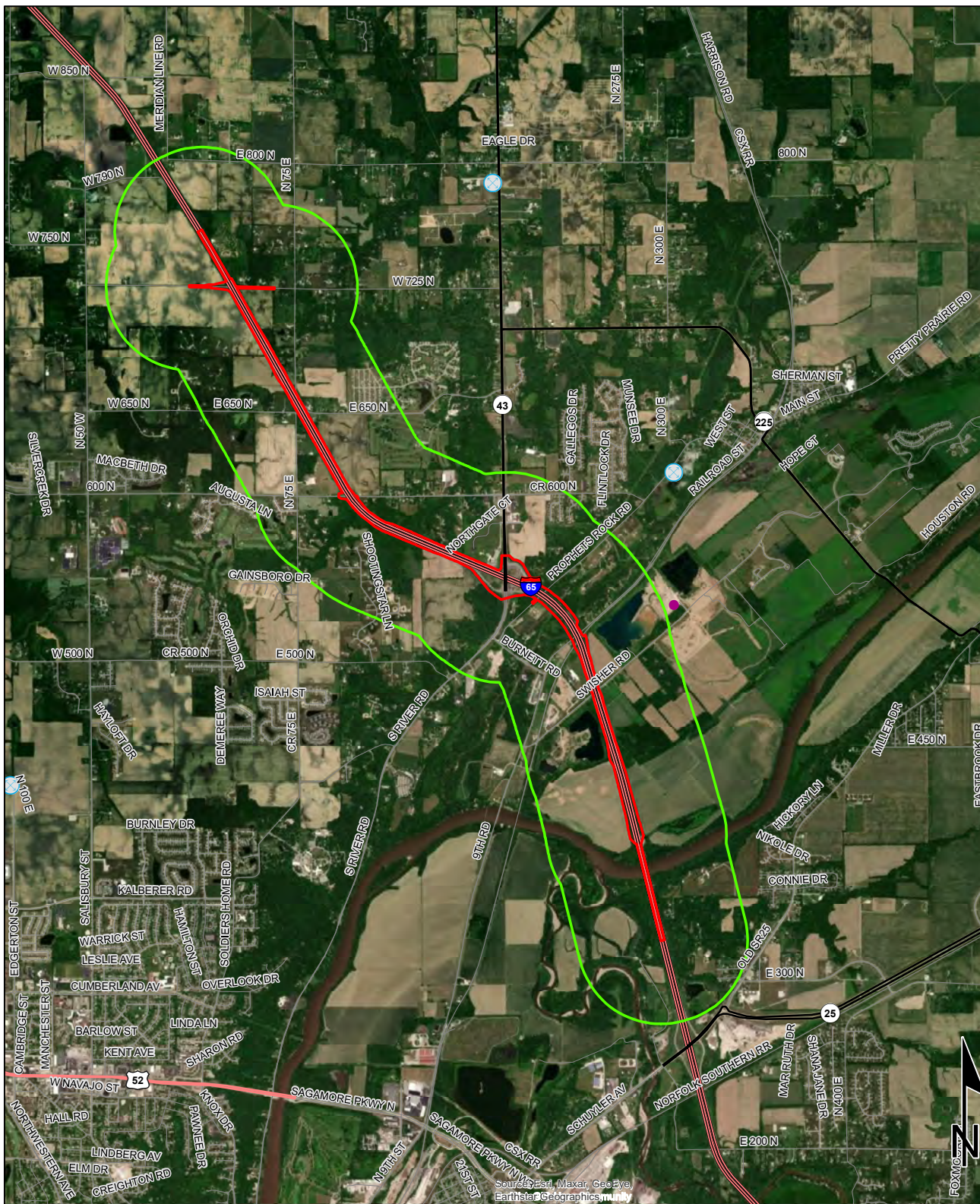


Red Flag Investigation - Mining/Mineral Exploration

I-65, 1.33 Miles North of SR 25 to 2.43 Miles North of SR 43

Des. No. 2001172, Added Travel Lanes

Tippecanoe County, Indiana



Sources: 0.75 0.375 0 0.75 Miles

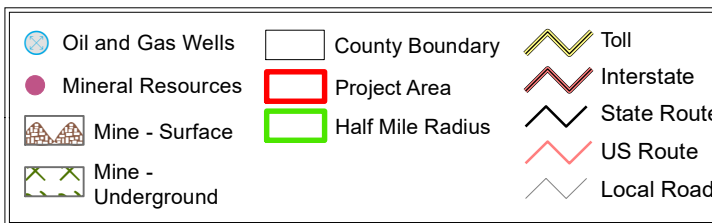
Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

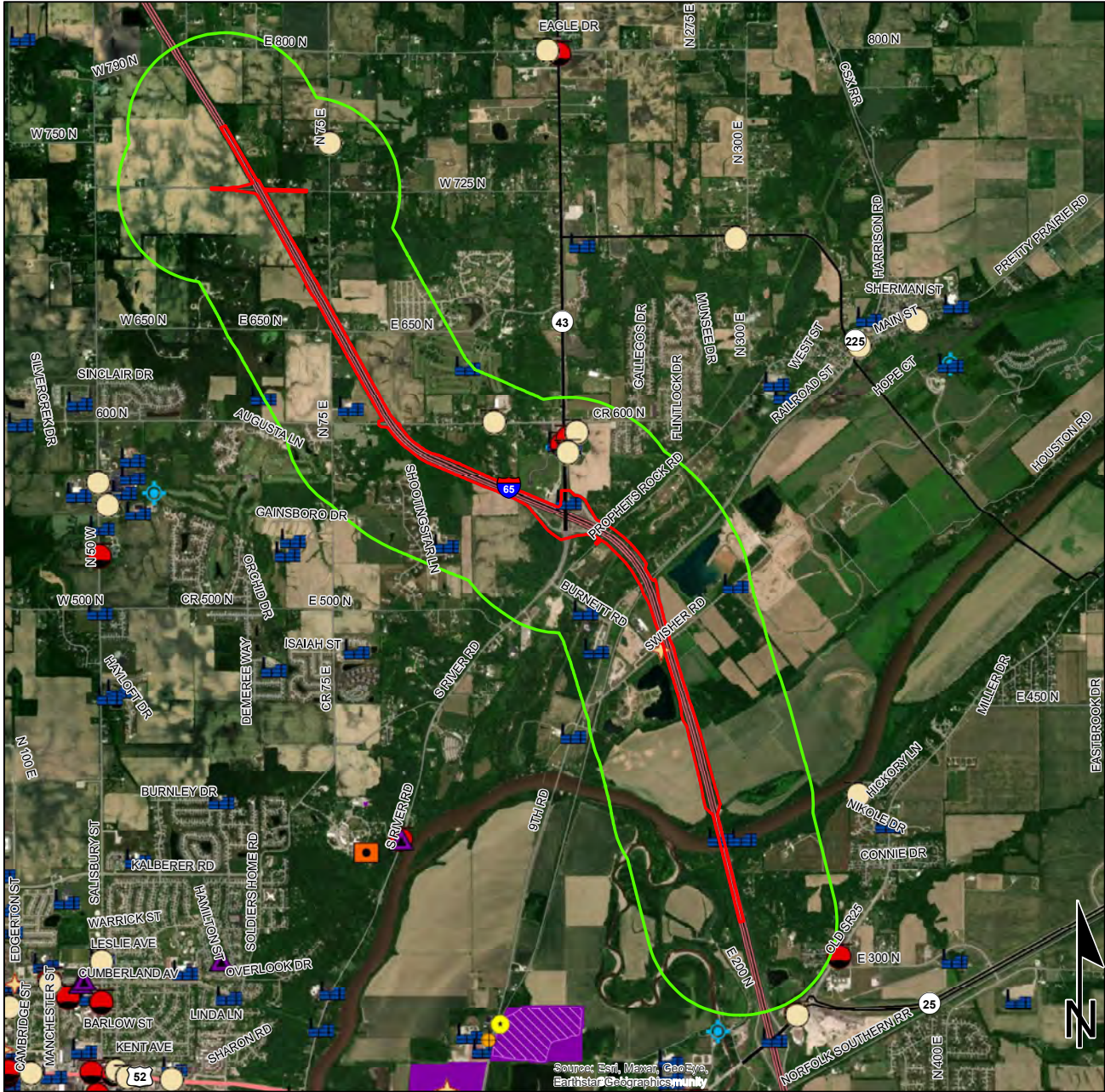


Red Flag Investigation - Hazardous Material Concerns

I-65, 1.33 Miles North of SR 25 to 2.43 Miles North of SR 43

Des. No. 2001172, Added Travel Lanes

Tippecanoe County, Indiana



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation		Septage Waste Site		Project Area
	Notice_of_Contamination		Solid Waste Landfill		Half Mile Radius
	Construction/Demolition Site		State Cleanup Site		Toll
	Infectious/Medical Waste Site		Superfund		Interstate
	Leaking Underground Storage Tank		Tire Waste Site		State Route
	Manufactured Gas Plant		Underground Storage Tank		US Route
	NPDES Facilities		Voluntary Remediation Program		Local Road
	NPDES Pipe Locations		Waste Transfer Station		
	Open Dump Waste Site				

0.75 0.375 0 0.75 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.
Des. No. 2001172

Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 Appendix E, Page 4 of 18

Indiana County Endangered, Threatened and Rare Species List

County: Tippecanoe



Species Name	Common Name	FED	STATE	GRANK	SRANK
Insect: Plecoptera (Stoneflies)					
<i>Isogenoides varians</i>	Rock Island Springfly		SE	G3G4	S1
<i>Pteronarcys dorsata</i>	American Salmonfly		SE	G5	S1
Mollusk: Bivalvia (Mussels)					
<i>Alasmodonta viridis</i>	Slippershell Mussel			SSC G4G5	S3
<i>Cyprogenia stegaria</i>	Eastern Fanshell Pearlymussel	LE	SE	G1Q	S1
<i>Epioblasma rangiana</i>	Northern Riffleshell	LE	SE	G1	S1
<i>Epioblasma torulosa</i>	Tubercled Blossom	LE	SX	GX	SX
<i>Epioblasma triquetra</i>	Snuffbox	LE	SE	G3	S1
<i>Eurynia dilatata</i>	Spike			SSC G5	S4
<i>Fusconaia subrotunda</i>	Longsolid	C	SX	G3	SX
<i>Lampsilis fasciola</i>	Wavyrayed Lampmussel			SSC G5	S3
<i>Lampsilis ovata</i>	Pocketbook			SSC G5	S2
<i>Leptodea leptodon</i>	Scaleshell	LE	SX	G1G2	SX
<i>Ligumia recta</i>	Black Sandshell			SSC G4G5	S2
<i>Obovaria retusa</i>	Ring Pink	LE	SX	G1	SX
<i>Obovaria subrotunda</i>	Round Hickorynut	C	SE	G4	S1
<i>Plethobasus cicatricosus</i>	White Wartyback	LE	SX	G1	SX
<i>Plethobasus cyphus</i>	Sheepnose	LE	SE	G3	S1
<i>Pleurobema clava</i>	Clubshell	LE	SE	G1G2	S1
<i>Pleurobema cordatum</i>	Ohio Pigtoe			SSC G4	S2
<i>Pleurobema plenum</i>	Rough Pigtoe	LE	SE	G1	S1
<i>Pleurobema rubrum</i>	Pyramid Pigtoe			SX G2G3	SX
<i>Potamilus capax</i>	Fat Pocketbook	LE	SE	G2	S1
<i>Ptychobranchus fasciolaris</i>	Kidneyshell			SSC G4G5	S2
<i>Simpsonaias ambigua</i>	Salamander Mussel	C	SSC	G3	S2
<i>Theliderma cylindrica</i>	Rabbitsfoot	LT	SE	G3G4	S1
<i>Toxolasma lividus</i>	Purple Lilliput	C	SSC	G3Q	S2
<i>Villosa fabalis</i>	Rayed Bean	LE	SE	G2	S1
Insect: Coleoptera (Beetles)					
<i>Lissobiops serpentinus</i>	A Rove Beetle		SE	GNR	S1
Insect: Ephemeroptera (Mayflies)					
<i>Paracloeodes minutus</i>	A Small Minnow Mayfly		WL	G5	S3
Insect: Hymenoptera					
<i>Bombus affinis</i>	Rusty-patched Bumble Bee	LE	SE	G2	S1
Insect: Lepidoptera (Butterflies & Moths)					
<i>Speyeria idalia</i>	Regal Fritillary	C	SE	G3?	S1S2
Insect: Mecoptera					
<i>Merope tuber</i>	Earwig Scorpionfly		SE	G3G5	S1

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
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Indiana County Endangered, Threatened and Rare Species List

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Species Name	Common Name	FED	STATE	GRANK	SRANK
Insect: Odonata (Dragonflies & Damselflies)					
<i>Erpetogomphus designatus</i>	Eastern Ringtail		ST	G5	S2
<i>Somatochlora tenebrosa</i>	Clamp-tipped Emerald		SR	G5	S2S3
Fish					
<i>Percina evides</i>	Gilt Darter		SE	G4	S1
Amphibian					
<i>Hemidactylium scutatum</i>	Four-toed Salamander		SSC	G5	S2
<i>Lithobates blairi</i>	Plains Leopard Frog		SE	G5	S1
Reptile					
<i>Clemmys guttata</i>	Spotted Turtle	C	SE	G5	S2
<i>Emydoidea blandingii</i>	Blanding's Turtle	C	SE	G4	S2
<i>Opheodrys vernalis</i>	Smooth Green Snake		SE	G5	S2
<i>Sistrurus catenatus</i>	Eastern Massasauga	LT	SE	G3	S2
<i>Terrapene carolina carolina</i>	Eastern Box Turtle		SSC	G5T5	S3
<i>Terrapene ornata ornata</i>	Ornate Box Turtle		SE	G5T5	S1
Bird					
<i>Aimophila aestivalis</i>	Bachman's Sparrow			G3	SXB
<i>Ammodramus henslowii</i>	Henslow's Sparrow		SE	G4	S3B
<i>Asio flammeus</i>	Short-eared Owl		SE	G5	S2
<i>Asio otus</i>	Long-eared Owl			G5	S2
<i>Aythya collaris</i>	Ring-necked Duck			G5	SHB
<i>Bartramia longicauda</i>	Upland Sandpiper		SE	G5	S3B
<i>Botaurus lentiginosus</i>	American Bittern		SE	G5	S2B
<i>Buteo platypterus</i>	Broad-winged Hawk		SSC	G5	S3B
<i>Carduelis pinus</i>	Pine Siskin			G5	S3N
<i>Chordeiles minor</i>	Common Nighthawk		SSC	G5	S4B
<i>Cistothorus platensis</i>	Sedge Wren		SE	G5	S3B
<i>Falco peregrinus</i>	Peregrine Falcon		SSC	G4	S2B
<i>Grus canadensis</i>	Sandhill Crane		SSC	G5	S2B,S1N
<i>Haliaeetus leucocephalus</i>	Bald Eagle		SSC	G5	S2
<i>Ixobrychus exilis</i>	Least Bittern		SE	G4G5	S3B
<i>Lanius ludovicianus</i>	Loggerhead Shrike		SE	G4	S3B
<i>Nycticorax nycticorax</i>	Black-crowned Night-heron		SE	G5	S1B
<i>Rallus elegans</i>	King Rail		SE	G4	S1B
<i>Setophaga cerulea</i>	Cerulean Warbler		SE	G4	S3B
<i>Sturnella neglecta</i>	Western Meadowlark		SSC	G5	S2B
<i>Tyto alba</i>	Barn Owl		SE	G5	S2
Mammal					
<i>Corynorhinus rafinesquii</i>	Rafinesque's Big-eared Bat		SSC	G3G4	SH
<i>Geomys bursarius</i>	Plains Pocket Gopher		SSC	G5	S2

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<i>Lasiurus borealis</i>	Eastern Red Bat		SSC	G3G4	S4
<i>Mustela nivalis</i>	Least Weasel		SSC	G5	S2?
<i>Myotis septentrionalis</i>	Northern Long Eared Bat	LT	SE	G1G2	S2S3
<i>Myotis sodalis</i>	Indiana Bat	LE	SE	G2	S1
<i>Nycticeius humeralis</i>	Evening Bat		SE	G5	S1
<i>Reithrodontomys megalotis</i>	Western Harvest Mouse			G5	S2
<i>Spermophilus franklinii</i>	Franklin's Ground Squirrel		SE	G5	S2
<i>Taxidea taxus</i>	American Badger		SSC	G5	S2
Vascular Plant					
<i>Androsace occidentalis</i>	western rockjasmine		ST	G5	S2
<i>Astragalus tennesseensis</i>	Tennessee milk-vetch		SRE	G3	SX
<i>Bacopa rotundifolia</i>	roundleaf water-hyssop		ST	G5	S2
<i>Besseyia bullii</i>	kitten tails		SE	G3	S1
<i>Botrychium matricariifolium</i>	chamomile grape-fern		ST	G5	S3
<i>Botrychium simplex</i>	least grape-fern		SE	G5	S1
<i>Camassia angusta</i>	wild hyacinth		SE	G5?Q	S1
<i>Carex flava</i>	yellow sedge		ST	G5	S2
<i>Carex gravida</i>	heavy sedge		SE	G5	S1
<i>Chelone obliqua var. speciosa</i>	rose turtlehead		WL	G4T3	S3
<i>Circaea alpina</i>	small enchanter's nightshade		SX	G5	SX
<i>Cirsium hillii</i>	Hill's thistle		SE	G3	S1
<i>Crataegus coccinea var. coccinea</i>	scarlet hawthorn		ST	G5	S2
<i>Cypripedium candidum</i>	small white lady's-slipper		ST	G4	S3
<i>Dactylorhiza viridis</i>	long-bract green orchid		SE	G5	S1
<i>Eriophorum angustifolium</i>	narrow-leaved cotton-grass		ST	G5	S3
<i>Erysimum capitatum</i>	prairie-rocket wallflower		SE	G5	S1
<i>Gentiana alba</i>	yellow gentian		ST	G4	S3
<i>Heterotheca camporum var. camporum</i>	hairy golden-aster		ST	G5TNR	S3
<i>Linum sulcatum</i>	grooved yellow flax		ST	G5	S3
<i>Lithospermum incisum</i>	narrow-leaved puccoon		SE	G5	S1
<i>Lithospermum parviflorum</i>	shaggy false-gromwell		SE	G4G5T4	S1
<i>Melampyrum lineare</i>	American cow-wheat		SE	G5	S1
<i>Minuartia patula</i>	Pitcher's stitchwort		SE	G4	S1
<i>Muhlenbergia cuspidata</i>	plains muhly		SE	G5	S1
<i>Napaea dioica</i>	glade mallow		ST	G4	S2
<i>Orobanche riparia</i>	bottomland broomrape		SE	G4?	S1
<i>Patis racemosa</i>	black-fruit mountain-ricegrass		ST	G5	S3
<i>Pediomelum tenuiflorum</i>	few-flowered scurf-pea		SX	G5	SX
<i>Plantago cordata</i>	heart-leaved plantain		SE	G4	S1
<i>Platanthera psycodes</i>	small purple-fringe orchid		ST	G5	S3

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<i>Sanguisorba canadensis</i>	Canada burnet		SE	G5	S1
<i>Selaginella apoda</i>	meadow spike-moss		WL	G5	S1
<i>Silene regia</i>	royal catchfly		SE	G3	S1
<i>Spiranthes lucida</i>	shining ladies'-tresses		ST	G4	S3
<i>Stenaria nigricans</i>	narrowleaf summer bluets		ST	G5	S3
<i>Symphotrichum oblongifolium</i>	aromatic aster		ST	G5	S3
<i>Trichostema dichotomum</i>	forked bluecurl		WL	G5	S3
<i>Viola pedatifida</i>	prairie violet		ST	G5	S2
High Quality Natural Community					
<i>Barrens - gravel</i>	Gravel Slope Barrens		SG	G3	S1
<i>Barrens - sand</i>	Sand Barrens		SG	G3	S2
<i>Forest - upland dry-mesic Central Till Plain</i>	Central Till Plain Dry-mesic Upland Forest		SG	GNR	S2
<i>Forest - upland mesic Central Till Plain</i>	Central Till Plain Mesic Upland Forest		SG	GNR	S3
<i>Lake - lake</i>	Lake		SG	GNR	S2
<i>Prairie - dry-mesic</i>	Dry-mesic Prairie		SG	G3	S2
<i>Wetland - fen</i>	Fen		SG	G3	S3
<i>Wetland - marsh</i>	Marsh		SG	GU	S4
<i>Wetland - seep circumneutral</i>	Circumneutral Seep		SG	GU	S1
Other Significant Feature					
<i>Geomorphic - Nonglacial Erosional Feature - Water Fall and Cascade</i>	Water Fall and Cascade			GNR	SNR

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