

CATEGORICAL EXCLUSION LEVEL 1 FORM

GENERAL PROJECT INFORMATION

Road No./County:	State Route (SR) 3 / Henry County
Designation Number(s):	1593230 (road work) and 2003091 (formerly 1902175) (installation of sidewalks and raised median markers)
Project Description/Termini:	Road rehabilitation project / along SR 3 (known locally as South Memorial Drive), from 3.14 miles north of I-70 (Sherry Lynn Drive) north to SR 38 (known locally as Broad Street).

CE Level 1 documentation for exempted projects

Additional Information to CE Level 1

Approval:

Ronald E. Bales 01/25/2024

INDOT DE/ESD Signature and Date

Release for Public Involvement:

REB

3-9-2022

INDOT DE/ESD Initials and Date

Certification of Public involvement:

JoAnn Wooldridge 12/11/2023

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer:

Ronald E. Bales 01/25/2024

For: Drew Passmore,
INDOT ESD

Signature and Date

CE Preparer:

Jaime Byerly / RQAW

Name and Organization

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

GENERAL PROJECT INFORMATION, DESCRIPTION, AND DESIGN INFORMATION	
Purpose and Need:	<p>Need</p> <p>The primary need for the project is due to the condition of the existing pavement along SR 3 between 3.14 miles north of I-70 and SR 38. According to the Mini Scope, completed by the Indiana Department of Transportation (INDOT) and dated January 31, 2014, the pavement is at the end of its service life and requires constant maintenance. There are numerous patches, failed joints, transverse and longitudinal cracks, spalls, and corner breaks. The International Roughness Index (IRI), used to evaluate pavement smoothness, is 134 inches / mile (Appendix I, pages I-30 and I-31). Per the Federal Highway Administration (FHWA) Pavement Condition Criteria 4, the IRI for “good” pavement is less than or equal to 95. Since 2014, the pavement has continued to deteriorate. According to the Engineering Assessment Report, completed by First Group Engineering and dated November 8, 2019, the two center lanes of the existing pavement within the project area have been overlaid, milled, resurfaced, and widened with different materials numerous times. The outside two concrete lanes in each direction exhibit numerous patches, failed joints, traverse and longitudinal cracks, spalls, and corner brakes (Appendix I, pages I-2 to I-18).</p> <p>Another primary need is due to the number of injury crashes within the project area. According to the Engineering Assessment Report completed by the INDOT and dated July 17, 2019, at least 101 crashes have occurred within the project area between 2016 and 2018. Of these, 17 were fatal or incapacitating crashes. The report noted there is a significant pattern of right-angle (turning or crossing) crashes, specifically at commercial driveways within the northern project area. The report documents an Index of Crash Frequency (ICF) value of 1.45 which indicates a higher number of expected crashes for this type of roadway. The report also documents an Index of Crash Cost (ICC) value of 1.69 which indicates a higher severity of crashes for this type of roadway. Additionally, there was an above average number of wet weather crashes (27% actual versus 18% normal) and the above average number of wet crashes occurred during the daytime (75% actual versus 66% normal) (Appendix I, pages I-19 to I-29).</p> <p>A secondary need is due to the poor condition of the existing storm water drainage system within the project area. The existing storm water drainage system gets overwhelmed during heavy rain events which results in poor surface water drainage with ponding. Two existing structures are undersized and in poor condition which also contribute to poor surface water drainage and ponding. Both factors contribute to the deteriorated pavement condition.</p> <p>Another secondary need is due to the total lack of pedestrian facilities (e.g., sidewalks or trails) within the project area. There are numerous commercial and residential properties within the project area; however, there are no existing facilities to provide pedestrian connectivity between the commercial and residential properties. According to the Engineering Assessment, the project area was studied as a candidate for funding under the Federal Highway Safety Improvement Program (HSIP). The FHWA provides funding for eligible roadway safety improvements through the HSIP.</p>

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

	<p>Purpose The primary purpose of the project is to improve the pavement condition to an IRI of less than or equal to 95 and reduce right-angle vehicle crashes (specifically at commercial driveways within the northern project area), thereby reducing the number of injury crashes. The secondary purpose is to improve the condition of roadway drainage and provide pedestrian facilities along SR 3 between 3.14 miles north of I-70 and SR 38.</p>															
<p>Project Description (Preferred Alternative):</p>	<p>Location The project is located on SR 3 (known locally as South Memorial Drive), from 3.14 miles north of I-70 (Sherry Lynn Drive) north to SR 38 (known locally as Broad Street) in Henry County, Indiana. The project is within Henry Township, Sections 15, 21, 22, 27, and 28 of Township 17 North, and Range 10 East of the New Castle West U.S. Geological Survey (USGS) Quadrangle (refer to maps in Appendix B, pages B-1 to B-8).</p> <p>Existing Conditions "Project area" is used throughout this document and refers to the areas within and adjacent to construction and right-of-way limits. Within the project area, SR 3 is classified as a Principal Arterial and consists of six lanes: five 12-foot-wide travel lanes (two southbound and three northbound) and a 16-foot-wide two-way left turn lane (TWLTL) median. The outside lanes are bordered with integral concrete curb throughout the project area. There are no shoulders or sidewalks present. There are seven signalized intersections: Riley Road, Trojan Lane, Parkview Drive, Commercial Entrance, Cherry Street, Indiana Avenue, and SR 38. There is a closed storm sewer system consisting of inlets, manholes, and storm sewers within the project area. The storm sewers have multiple outlets where storm water is conveyed to roadside ditches. The following structures are within the project area:</p> <table border="1" data-bbox="506 1142 1422 1751"> <thead> <tr> <th>Structure Number</th> <th>Existing Structure Dimensions</th> <th>Location</th> </tr> </thead> <tbody> <tr> <td>CV 003-033-112.35</td> <td>115-foot-long, 24-inch-diameter corrugated metal pipe (CMP)</td> <td>Approximately 380 feet north of North Pleasantview Drive and conveys roadside and storm water drainage under SR 3</td> </tr> <tr> <td>CV 003-033-112.57</td> <td>70-foot-long, 5-foot by 5-foot reinforced concrete box (RCB) culvert with 54-inch diameter CMP on both ends</td> <td>Approximately 310 feet south of Lynn View Drive and conveys (encapsulates) Cemetery Run under SR 3</td> </tr> <tr> <td>CV 003-033-113.96</td> <td>600-foot-long, 8-foot by 8-foot RCB culvert</td> <td>Approximately 0.3 mile south of Cherry Street and conveys (encapsulates) Mound Run under SR 3</td> </tr> <tr> <td>CV 003-033-113.21</td> <td>240-foot-long, 13-foot-diameter CMP arch</td> <td>Approximately 190 feet south of Spiceland Road and conveys (encapsulates) Elliott Run under SR 3</td> </tr> </tbody> </table>	Structure Number	Existing Structure Dimensions	Location	CV 003-033-112.35	115-foot-long, 24-inch-diameter corrugated metal pipe (CMP)	Approximately 380 feet north of North Pleasantview Drive and conveys roadside and storm water drainage under SR 3	CV 003-033-112.57	70-foot-long, 5-foot by 5-foot reinforced concrete box (RCB) culvert with 54-inch diameter CMP on both ends	Approximately 310 feet south of Lynn View Drive and conveys (encapsulates) Cemetery Run under SR 3	CV 003-033-113.96	600-foot-long, 8-foot by 8-foot RCB culvert	Approximately 0.3 mile south of Cherry Street and conveys (encapsulates) Mound Run under SR 3	CV 003-033-113.21	240-foot-long, 13-foot-diameter CMP arch	Approximately 190 feet south of Spiceland Road and conveys (encapsulates) Elliott Run under SR 3
Structure Number	Existing Structure Dimensions	Location														
CV 003-033-112.35	115-foot-long, 24-inch-diameter corrugated metal pipe (CMP)	Approximately 380 feet north of North Pleasantview Drive and conveys roadside and storm water drainage under SR 3														
CV 003-033-112.57	70-foot-long, 5-foot by 5-foot reinforced concrete box (RCB) culvert with 54-inch diameter CMP on both ends	Approximately 310 feet south of Lynn View Drive and conveys (encapsulates) Cemetery Run under SR 3														
CV 003-033-113.96	600-foot-long, 8-foot by 8-foot RCB culvert	Approximately 0.3 mile south of Cherry Street and conveys (encapsulates) Mound Run under SR 3														
CV 003-033-113.21	240-foot-long, 13-foot-diameter CMP arch	Approximately 190 feet south of Spiceland Road and conveys (encapsulates) Elliott Run under SR 3														

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

CLV 46722	108-foot-long, 30-inch-diameter CMP	Approximately 260 feet south of Indiana Avenue and conveys roadside and storm water drainage under SR 3
No assigned structure number	145-foot-long, 27-inch-diameter high density polyethylene (HDPE) pipe	Approximately 610 feet north of Riley Road and conveys roadside and storm water drainage under SR 3
No assigned structure number	72-foot-long, 15 inch CMP	Located underneath Pleasantview Drive and conveys roadside and stormwater drainage

The existing pavement is at the end of its service life and requires constant maintenance. The existing pavement has been overlaid, milled, resurfaced, and widened with different materials numerous times. The pavement exhibits numerous patches, failed joints, traverse and longitudinal cracks, spalls, and corner breaks. There have been several injury crashes within the project area, largely due to right-angle (turning or crossing) crashes at commercial driveways within the northern project area. The existing storm sewer gets overwhelmed during heavy rain events which results in poor surface water drainage with ponding. Structure Numbers 003-033-112.35 and 003-033-112.57 are undersized and in poor condition which also contribute to poor surface water drainage and ponding. There is a total lack of pedestrian facilities (e.g., sidewalks or trails) to provide pedestrian connectivity between the numerous commercial and residential properties within the project area. Adjacent land use consists of wooded, commercial, and residential properties (refer to photos in Appendix B, pages B-9 to B-22).

Preferred Alternative

The preferred alternative will include a full depth hot mix asphalt (HMA) pavement reconstruction with two 12-foot-wide travel lanes in each direction within the project area. The TWLTL median will be removed, and a raised asphalt / concrete median will be installed within the northern project area (SR 38 south to Trojan Lane). The elimination of the TWLTL median will result in traffic accessing properties via right-in and right-out only. The TWLTL median will be reconstructed and maintained within the southern project area (Trojan Lane south to Sherry Lynn Drive). A 2.58-foot-wide curb and gutter will be constructed along both sides of SR 3. Six-foot-wide sidewalks will be constructed adjacent to the back of curb on both sides of SR 3 from SR 38 south to Trojan Lane. Six-foot-wide sidewalks will be constructed adjacent to the back of curb on the east side of SR 3 from Trojan Lane south to Sherry Lynn Drive. Raised pavement markers will be installed at fire hydrant locations.

Existing traffic signal equipment will be reused as much as possible at the existing seven signalized intersections. Signal heads will need replaced / relocated to accommodate new lane configurations, as needed. Conduit, underground wiring, signal detection, handholes and detector housings will be replaced, as needed. Pedestrian signals and push buttons will be installed and connected to existing signals and pedestrian crosswalks will be placed on the roadway. Within the project area, the speed limit will be lowered from 45 to 40

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

	<p>miles per hour (mph). This will result in incidental construction north of SR 38 to install signs indicating the lowered speed limit.</p> <p>A new storm sewer trunkline will be installed under the roadway, along the entire length of the project area. New drainage inlets and / or castings will be installed along SR 3 and connect to the new storm sewer trunkline. The project will impact the following structures:</p> <table border="1" data-bbox="506 489 1396 770"> <thead> <tr> <th>Structure Number</th> <th>Preferred Alternative</th> </tr> </thead> <tbody> <tr> <td>CV 003-033-112.35</td> <td>Replace with a 115-foot-long, 36-inch-diameter CMP. Riprap will be installed at structure outlet.</td> </tr> <tr> <td>CV 003-033-112.57</td> <td>Replace with a 7-foot-diameter smooth circular pipe. Riprap will be installed at structure outlet.</td> </tr> <tr> <td>CLV 46722</td> <td>Line with a HDPE or cured-in-place-pipe (CIPP)</td> </tr> <tr> <td>39A (No assigned structure number)</td> <td>Replace with a 72-foot-long, 18-inch-diameter CMP that will be sumped 3-inches. Pipe end sections will be installed, and riprap will be placed at the structure outlet.</td> </tr> </tbody> </table> <p>Approximately 0.07 acre of permanent and 0.21 acre of temporary right-of-way will be required. The maintenance of traffic (MOT) will involve two phases to allow for continued vehicular traffic northbound and southbound on SR 3. Refer to the <i>Maintenance of Traffic (MOT) During Construction</i> section of this document for further details on the proposed MOT (refer to plan sheets in Appendix B, pages B-23 to B-138).</p> <p>The preferred alternative satisfies the primary purpose and need of the project by improving the condition of the pavement and reducing right-angle vehicle crashes (specifically at commercial driveways within the northern project area), thereby reducing the number of injury crashes. Furthermore, drainage issues will be improved, and pedestrian facilities will be provided, meeting the secondary purpose and need of the project.</p> <p><u>Logical Termini and Independent Utility</u></p> <p>The termini for the project are logical because the pavement located north of the project area does not exhibit the same pavement deficiencies. The pavement south of the project area demarcates the transition from an urban section with curbs and storm sewers to a rural section with asphalt and shoulders. The termini are also logical due to the significant pattern of right-angle crashes at commercial driveways along this stretch of SR 3. The project is an independent and complete project because it is not dependent on any other projects to meet its purpose and need.</p>	Structure Number	Preferred Alternative	CV 003-033-112.35	Replace with a 115-foot-long, 36-inch-diameter CMP. Riprap will be installed at structure outlet.	CV 003-033-112.57	Replace with a 7-foot-diameter smooth circular pipe. Riprap will be installed at structure outlet.	CLV 46722	Line with a HDPE or cured-in-place-pipe (CIPP)	39A (No assigned structure number)	Replace with a 72-foot-long, 18-inch-diameter CMP that will be sumped 3-inches. Pipe end sections will be installed, and riprap will be placed at the structure outlet.
Structure Number	Preferred Alternative										
CV 003-033-112.35	Replace with a 115-foot-long, 36-inch-diameter CMP. Riprap will be installed at structure outlet.										
CV 003-033-112.57	Replace with a 7-foot-diameter smooth circular pipe. Riprap will be installed at structure outlet.										
CLV 46722	Line with a HDPE or cured-in-place-pipe (CIPP)										
39A (No assigned structure number)	Replace with a 72-foot-long, 18-inch-diameter CMP that will be sumped 3-inches. Pipe end sections will be installed, and riprap will be placed at the structure outlet.										
<p>Other Alternatives Considered:</p>	<p><u>No-Build Alternative</u></p> <p>This alternative would not involve any improvements to the existing roadway. This alternative would not require additional permanent or temporary right-of-way, not involve any cost, and would not result in any impacts to the surrounding environment. This alternative would not address the primary purpose of improving the condition of the existing pavement and reducing right-angle vehicle crashes (specifically at commercial driveways within the northern project area). This alternative would not address the secondary purpose of improving</p>										

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

	<p>the condition of roadway drainage or providing pedestrian facilities within the project area. As such, this alternative was dismissed from further consideration because it would not address the purpose and need of the project.</p> <p><u>Reconstruct two outside travel lanes in each direction and patch and overlay center lanes</u></p> <p>This alternative would reconstruct the two outside travel lanes with new full depth pavement and curbs. The TWLTL median and inside northbound travel lane would require significant full depth concrete patching, as well as asphalt milling and HMA overlay. In addition, the pavement design life of the four outside lanes would be significantly greater than the life of the two center lanes, which have underlying concrete pavement over 73 years old. Reflective cracking in the center two lanes would likely return before the end of the pavement design life of the two outside travel lanes. This alternative would meet the primary purpose of improving the condition of the existing pavement condition. However, because it would not involve the construction of the raised concrete median in the northern project area, this alternative would not satisfy the other primary purpose of reducing right-angle crashes, therefore, this alternative would not reduce the number of injury crashes. Also, it would not meet the secondary purpose of improving the condition of the roadway drainage or providing pedestrian facilities within the project area. Therefore, this alternative was dismissed from further consideration.</p> <p><u>Reconstruct two outside travel lanes in each direction and raised center median with no new storm sewers or pedestrian facilities</u></p> <p>This alternative would reconstruct the two outside travel lanes with new full depth pavement and curbs. Most construction activities would occur within the existing roadway footprint. This alternative would avoid impacts to wetlands. This alternative would also meet the primary purpose of the project by improving the condition of the existing pavement and reducing right-angle crashes (specifically at commercial driveways within the northern project area), therefore, reducing the number of injury crashes. This alternative is not recommended because it would not meet the secondary purpose of improving the condition of the roadway drainage or providing pedestrian facilities within the project area. Therefore, this alternative was dismissed from further consideration.</p>		
Funding Source(s):	<input checked="" type="checkbox"/> Federal	<input checked="" type="checkbox"/> State	<input type="checkbox"/> Local <input type="checkbox"/> Other
Project Sponsor:	INDOT, Greenfield District		
Estimated Cost:	\$36,150,000	Project Length:	2.78 miles
Public Involvement:	No:		Yes: <input checked="" type="checkbox"/>
<p>Notice of Entry letters were mailed to potentially affected property owners near the project area on July 14, 2020, notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area (Appendix G, pages G-1 and G-2).</p> <p><u>Public Hearing</u></p> <p>The project met the minimum requirements described in the current <i>INDOT Project Development Public Involvement Procedures Manual</i> which requires the project sponsor to offer the public an opportunity to</p>			

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

submit comments and / or request a public hearing. The INDOT and FHWA have determined that projects installing center medians that restrict turning movements and change how adjacent businesses / residences are accessed require public involvement.

A legal notice of public hearing was advertised in the *Courier Times*, a widely circulated newspaper in Henry County, on September 26, 2023, and October 03, 2023. A copy of the legal notice was also sent to all property owners on September 25, 2023, and all early coordination letter recipients on October 10, 2023. Please refer to Appendix G, pages G-7 to G-12 for a mailing list of all legal notice recipients, which included all property owners and early coordination recipients. The legal notice notified the public of the hearing and offered the public an opportunity to comment on the environmental document and preliminary design plans. A copy of the legal notice can be found in Appendix G, pages G-3 to G-4, and the publisher's affidavit can be found in Appendix G, pages G-5 to G-6. The public hearing was held on Wednesday, October 11, 2023 (a minimum 15 business days after legal notice publishing), at the Bundy Auditorium located at 601 Parkview Drive, New Castle, IN 47362 on the campus of the New Castle Middle/High School. Project information packets were handed out to the public at the sign-in desk and a pre-recorded PowerPoint presentation was presented during the hearing. A copy of the information packets distributed can be found in Appendix G, pages G-23 to G-27 and a copy of the PowerPoint presentation can be found in Appendix G, pages G-28 to G-34. Project representatives from INDOT, First Group Engineering, and RQAW were in attendance.

The public hearing began at 6:00pm and allowed the public an opportunity to hear information on the project and comment on the environmental document and preliminary design plans. Fifty-eight (58) people from the public attended the public hearing, seven (7) of which gave formal public comments after the presentation concluded. Please refer to Appendix G, pages G-13 to G-22 for public sign-in sheets and Appendix G, page G-35 for the public speaker sign-up sheet. Additionally, please refer to Appendix G, pages G-35 to G-43 for a transcript of all verbal comments received during the hearing. Comments received during the hearing pertained primarily to the design of the project, specifically about potentially implementing the use of traffic signal preemption devices to improve safety and allow emergency services vehicles the right-of-way and primarily about the concrete median that would restrict turning movements into access drives where businesses are located. Additionally, twenty-three (23) additional comments were received during the comment period and consisted of written letters, phone calls, INDOT4U inquiries, comments forms, and emails (Appendix G, pages G-44 to G-77). These comments generally pertained to the design of the project, specifically about the concrete median restricting turning movements into access drive for businesses and pedestrian safety. Please note that the comment period expired on October 26, 2023, which was 15 days after the public hearing was held. Please refer to Appendix G, pages G-78 to G-84 for all comments received and responses to those comments.

Once the NEPA process is completed, a notice of project advancement will be sent to all project stakeholders, local/public officials, and all adjacent property owners to inform all community members that the final environmental document is available for viewing, which notifies the public and provides them an opportunity to learn of the project decision.

Right-of-Way:	No:	Yes: ✓
----------------------	-----	--------

The existing right-of-way width varies from approximately 50 to 90 feet from the roadway centerline. The project requires approximately 0.07 acre of permanent right-of-way and 0.21 acre of temporary right-of-way (Appendix B, pages B-81 to B-100). Permanent right-of-way will be needed near Cemetery Run to replace the existing structure and install riprap; this land consists of residential (maintained lawn) and wooded. Temporary right-of-way will be needed for traffic light signal improvements, grading, and driveway reconstruction; this land consists of wooded, commercial, and residential properties. Generally, the proposed right-of-way width will still vary from approximately 50 to 90 feet from the roadway centerline.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Maintenance of Traffic (MOT) During Construction: No: Yes:

The MOT will involve two phases to allow for continued vehicular traffic northbound and southbound on SR 3. Phase 1 will divert traffic onto the three existing southbound travel lanes, while the northbound side of SR 3 is constructed. Phase 2 will divert traffic onto the three new northbound / TWLTL median travel lanes, while the southbound side of SR 3 is constructed. Access to all properties will be maintained during construction (Appendix B, pages B-26 to B-80).

The lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences will cease upon project completion.

Bridge(s) and/or Small Structure(s) (include structure number(s)): No: Yes:

New drainage inlets and / or castings will be installed and connect to the new storm sewer trunkline. The following structures are within the project area:

Structure Number	Historic	Existing Structure Dimensions	Preferred Alternative
CV 003-033-112.35	No	115-foot-long, 24-inch-diameter CMP	Replace with a 115-foot-long, 36-inch-diameter CMP. Riprap will be installed at structure outlet.
CV 003-033-112.57	No	70-foot-long, 5-foot by 5-foot RCB culvert with 54-inch diameter CMP on both ends	Replace with a 7-foot-diameter smooth circular pipe. Riprap will be installed at structure outlet.
CV 003-033-113.96	No	600-foot-long, 8-foot by 8-foot RCB culvert	Will not be impacted
CV 003-033-113.21	No	240-foot-long, 13-foot-diameter CMP arch	Will not be impacted
CLV 46722	No	108-foot-long, 30-inch-diameter CMP	Line with a HDPE or CIPP
No assigned structure number	No	145-foot-long, 27-inch-diameter CMP	Will not be impacted
No assigned structure number	No	72-foot-long, 15-inch-diameter CMP	Replace with 72-foot-long, 18-inch-diameter CMP that will be sumped 3-inches. Pipe end sections will be installed, and riprap will be placed at the structure outlet.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

IDENTIFICATION AND EVALUATION OF IMPACTS

Early Coordination:

Early coordination letters were sent on March 2, 2021 (Appendix C, pages C-1 to C-4). Refer to the table below for which agencies responded to the early coordination letter.

Agency	Date Sent	Date Response Received	Appendix Pages
FHWA	March 2, 2021	N/A	N/A
INDOT Greenfield District	March 2, 2021	October 26, 2021	C-45 and C-46
Natural Resources Conservation Service (NRCS)	March 2, 2021	April 5, 2021	C-22
Indiana Department of Natural Resources (IDNR) Division of Fish and Wildlife	March 2, 2021	April 1, 2021	C-19 to C-21
U.S. Department of Housing and Urban Development (USHUD)	March 2, 2021	No response received	N/A
U.S. Army Corps of Engineers (USACE) Louisville District	March 2, 2021	No response received	N/A
Local Floodplain Administrator	March 2, 2021	No response received	N/A
National Parks Service, Midwest Regional Office	March 2, 2021	No response received	N/A
New Castle Utilities	March 2, 2021	No response received	N/A
INDOT Office of Aviation	March 2, 2021	March 3, 2021	C-17
Indiana Department of Environmental Management (IDEM)	March 2, 2021 (electronic upload)	March 2, 2021	C-5 to C-11
Indiana Geological and Water Survey (IGWS)	March 2, 2021 (electronic upload)	March 2, 2021	C-14 to C-16
IDEM Ground Water Section	January 12, 2021 (electronic query)	February 5, 2021	C-12 and C-13
Henry County Council	March 2, 2021	No response received	N/A
Henry County Board of Commissioners	March 2, 2021	No response received	N/A
Henry County Surveyor's Office	March 2, 2021	No response received	N/A
Henry County Highway Department	March 2, 2021	No response received	N/A
New Castle Municipal Separate Storm Sewer System (MS4)	March 2, 2021	No response received	N/A
First Assembly Church of God	March 2, 2021	No response received	N/A
Kingdom Hall of Jehovah's Witness	March 2, 2021	No response received	N/A
South Memorial Drive Church of God	March 2, 2021	No response received	N/A
New Castle Parks and Recreation Department	March 2, 2021	No response received	N/A
New Castle Mayor	March 2, 2021	No response received	N/A
New Castle Street Commissioner	March 2, 2021	No response received	N/A
New Castle Public Works	March 2, 2021	March 2, 2021	C-18

Note: Early coordination with the United States Fish and Wildlife Service (USFWS) was not conducted because the project qualifies under the 2013 Interim Policy.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

After early coordination was sent, it was determined that temporary right-of-way will be needed at five areas (commercial entrances) for traffic light signal improvements. Because there are no additional resources not previously described in the early coordination letter located within the proposed temporary right-of-way areas, re-coordination with resource agencies was not needed (Appendix C, pages C-52 and C-53).

All applicable recommendations are included in the *Environmental Commitments* section of this Categorical Exclusion (CE) document.

Streams, Rivers, and Other Jurisdictional Features Impacted:	No:	Yes: <input checked="" type="checkbox"/>
---	-----	--

Based on a desktop review, aerial map of the project area, and the Red Flag Investigation (RFI) report (Appendix E, pages E-17 to E-20), there are 18 streams, rivers, watercourses, or other jurisdictional features mapped within the 0.50-mile search radius. There are three streams, river, or other jurisdictional features mapped within or adjacent to the project area. Three streams were confirmed by the site visit on October 12, 2020 by RQAW.

A *Waters of the U.S. Determination Report* was approved by the INDOT Ecology and Waterway Permitting Office on August 24, 2021. Please refer to Appendix F, pages F-1 to F-25 for the *Waters of the U.S. Determination Report*. It was determined that three likely jurisdictional waters are present within or adjacent to the project area. The USACE makes all final determinations regarding jurisdiction.

Stream Name	Stream Classification	Total Size in Project Area (linear feet)	Impacts (linear feet / acre) below OHWM	Comments (i.e., location, flow direction, likely Waters of the U.S., appendix reference)
Cemetery Run	Intermittent	400	195 linear feet (0.06 acre) to replace the existing structure (CV 003-033-112.57) and install riprap	Located south of the SR 3 / Lynn View Lane Intersection, flows west/southwest under SR 3, and is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, pages B-7 and B-8 and Appendix F, page F-3).
Elliott Run	Intermittent	485	0	Located south of the SR 3 / Spiceland Parkway Intersection, flows west under SR 3, and is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, pages B-6 and B-7 and Appendix F, page F-3).
Mound Run	Intermittent	665	0	Located south of the SR 3 / Cherry Street Intersection, flows west under SR 3, and is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, page B-5 and Appendix F, page F-4).
Total Permanent Stream Impacts (linear feet / acre)			195 (0.06)	

These streams are not listed as a Federal, Wild and Scenic River or on the National Rivers Inventory and are not listed as a State Natural, Scenic and Recreational River, navigable waterways, or as an Outstanding River for Indiana.

Fourteen roadside ditches (RSDs) were observed throughout the project area (Appendix B, pages B-4 to B-8). All roadside ditches lacked an OHWM and wetland characteristics; therefore, they considered to be non-jurisdictional features.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

The project will permanently impact approximately 195 linear feet (0.06 acre) of streams below the OHWM. Cemetery Run is located under and adjacent to the existing roadway; therefore, there are no practical alternatives which avoid impacts to the jurisdictional stream while meeting the purpose and need of the project. Because impacts will not exceed the 300 linear feet mitigation threshold, stream mitigation will not likely be required. Waterway permits will be needed due to stream impacts. Refer to the *Permits* section of this CE document for more details.

In their automated early coordination response, IDEM recommended limiting stream disturbance and coordinating with appropriate permitting agencies (Appendix C, pages C-5 to C-11). In their early coordination response, the IDNR Division of Fish and Wildlife recommended implementing erosion and sediment control measures and stream bank stabilization measures, minimizing in-channel disturbance, time restrictions for working within the waterway, proposed stream crossing design, and proper use of / placement of riprap (Appendix C, pages C-19 to C-21). The project qualifies under the 2013 Interim Policy; as such, the USFWS recommendations generally include avoidance of stream habitat impacts via erosion and sediment control measures and time restrictions for working within the stream channel.

Open Water Feature(s):	No: ✓	Yes:
-------------------------------	-------	------

Based on a desktop review, aerial map of the project area, and the RFI report (Appendix E, pages E-17 to E-20), there are three open water features mapped within the 0.50-mile search radius. There are no open water features mapped within or adjacent to the project area. No open water features were confirmed by the site visit on October 12, 2020 by RQAW.

A *Waters of the U.S. Determination Report* was approved by the INDOT Ecology and Waterway Permitting Office on August 24, 2021. Please refer to Appendix F, pages F-1 to F-25 for the *Waters of the U.S. Determination Report*. It was determined that no open water features are present within or adjacent to the project area. The USACE makes all final determinations regarding jurisdiction.

No agency recommendations were received that pertain to open water features.

Wetlands:	No:	Yes: ✓
------------------	-----	--------

Based on a desktop review, aerial map of the project area, and the RFI report (Appendix E, pages E-17 to E-20), there are 34 wetlands mapped within the 0.50-mile search radius. There are four wetlands mapped within or adjacent to the project area. Seven wetlands were confirmed by the site visit on October 12, 2020 by RQAW.

A *Waters of the U.S. Determination Report* was approved by the INDOT Ecology and Waterway Permitting Office on August 24, 2021. Please refer to Appendix F, pages F-1 to F-25 for the *Waters of the U.S. Determination Report*. It was determined that seven wetlands (Wetlands A through G) are present within or adjacent to the project area. Please note that guidance on jurisdiction has been revised since approval of the *Waters of the U.S. Determination Report*. Wetlands A to D and G, previously considered non-jurisdictional, are now likely to be considered jurisdictional. The USACE makes all final determinations regarding jurisdiction.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

Wetland Name	Wetland Classification	Total Size in Project Area (acres)	Impacts (acre)	Comments (i.e., location, likely Waters of the U.S., appendix reference)
Wetland A	Palustrine emergent (PEM)	0.04	0.006 to install new drainage inlet and place riprap at outlet of structure 39A	On the east side of SR 3 within a RSD north of the SR 3 / Pleasant View Drive Intersection. It is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, page B-8 and Appendix F, pages F-4 and F-5).
Wetland B	PEM	0.01	0 (outside construction limits)	On the east side of SR 3 within a RSD in the southeast quadrant of the SR 3/Sandy Gale Avenue Intersection. It is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, page B-8 and Appendix F, page F-5).
Wetland C	PEM	0.03	0 (outside construction limits)	On the west side of SR 3 at the SR 3 / Sandy Gale Avenue Intersection. It is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, page B-8 and Appendix F, page F-5).
Wetland D	PEM	0.01	0 (outside construction limits)	On the west side of SR 3 within a RSD north of the SR 3 / Riley Road Intersection. It is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, page B-7 and Appendix F, page F-6).
Wetland E	PEM	0.06	0 (outside construction limits)	On the west side of SR 3 southwest of the SR 3 / Spiceland Pike Intersection. It is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, pages B-6 and B-7 and Appendix F, page F-6).
Wetland F	PEM	0.08	0 (outside construction limits)	On the west side of SR 3 southwest of the SR 3 / Spiceland Pike Intersection. It is likely to be considered jurisdictional (i.e., a <i>Waters of the United States</i>) (Appendix B, page B-6 and Appendix F, pages F-6 and F-7).
Wetland G	PEM	0.03	0.003 to install new drainage inlet	On the west side of SR 3 within a RSD in the northwest quadrant of the SR 3 / Indiana Avenue Intersection. It is likely to be considered jurisdictional (i.e., a <i>Waters of the States</i>) (Appendix B, page B-4 and Appendix F, page F-7).
Total Permanent Wetland Impacts (acre)			0.009	

Cumulatively, the project will permanently impact approximately 0.009 acre of wetlands. Avoidance of the wetlands would not be practicable because the wetlands are within the limits where work activities are required for installation of new drainage inlets and placement of riprap. If the wetlands are not impacted, the project would not be able to be properly constructed. Because impacts will not exceed the 0.10-acre mitigation threshold, wetland mitigation will not likely be required. The locations of Wetlands A to G will be shown on the final design plan sheets and *Do Not Disturb Outside Construction Limits* will be added to the plan sheets. This is included as a firm commitment in the *Environmental Commitments* section of this CE

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

document. Waterway permits will be needed due to wetland impacts. Refer to the *Permits* section of this CE document for more details.

In their automated early coordination response, IDEM recommended conducting a site visit to look for the presence of wetlands, avoiding / limiting impacts to wetlands, and coordinating with the appropriate permitting agencies (Appendix C, pages C-5 to C-11). In their early coordination response, the IDNR Division of Fish and Wildlife recommended mitigating impacts to wetland habitat at the appropriate ratio according to the 1991 INDOT / IDNR / USFWS Memorandum of Understanding (MOU) and not excavating or placing fill in a riparian wetland (Appendix C, pages C-19 to C-21). The project qualifies under the 2013 Interim Policy; however, there are no USFWS recommendations that pertain to wetlands.

Terrestrial Habitat:	No:	Yes: ✓
-----------------------------	-----	--------

Based on a desktop review, a site visit on October 12, 2020 by RQAW, and aerial map of the project area (Appendix B, pages B-3 to B-8), terrestrial habitat (e.g. maintained lawn and trees) is present within and adjacent to the project area. Dominant tree species include sugar maple (*Acer saccharum*), white mulberry (*Morus alba*), and gray dogwood (*Cornus racemosa*). Dominant herbaceous species include tall fescue (*Schedonorus arundinaceus*), Kentucky bluegrass (*Poa pratensis*), yellow foxtail (*Setaria pumila*), and Queen Anne's lace (*Daucus carota*).

Up to 1 acre of trees will be cleared during the inactive bat season (October 1 through March 31) to install sidewalks and replace / line existing structures. Of this, approximately 0.016 acre will be cleared around Cemetery Run to replace the existing structure and install riprap. Because Cemetery Run has an upstream drainage area less than 1 square mile, a Construction in a Floodway Permit is not anticipated to be required at this location, and mitigation is not anticipated to be required. Avoidance of the trees would not be practicable because the trees are within the limits where work activities are required for the roadway and underground storm sewer trunkline. If the trees are not removed, the project would not be able to be properly constructed. All disturbed areas will be re-seeded post construction.

In their automated early coordination response, IDEM recommended limiting impacts to vegetation, post-construction re-vegetation measures, and coordinating with the appropriate permitting agencies (Appendix C, pages C-5 to C-11). In their early coordination response, the IDNR Division of Fish and Wildlife recommended revegetating disturbed areas, clearing restrictions, and planting trees for trees that are removed (Appendix C, pages C-19 to C-21). The project qualifies under the 2013 Interim Policy; as such, the USFWS recommendations generally include avoidance of terrestrial habitat impacts via restricting vegetation clearing.

Protected Species:	No:	Yes: ✓
---------------------------	-----	--------

Based on a desktop review of the IDNR Endangered, Threatened and Rare (ETR) Species website ([DNR: Nature Preserves: Endangered Threatened & Rare Species \(in.gov\)](http://DNR.NaturePreserves.EndangeredThreatened&RareSpecies.in.gov)) on July 20, 2021 by RQAW, the Henry County ETR Species List was checked. In their early coordination response, the IDNR Division of Fish and Wildlife stated the Natural Heritage Program's database was checked and, to date, no plant or animal species listed as state federally threatened, endangered, or rare have been reported near the project area (Appendix C, pages C-19 to C-21).

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal and an official species list was generated (Appendix C, pages C-25 to C-37). The project is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and Northern long-eared bat (*Myotis septentrionalis*). Other species were generated in the IPaC species list along with the Indiana bat and northern long-eared bat.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

The official species list generated from IPaC indicated three other species present within the project area. These species include the monarch butterfly (*Danaus plexippus*) as a candidate species for listing, the tricolored bat (*Perimyotis subflavus*) as a proposed endangered species, and the whooping crane (*Grus americana*) as an experimental population. As candidate, proposed, and experimental, non-essential species, the monarch butterfly, tricolored bat, and whooping crane are not given any statutory protection under the Endangered Species Act. Therefore, no further coordination is needed with the USFWS regarding these species.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and Northern long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and the USFWS. A structure inspection occurred on October 12, 2020 by RQAW and bats, or evidence of bats, were not observed (Appendix C, page C-24). An effect determination key was completed on March 17, 2021 and based on the responses provided, the project was found to *May Affect, Not Likely to Adversely Affect* the Indiana bat and Northern long-eared bat (Appendix C, pages C-38 to C-51). INDOT Greenfield District reviewed and concurred with the effect finding and requested USFWS's review of the finding on March 17, 2021. No response was received from the USFWS within the 14-day review period; therefore, it was concluded the USFWS concurs with the finding. Avoidance and Minimization Measures (AMMs) are included as firm commitments in the *Environmental Commitments* section of this CE document. AMMs generally pertain to tree cutting and lighting restrictions.

After the effect determination key was completed and concurred, it was determined that temporary right-of-way will be needed at five areas (commercial entrances) for traffic light signal improvements. Because the effect finding would not have changed as a result of these revisions, the effect determination key was not updated (Appendix C, pages C-52 and C-53). Additionally, please note that the updated species list generated on January 24, 2024, included two additional species: the tricolored bat (*Perimyotis subflavus*) and the whooping crane (*Grus americana*). As mentioned above, these species do not require additional coordination as they are not given any statutory protection under the Endangered Species Act. Based on this information and the scope of the project remaining relatively unaltered, the *May Affect, Not Likely to Adversely Affect* finding made on March 17, 2021, remains valid.

This precludes the need for further consultation on the project under Section 7 of the Endangered Species Act of 1973, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, the USFWS will be contacted for consultation.

Geological and Mineral Resources:	No: ✓	Yes:
--	-------	------

Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the July 15, 2021 Karst Protection Procedure. According to the topographic map of the project area (Appendix B, page B-2) and the RFI report (Appendix E, pages E-17 to E-20), there are no karst features identified within or adjacent to the project area.

In their early coordination response, the IGWS did not indicate that karst features exist in the project area (Appendix C, pages C-14 to C-16). The IGWS stated the project is located within a floodway, moderate liquefaction potential, high potential for bedrock resources and sand and gravel resources, and petroleum wells. The response from the IGS was communicated to the project designer on March 3, 2021. Impacts are not expected.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

Drinking Water Resources:	No:	Yes: ✓
<p><u>Sole Source Aquifer</u> The project is in Henry County which is not located within the Saint Joseph Sole Source Aquifer, the only legally designated sole source aquifer in Indiana. Therefore, the FHWA / Environmental Protection Agency (EPA) Sole Source Aquifer MOU is not applicable, and a detailed groundwater assessment is not needed. Impacts are not expected.</p> <p><u>Wellhead Protection Area and Source Water Area</u> The IDEM Wellhead Proximity Determinator website (IDEM: Water Quality In Indiana: Source Water Proximity Determination Tool) was accessed on January 12, 2021 by RQAW. The project is located within a Wellhead Protection Area. In their early coordination response, the IDEM Ground Water Section stated they do not have any special requirements but requested coordination with the New Castle Utilities' Wellhead Protection Area (Appendix C, pages C-12 and C-13). An early coordination letter was sent to the New Castle Utilities on March 2, 2021. The New Castle Utilities did not respond to the early coordination letter. Utility coordination is ongoing. Impacts are not expected.</p> <p><u>Water Wells</u> The IDNR Water Well Record Database website (DNR: Water: Water Well Record Database (in.gov)) was accessed on March 15, 2021 by RQAW. Ten unconsolidated water wells are located near the project. According to the project designer, impacts to residential water wells may occur. Should it be determined during the right-of-way phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells.</p> <p><u>Urban Area Boundary</u> Based on a desktop review of the INDOT Municipal Separate Storm Sewer System (MS4) website (IDEM: Water Quality In Indiana: MS4s Boundaries Map for Indiana) on March 2, 2021 by RQAW, the project is located within an Urbanized Area Boundary (UAB). New drainage inlets and / or castings will be installed along SR 3 and connect to the new storm sewer trunkline. An early coordination letter was sent to the New Castle MS4 coordinator on March 2, 2021. The New Castle MS4 coordinator did not respond to the early coordination letter.</p> <p><u>Public Water System</u> Based on a desktop review, a site visit on October 12, 2020 by RQAW, aerial map of the project area (Appendix B, pages B-3 to B-8), and coordination with the project designer, the project is located where there is a public water system. Impacts to the public water system may occur due to relocation of water mains, service lines, and / or fire hydrants. An early coordination letter was sent to the New Castle Utilities on March 2, 2021. The New Castle Utilities did not respond to the early coordination letter. Utility coordination is ongoing.</p>		
Floodplains:	No:	Yes: ✓
<p>Based on a desktop review of the IDNR Indiana Floodway Information Portal website (Indiana Floodplain Information Portal) on March 2, 2021 by RQAW, and the RFI report, the project is in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix E, pages E-17 to E-20 and Appendix F, pages F-28 and F-29). An early coordination letter was sent to the Local Floodplain Administrator on March 2, 2021. The Local Floodplain Administrator did not respond to the early coordination letter.</p> <p>Proposed slip-lined and replacement structures are not located within a mapped floodplain. Structure Number CV 003-033-113.21, the only structure within a mapped floodplain, will not be impacted. As such, the project qualifies as a Category 3 according to the current INDOT CE Manual, which states, The modifications to drainage structures included in this project will result in an insubstantial change in their</p>		

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this encroachment is not substantial.

Farmland:	No: <input checked="" type="checkbox"/>	Yes:
------------------	---	------

Based on a desktop review, a site visit on October 12, 2020 by RQAW, and aerial map of the project area (Appendix B, pages B-3 to B-8), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to the project. Impacts are not expected. In their early coordination response, the NRCS stated the project will not cause a conversion of prime farmland (Appendix C, page C-22).

Cultural Resources:	No: <input checked="" type="checkbox"/>	Yes:
----------------------------	---	------

On August 6, 2021 the INDOT Cultural Resources Office (CRO) determined the project falls within the guidelines of Category A, Type A-9 and Category B, Types B-1, B-2, and B-9 under the Minor Projects Programmatic Agreement (MPPA) (Appendix D, pages D-1 to D-6).

1. **Category A, Type A-9** projects involve installation, repair, or replacement of erosion control measures along roadways, waterways and bridge piers occurs within previously disturbed soils.
2. **Category B, Type B-1** projects involve replacement, repair, or installation of curbs, curb ramps, or sidewalks, including when such projects are associated with roadway work such as surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking.
3. **Category B, Type B-2** projects involve installation of new lighting, signals, signage, and other traffic control devices.
4. **Category B, Type B-9** projects involve installation, replacement, repair, lining, or extension of culverts and other drainage structures.

South Mound Cemetery is adjacent to both sides of SR 3 in the central project area and Elliott Cemetery is adjacent to the west side of SR 3 in the southern project area (Appendix B, pages B-4 and B-6). Per coordination with INDOT CRO on June 10, 2021, a Cemetery Development Plan will not be needed because both cemetery boundaries are clearly defined at the existing right-of-way and the project will not require any right-of-way from either cemetery. Additionally, work outside the existing pavement will occur in previously disturbed soils; therefore, the project will not impact either cemetery and / or need to purchase right-of-way from either cemetery (Appendix D, page D-7).

An archaeological survey was not required because the project will take place in previously disturbed soils. No further consultation is required. This completes the Section 106 process and the responsibilities of the FHWA under Section 106 have been fulfilled.

Section 4(f) and Section 6(f) Resources:	No: <input checked="" type="checkbox"/>	Yes:
---	---	------

Section 4(f)
 Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and National Register of Historic Places (NRHP) eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

Based on a desktop review, aerial map of the project area (Appendix B, pages B-3 to B-8), and the RFI report (Appendix E, page E-16), there are 20 potential Section 4(f) resources mapped within the 0.50-mile search radius. According to additional research, and a site visit on October 12, 2020 by RQAW, there are four Section 4(f) resources located within or adjacent to the project area.

1. **Your Park** is adjacent to SR 3 in the northern project area (Appendix B, page B-4). Your Park is a Section 4(f) resource because it is publicly owned and managed by the New Castle Parks and Recreation Department. The project will not use this resource by taking permanent right-of-way and will not indirectly use the resource in such a way that the protected activities, features, or attributes that qualify a resource for protection under Section 4(f) are substantially impaired. Therefore, no Section 4(f) use is expected. An early coordination letter was sent to the New Castle Parks and Recreation Department on March 2, 2021. The New Castle Parks and Recreation Department did not respond to the early coordination letter.
2. **Potential Trail** is part of the New Castle Share Use Paths and is in the northern project area (Appendix B, page B-4).
3. **Potential Trail** is part of the New Castle Share Use Paths and is in the central project area (Appendix B, page B-6).
4. **Potential Trail** is part of the New Castle Share Use Paths and is along the entire project area (Appendix B, pages B-4 to B-8).

These three potential trails are potential Section 4(f) resources because they would be publicly owned and managed by New Castle once constructed. The project will not alter the project area to prevent these potential trails from being constructed in the future. An early coordination letter was sent to the Public Works of New Castle on March 2, 2021. The Public Works of New Castle responded to the early coordination letter on March 2, 2021 and stated that, currently, there is no funding, design, or schedule for the potential trails (Appendix C, page C-18). Therefore, no Section 4(f) use is expected.

Section 6(f)

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of Section 6(f) properties on the INDOT ESD website revealed a total of two properties in Henry County (Appendix I, page I-1). Neither of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to Section 6(f) resources.

Air Quality:	No: <input checked="" type="checkbox"/>	Yes: <input type="checkbox"/>
---------------------	---	-------------------------------

STIP

The project is included in the Fiscal Year (FY) 2024-2028 Statewide Transportation Improvement Program (STIP) (Appendix H, page H-1). Des. Number 2003091 is included by reference. The construction funds will need to be updated to reflect the current cost of construction, which is being updated by STIP Amendment A24-04.

Attainment Status

The project is in Henry County, which is currently in attainment for all criteria pollutants according to the EPA Green Book website ([Nonattainment Areas for Criteria Pollutants \(Green Book\) | US EPA](#)). Therefore, the conformity procedures of 40 Code of Federal Regulations (CFR) Part 93 do not apply.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

Mobile Source Air Toxics (MSAT)

The project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126, and as such, a MSAT analysis is not required.

Community Impacts:

No:

Yes:

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. The project will have no relocations and will require less than 0.50 acre of additional permanent right-of-way; therefore, an EJ analysis is not required according to the current INDOT CE Manual.

The project is not anticipated to result in substantial impacts to the community. The installation of sidewalks and pedestrian signals and crosswalks will improve access to properties within the area. However, the elimination of the TWLTL median will result in traffic accessing properties via right-in and right-out only which will deviate from current conditions. There are several businesses and residences within the project area; however, access to all properties will be maintained during construction. According to the Fairs and Festivals website ([Find Art Shows, Craft Shows, and Festivals near you \(fairsandfestivals.net\)](http://fairsandfestivals.net)), accessed on March 15, 2021 by RQAW, no fairs or festivals are scheduled within a 10-mile radius of zip code 47362 (project area). Any future fairs / festivals that may be planned are unlikely to be impacted by the project since vehicles can utilize SR 3 during construction.

Public Facilities and Services (e.g., schools, emergency services):

No:

Yes:

Based on a desktop review, aerial map of the project area (Appendix B, pages B-3 to B-8), and the RFI report (Appendix E, page E-16), there are 38 public facilities mapped within the 0.50-mile search radius. Nine public facilities are mapped within or adjacent to the project area. Seven public facilities were confirmed by the site visit on October 12, 2020 by RQAW.

1. **Your Park** is adjacent to SR 3 in the northern project area (Appendix B, page B-4). Permanent or temporary right-of-way will not be needed from the park. An early coordination letter was sent to the New Castle Parks and Recreation Department on March 2, 2021. The New Castle Parks and Recreation Department did not respond to the early coordination letter. Impacts are not expected.
2. **Potential Trail** is part of the New Castle Share Use Paths and is in the northern project area (Appendix B, page B-4). The project will not alter the project area to prevent this potential trail from being constructed in the future. Impacts are not expected.
3. **Potential Trail** is part of the New Castle Share Use Paths and is in the central project area (Appendix B, page B-6). The project will not alter the project area to prevent this potential trail from being constructed in the future. Impacts are not expected.
4. **Potential Trail** is part of the New Castle Share Use Paths and is along the entire project area (Appendix B, pages B-4 to B-8). The project will not alter the project area to prevent this potential trail from being constructed in the future. Impacts are not expected.
5. **First Assembly of God** is a religious facility adjacent to SR 3 in the southern project area (Appendix B, page B-8). Access to the church will be maintained during construction. Permanent or temporary right-of-way will not be needed from the church. An early coordination letter was sent to the First Assembly of God on March 2, 2021. The church did not respond to the early coordination letter. Impacts are not expected.
6. **South Memorial Drive Church of God** is a religious facility adjacent to SR 3 in the southern project area (Appendix B, page B-8). Access to the church will be maintained during construction. Permanent or temporary right-of-way will not be needed from the church. An early coordination letter was sent to the South Memorial Drive Church of God on March 2, 2021. The church did not respond to the early coordination letter. Impacts are not expected.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

7. **Kingdom Hall of Jehovah's Witness** is a religious facility within the southern project area (Appendix B, page B-8). Although not adjacent to the project area, the only access roads to the church are located off SR 3. Access to the church will be maintained during construction. Permanent or temporary right-of-way will not be needed from the church. An early coordination letter was sent to the Kingdom Hall of Jehovah's Witness on March 2, 2021. The church did not respond to the early coordination letter. Impacts are not expected.

Refer to *Section 4(f) Resources / Section 6(f) Resources* of this CE document for a detailed discussion of the park and three potential trails. Impacts are not expected. Access to all properties will be maintained during construction.

There are two underground gas pipelines within the project area; one pipeline segment is within the central portion of the project area and one pipeline segment is within the southern portion of the project area (Appendix B, pages B-4 to B-8). Coordination with INDOT Utilities and Railroads will occur by the designer during utility coordination.

Additionally, the New Castle-Henry County Municipal Airport is within 3.80 miles of the project area. An early coordination letter was sent to the INDOT Office of Aviation on March 2, 2021. The INDOT Office of Aviation responded to the early coordination letter on March 3, 2021 and stated that if any object, obstruction, or equipment will exceed 145 feet in height, further coordination will be required (Appendix C, page C-17). According to the project designer, the project will not involve any object, obstruction, or equipment taller than 145 feet. Additional coordination is not needed. Impacts are not expected.

The Public Works of New Castle responded to the early coordination letter on March 2, 2021 and stated that, currently, there is no funding, design, or schedule for the potential trails (Appendix C, page C-18).

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Hazardous Materials and Regulated Substances:	No:	Yes: <input checked="" type="checkbox"/>
--	-----	--

Based on a review of geographic information system (GIS) and available public records, a RFI report was concurred by INDOT Site Assessment & Management (SAM) on May 26, 2021 (Appendix E, pages E-1 to E-25). Fifty-eight hazardous material concern sites are mapped within the 0.50-mile search radius. Eleven hazardous material concern sites could affect the project area.

1. Doug Furbee, a State Cleanup, Voluntary Remediation Program (VRP), and Institutional Control Site located at 715 South Memorial Drive, is adjacent to the northern project area. Remediation efforts have been completed under the VRP. The site received a Certification of Completion, issued by IDEM on November 18, 2002, and a Covenant Not to Sue, issued by IDEM on June 25, 2003. Low levels of petroleum contamination in the groundwater and soil remain on the site and groundwater flow is to the northwest. The only institutional control / land use restrictions for the site limit the use of the property for residential purposes. If excavation occurs in this area, proper handling, removal, and disposal of soil and / or groundwater may be necessary. Before proper removal and disposal of soil and / or groundwater, analysis for lead will be necessary. A deed notice was recorded on the site on December 18, 2002. Coordination with Lynette Schrowe (lschrowe1@idem.in.gov) should occur before Ready for Proposals (RFP).
2. Q Lube, an Underground Storage Tank (UST) Site located at 1464 South Memorial Drive, is adjacent to central project area. The site is an automotive oil change facility. Documentation regarding the status of the bulk oil and spent oil USTs was not found. A Phase II Environmental Site Assessment (ESA) was recommended.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

A Phase II ESA was completed by SESCO on September 17, 2021. Soil and groundwater samples were collected at Q-Lube. Results indicate the concentrations of all constituents of concern (COCs) were below their respective IDEM Remediation Closure Guide (RCG) screening levels. Based on the soil and groundwater analytical results, the use of the Uncontaminated Soil Policy (Waste-0064-NPD) can be applied for the project area that was investigated during the Phase II ESA activities and standard Personal protective equipment (PPE) is sufficient for providing worker safety (Appendix E, pages E-26 to E-31).

3. Amoco (Goodwin Brothers), a UST and Leaking Underground Storage Tank (LUST) Site located at 250 Broad Street, is adjacent to the northern project area. The site is a former gas station. According to the No Further Action (NFA) Approval Pursuant to 1994 UST Branch Guidance, issued by IDEM on January 7, 2008, soil and groundwater contamination remains on the site, and near the project area. Groundwater flow is to the southwest. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Before proper removal and disposal of soil and / or groundwater, analysis for lead will be necessary.
4. Bob Morris Chevrolet (CVS), a LUST Site located at 200 South Memorial Drive, is adjacent to the northern project area. Two USTs were removed from the site, with remediation efforts following the removal in in 1990. A letter from IDEM, dated October 7, 1991, states that low levels of contamination remain on site after remediation efforts took place. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Before proper removal and disposal of soil and / or groundwater, analysis for lead will be necessary.
5. Fast Max Sunoco (Fast Shop N Go) (BP Fueling Station), a LUST Site located at 310 South Memorial Drive, is adjacent to the northern project area. The site is an active gas station. According to the Initial Site Characterization Report, dated October 28, 2019, the extent of the petroleum contamination within the soil and groundwater is not fully delineated. Contamination appears to be present at the western portion of the site, adjacent to the project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Coordination with Stephen Onochie (sonochie@idem.in.gov) should occur before RFP. If groundwater monitoring wells are encountered in the project area, they should be maintained in place. If they cannot be maintained, then the contractor must contact the INDOT Project Manager who will notify the INDOT Permits Group. The INDOT Permits Group will notify the permit holder that the well must be removed prior to construction. The permit holder is responsible for coordination with IDEM and the INDOT Permits Group for replacement or relocation of the well. If a property owner cannot be found in connection with the monitoring well, then well abandonment will be included in the project contract. All well abandonment activities must be completed by an Indiana Licensed Well Driller in accordance with Indiana Administrative Code (IAC) 312- 13-10. Regardless of whether the well is abandoned by the contractor or the property owner, a record of well abandonment, including the well driller's license number, must be provided to the INDOT Project Manager once the well has been abandoned.
6. Former Speedway (Emro Marketing Wake Up) #6034, an Institutional Control and LUST Site located at 315 South Memorial Drive, is adjacent to the northern project area. The site is a former gas station. According to the NFA Approval Determination Pursuant to RCG, issued by IDEM on July 25, 2018, contamination remains in the groundwater, in the vicinity of the former UST basin. An Environmental Restrictive Covenant (ERC) for the site was recorded with Henry County on June 28, 2018. The ERC specifically prohibits the use of groundwater, but not soil. The groundwater at

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

the site appears to fluctuate between approximately 8 feet and 11 feet below ground surface (bgs). If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Coordination will be conducted with the IDEM Project Manager Lynette Schrowe at lschrowe1@idem.in.gov before further site activities occur.

7. Red Barn #68 (Walgreens), an Institutional Control and LUST Site located at 100 North Memorial Drive, is adjacent to the northern project area. The site is a former gas station. According to the NFA Approval Pursuant to RSG, issued by IDEM on December 19, 2018, contamination remains on the site at the southwest corner, near the project area. However, up to approximately 5 feet of soil bgs has been remediated in this area and is clean site wide. An ERC for the site was recorded with Henry County on November 28, 2018. The ERC specifically prohibits the use of groundwater, but not soil. Groundwater appears to fluctuate between approximately 5 feet bgs and 10 feet bgs at the site, in the vicinity of the project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Before proper removal and disposal of soil and / or groundwater, analysis for lead will be necessary. Coordination will be conducted with Lynette Schrowe at lschrowe1@idem.in.gov before further site activities occur.
8. Swifty Station # 175 (Marathon Star Light Food Mart), an Institutional Control and LUST Site located at 1909 South Memorial Drive, is adjacent to the central project area. The site is an active gas station. According to the NFA Approval Determination Pursuant to Risk Integrated System of Closure, issued by IDEM on August 10, 2011, all soil samples collected were below the Risk Integrated System of Closure (RISC). Contamination remains on the site at depths nearing approximately 20 feet bgs. An ERC for the site was recorded with Henry County on November 4, 2010. The ERC specifically prohibits the use of groundwater and restricts the soil disturbance activities. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Coordination with Lynette Schrowe (lschrowe1@idem.in.gov) should occur before RFP.
9. Storage Prime New Castle, a National Pollutant Discharge Elimination System (NPDES) Facility located at 1501 South Memorial Drive, is adjacent to the central project area. The permit for this facility expires on January 8, 2025. Coordination with Storage Prime New Castle will occur.
10. South Industrial Park Interceptor Project, a NPDES Facility located at Spiceland Road and Memorial Drive, is adjacent to the southern project area. The permit for this facility expires on March 4, 2023. Coordination with the City of New Castle will occur.
11. A NPDES pipe at the SR 3/Spiceland Road Intersection is adjacent to the central project area. Coordination with the New Castle Waste Water Treatment Plant (WWTP) will occur.

After the RFI was concurred by INDOT SAM, it was determined that temporary right-of-way will be needed at five areas (commercial entrances) for traffic light signal improvements. Because the temporary right-of-way will be needed within the RFI footprint already examined, the RFI was not updated. Additionally, a supplemental desktop review of GIS and available public records was completed on January 24, 2024, by RQAW due to the RFI approval exceeding one year. No additional sites with hazardous material concerns (hazmat sites) or sites involved with regulated substances were identified near the project area. Therefore, an RFI addendum is not needed.

Permits:	No:	Yes: ✓
-----------------	-----	--------

A USACE Section 404 Regional General Permit and IDEM Section 401 Water Quality Certification will likely be required due to stream and wetland impacts.

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

The total area of land disturbance is approximately 37 acres. Because the project will result in more than one acre of land disturbance, an IDEM Construction Stormwater General Permit (CSGP) (formerly Rule 5) permit will likely be required.

In their early coordination response, the IDNR Division of Fish and Wildlife stated the project may require formal approval for construction in a floodway unless it qualifies for a bridge exemption or qualifies under the INDOT and IDNR MOU for Maintenance Activity, dated March 1997 (Appendix C, pages C-19 to C-21). An IDNR Construction in a Floodway permit is not anticipated to be required for this project as no impacts below the base floodplain elevation (Q100) are anticipated.

Applicable recommendations provided by resource agencies are included in the *Environmental Commitments* section of this CE document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

ENVIRONMENTAL COMMITMENTS:

Firm

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division and the INDOT District Environmental Section will be contacted immediately. (INDOT Environmental Services Division and INDOT District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT Environmental Services Division)
3. Any work in a wetland area within right-of-way or in borrow / waste areas is prohibited unless specifically allowed in the U.S. Army Corps of Engineers permit. (INDOT Environmental Services Division)
4. There are two underground gas pipelines within the project area; one pipeline segment is within the central portion of the project area and one pipeline segment is within the southern portion of the project area. Coordination with INDOT Utilities and Railroads will occur by the designer during utility coordination. (INDOT Environmental Services Division)
5. USFWS Bridge / Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after October 2022, inspection of Structure Numbers CV 003-033-112.35, CV 003-033-112.57, and CLV 46722 by a qualified individual, must be performed. Inspections of the structures should check for presence of bats / bat indicators and / or presence of birds. The results of the inspections must indicate no signs of bats or birds. If signs of bats or birds are documented during these inspections, the INDOT District Environmental Manager must be contacted immediately. (INDOT Greenfield District)
6. Portions of Cemetery Run and Wetlands A through G to remain undisturbed by construction will be labeled as 'Do Not Disturb' on the final design plans. Elliot Run, Mound Run, as well as Wetlands B-D and F to remain undisturbed by construction will be labeled as 'Do Not Disturb' on the final design plans. (INDOT Environmental Services Division)
7. If any object, obstruction, or equipment will exceed 145 feet in height, further coordination will be required. (INDOT Office of Aviation)
8. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA / FRA / FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

9. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
10. Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" or 0 and "backlight" as low as practicable. (USFWS)
11. Tree Removal AMM 1: Modify all phases / aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
12. Tree Removal AMM 2: Apply time of year restrictions (October 1 through March 31) for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road / rail surface and outside of documented roosting / foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS)
13. Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging / fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
14. Tree Removal AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)
15. A State Cleanup / VRP / Institutional Control Site (Doug Furbee, 715 South Memorial Drive) is adjacent to the northern project area. If excavation occurs in this area, proper handling, removal, and disposal of soil and / or groundwater may be necessary. Before proper removal and disposal of soil and / or groundwater, analysis for lead will be necessary. Prior to letting, RQAW will coordinate with Lynette Schrowe (lschrowe1@idem.in.gov). (INDOT Site Assessment & Management)
16. A UST Site (Q Lube, 1464 South Memorial Drive) is adjacent to central project area. A Phase II Environmental Site Assessment (ESA) was completed and it was determined that the use of the Uncontaminated Soil Policy (Waste-0064-NPD) can be applied for the project area that was investigated during the Phase II ESA activities and standard Personal protective equipment (PPE) is sufficient for providing worker safety. (Appendix E, pages E-26 to E-31).
17. A UST / LUST Site (Amoco [Goodwin Brothers], 250 Broad Street) is adjacent to the northern project area. If excavation occurs in this area, it is possible that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Before proper removal and disposal of soil and / or groundwater, analysis for lead will be necessary. (INDOT Site Assessment & Management)
18. A LUST Site (Bob Morris Chevrolet [CVS], 200 South Memorial Drive) is adjacent to the northern project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Before proper removal and disposal of soil and / or groundwater, analysis for lead will be necessary. (INDOT Site Assessment & Management)
19. A LUST Site (Fast Max Sunoco [Fast Shop N Go] [BP Fueling Station], 310 South Memorial Drive) is adjacent to the northern project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and/or groundwater may be necessary. Prior to letting, RQAW will coordinate with Stephen Onochie (sonochie@idem.in.gov). If groundwater monitoring wells are encountered in the project area, they should be maintained in place. If they cannot be maintained, then the contractor must contact the INDOT Project Manager who will notify the INDOT Permits Group. The INDOT Permits Group will notify the permit holder that the well must be removed prior to construction. The permit holder is

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

responsible for coordination with IDEM and the INDOT Permits Group for replacement or relocation of the well. If a property owner cannot be found in connection with the monitoring well, then well abandonment will be included in the project contract. All well abandonment activities must be completed by an Indiana Licensed Well Driller in accordance with IAC 312- 13-10. Regardless of whether the well is abandoned by the contractor or the property owner, a record of well abandonment, including the well driller's license number, must be provided to the INDOT Project Manager once the well has been abandoned. (INDOT Site Assessment & Management)

20. A LUST / Institutional Control Site (Former Speedway [Emro Marketing Wake Up] #6034, 315 South Memorial Drive) is adjacent to the northern project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Prior to letting, RQAW will coordinate with the IDEM Project Manager Lynette Schrowe at lschrowe1@idem.in.gov. (INDOT Site Assessment & Management)
21. A LUST / Institutional Control Site (Red Barn #68, 100 North Memorial Drive) is adjacent to the northern project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Before proper removal and disposal of soil and / or groundwater, analysis for lead will be necessary. Prior to letting, RQAW will coordinate with Lynette Schrowe at lschrowe1@idem.in.gov. (INDOT Site Assessment & Management)
22. A LUST / Institutional Control Site (Swift Station # 175 [Marathon Star Light Food Mart], 1909 South Memorial Drive) is adjacent to the central project area. If excavation occurs in this area, it is likely that petroleum contamination will be encountered. Proper handling, removal, and disposal of soil and / or groundwater may be necessary. Prior to letting, RQAW will coordinate with Lynette Schrowe (lschrowe1@idem.in.gov). (INDOT Site Assessment & Management)
23. A NPDES Facility (Storage Prime New Castle, 1501 South Memorial Drive) is adjacent to the central project area. Prior to letting, RQAW will coordinate with Storage Prime New Castle. (INDOT Site Assessment & Management)
24. A NPDES Facility (South Industrial Park Interceptor Project, Spiceland Road and Memorial Drive) is adjacent to the southern project area. Prior to letting, RQAW will coordinate with the City of New Castle. (INDOT Site Assessment & Management)
25. A NPDES Pipe at the SR 3 / Spiceland Road Intersection is adjacent to the central project area. Prior to letting, RQAW will coordinate with the New Castle Waste Water Treatment Plant (WWTP). (INDOT Site Assessment & Management)

For Further Consideration

1. If box or pipe culverts are used, the bottoms should be buried to a minimum of six inches (or 20% of the culvert height / pipe diameter, whichever is greater up to a maximum of two feet) below the stream bed elevation to allow a natural streambed to form within or under the crossing structure. Crossings should span the entire channel width (a minimum of 1.2 times the bankful width), maintain the natural stream substrate within the structure, have a minimum openness ratio (height x width / length) of 0.25, and have stream depth and water velocities during low-flow conditions that are approximate to those in the natural stream channel. Banklines should be restored within box and pipe structures to allow for wildlife passage above the OHWM. (IDNR Division of Fish and Wildlife)
2. The new, replacement, or rehabbed structure, and any bank stabilization under the structure, should not create conditions that are less favorable for wildlife passage under the structure compared to the current conditions. (IDNR Division of Fish and Wildlife)
3. If feasible, a larger bridge or culvert opening can allow for the movement of wildlife under the roadway to minimize wildlife / vehicle collisions. (IDNR Division of Fish and Wildlife)

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

4. While hard armoring alone (e.g., riprap or glacial stone) may be needed in certain instances, soft armoring and bioengineering techniques should be considered first. In many instances, one or more methods are necessary to increase the likelihood of vegetation establishment. Combining vegetation with most bank stabilization methods can provide additional bank protection and help reduce impacts upon fish and wildlife. (IDNR Division of Fish and Wildlife)
5. Riprap must not be placed in the active thalweg channel or placed in the streambed in a manner that precludes fish or aquatic organism passage (riprap must not be placed above the existing streambed elevation). Riprap may be used only at the toe of the side slopes up to the OHWM. The banks above the OHWM must be restored, stabilized, and revegetated using geotextiles and a mixture of grasses, sedges, wildflowers, shrubs, and trees native to Eastern Indiana and specifically for stream bank / floodway stabilization purposes as soon as possible upon completion. (IDNR Division of Fish and Wildlife)
6. Impacts to non-wetland forest of 1 acre or more should be mitigated at a minimum 2:1 ratio. Impacts to non-wetland forest under 1 acre in an urban setting should be mitigated by planting five trees, at least 2 inches in dbh, for each tree which is removed that is 10 inches dbh or greater (5:1 mitigation based on the number of large trees) or by using the 1:1 replacement ratio based on area depending on the type of habitat impacted (individual canopy tree removal in an urban streetscape or park-like environment versus removal of habitat supporting a tree canopy, woody understory, and herbaceous layer). Impacts under 0.10 acre in an urban area may still involve the replacement of large diameter trees but typically do not require any additional mitigation or additional plantings beyond seeding and stabilizing disturbed areas. There are exceptions for high quality habitat sites, however. (IDNR Division of Fish and Wildlife)
7. Impacts to wetland habitat should be mitigated at the appropriate ratio according to the 1991 INDOT / IDNR / USFWS Memorandum of Understanding. (IDNR Division of Fish and Wildlife)
8. Do not excavate in the low flow area except for the placement of piers, foundations, and riprap, or removal of the old structure. (IDNR Division of Fish and Wildlife)
9. Do not construct any temporary runarounds, access bridges, causeways, cofferdams, diversions, or pumparounds. (IDNR Division of Fish and Wildlife)
10. Do not cut any trees suitable for Indiana bat or Northern long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30. (IDNR Division of Fish and Wildlife)
11. Operate equipment used to replace the bridge from the existing roadway. (IDNR Division of Fish and Wildlife)
12. Use minimum average 6-inch graded riprap stone extended below the normal water level to provide habitat for aquatic organisms in the voids. (IDNR Division of Fish and Wildlife)
13. Restrict below low-water work in streams to placement of culverts, piers, pilings and / or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap. (USFWS)
14. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If riprap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat. (USFWS)
15. Avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30), except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below the OHWM during this time unless the machinery is within the caissons or on the cofferdams. (USFWS)
16. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing. (USFWS)
17. Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottom culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel,

Indiana Department of Transportation

County Henry Route SR 3 Des. No. 1593230 & 2003091

cobbles, and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community. (USFWS)
18. INDOT will evaluate the possibility of utilizing traffic signal pre-emption devices for this project based on public comment. (INDOT Environmental Services Division)

Designation (Des.) Numbers 1593230 & 2003091

SR 3 Road Project – New Castle, Henry County, Indiana

Appendix A: INDOT Supporting Documentation

Categorical Exclusion Level Thresholds	A-1
--	-----

Appendix B: Graphics

General Location Map	B-1
Topographic Map	B-2
Aerial Map	B-3
Photograph Key Maps	B-4
Project Area Photographs	B-9
Preliminary Design Plans	B-23

Appendix C: Early Coordination

Example Early Coordination Letter (<i>graphics omitted</i>)	C-1
Indiana Department of Environmental Management (IDEM)	
Standard Response Letter	C-5
Groundwater Section Response E-mail.....	C-12
Indiana Geological and Water Survey	
Electronic Response	C-14
Indiana Department of Transportation (INDOT)	
Office of Aviation Response E-mail	C-17
City of New Castle	
Public Works Response E-mail	C-18
Indiana Department of Natural Resources (IDNR)	
Division of Fish and Wildlife Response Letter	C-19
Natural Resources Conservation Service (NRCS)	
Response Letter and NRCS-CPA-106 Form.....	C-22
U.S. Fish and Wildlife Service (USFWS) Coordination	
INDOT District Coordination E-mail	C-23
Structure Assessment Form	C-24
Information for Planning and Consultation (IPaC) Species List Letter	C-25
IPaC Concurrence Verification Letter.....	C-38
INDOT District Coordination E-mail	C-52

Appendix D: Section 106 of the National Historic Preservation Act (NHPA)

Minor Projects Programmatic Agreement (MPPA) Assessment Form.....	D-1
INDOT Cultural Resources Office Coordination E-mail.....	D-7

Appendix E: Red Flag Investigation and Hazardous Materials

Red Flag Investigation	E-1
Phase II Environmental Site Assessment (ESA) (<i>most graphics omitted</i>)	E-26

Appendix F: Water Resources

Waters of the U.S. Determination Report (<i>some graphics omitted</i>).....	F-1
Preliminary Jurisdictional Determination	F-26
IDNR Floodplain Information Portal Printout.....	F-30

Designation (Des.) Numbers 1593230 & 2003091

SR 3 Road Project – New Castle, Henry County, Indiana

Appendix G: Public Involvement

Example Notice of Entry for Survey or Investigation Letter	G-1
Notice of Public Hearing	G-3
Publisher’s Affidavit.....	G-5
Mailing List	G-7
Public Sign-in Sheets.....	G-13
Information Packet.....	G-23
PowerPoint Presentation	G-28
Public Speaker Sign-up Sheets	G-35
Public Hearing Transcript	G-36
Public Comments Received During Comment Period	G-44
Public Comments and Response Spreadsheet	G-78

Appendix H: Air Quality

2024-2028 Statewide Transportation Improvement Program (<i>relevant pages only</i>)	H-1
---	-----

Appendix I: Additional Studies

National Park Service Land and Water Conservation Fund List	I-1
Engineering Assessment Report (<i>appendices omitted</i>).....	I-2
Engineering Assessment Report (<i>attachments omitted</i>)	I-19
Mini Scope.....	I-30

Categorical Exclusion
Appendix A
INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	“No Effect”, “Not likely to Adversely Affect” (With select AMMs ⁶)	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic ⁷
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁸
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ⁹
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹⁰
Approval Level <ul style="list-style-type: none"> • District Env. (DE) • Env. Serv. Div. (ESD) • FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁷ Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower level CE.

⁸ Potential for causing a disproportionately high and adverse impact.

⁹ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹⁰ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

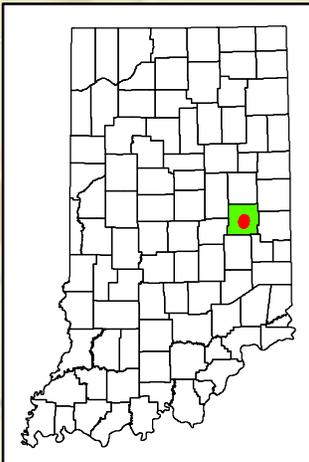
* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

Categorical Exclusion
Appendix B
Graphics

**General Location Map
Road Project
SR 3, City of New Castle
Henry County, Indiana
Des. Number 1593230**

Project Area



Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan, Esri Japan, METI, Esri China (Hong Kong), Esri Korea, Esri (Thailand), NGCC, (c) OpenStreetMap contributors, and the GIS User Community

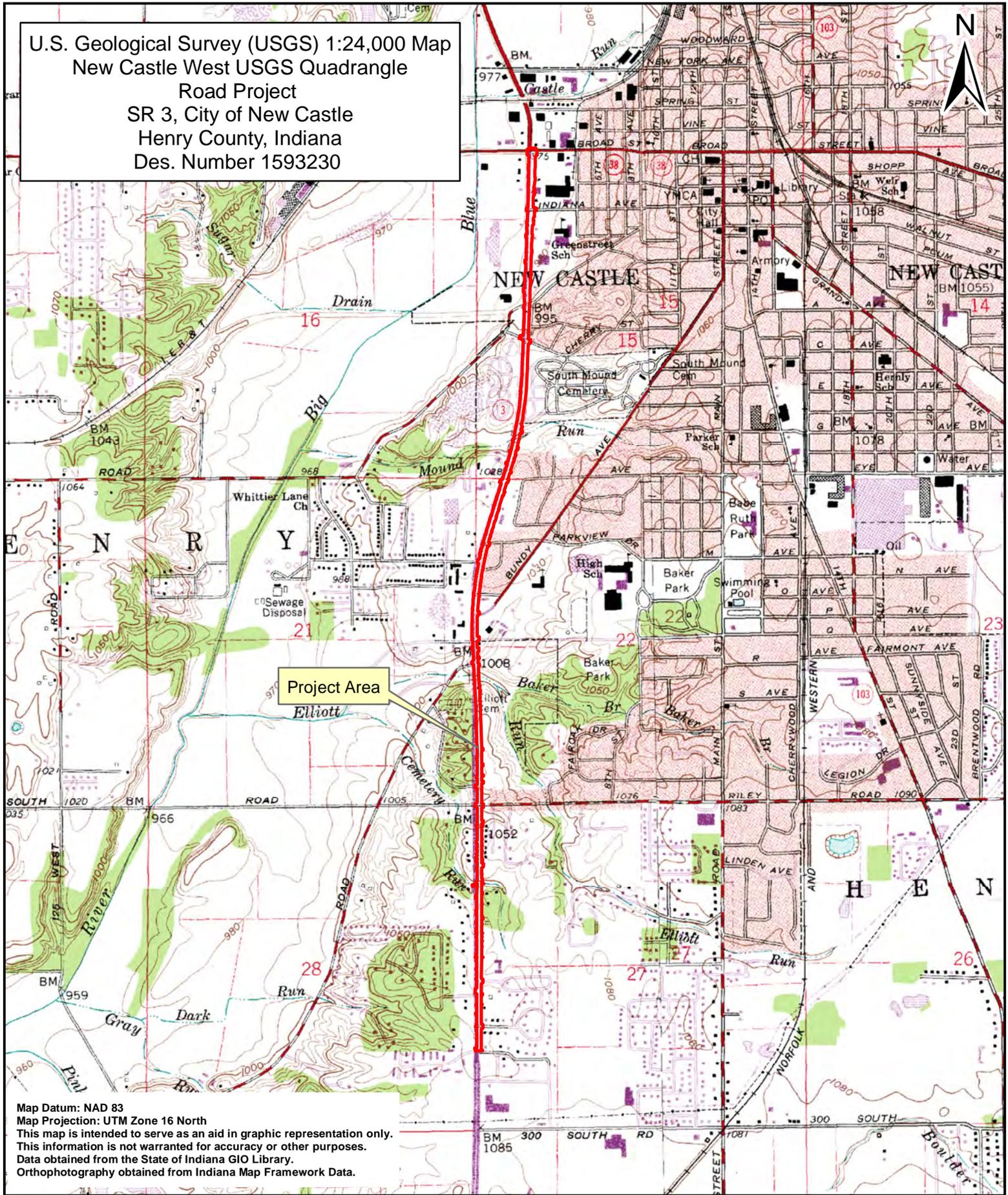


This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83
Map Projection: UTM Zone 16 North



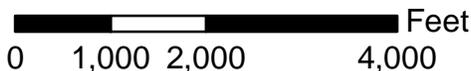
U.S. Geological Survey (USGS) 1:24,000 Map
 New Castle West USGS Quadrangle
 Road Project
 SR 3, City of New Castle
 Henry County, Indiana
 Des. Number 1593230



Map Datum: NAD 83
 Map Projection: UTM Zone 16 North
 This map is intended to serve as an aid in graphic representation only.
 This information is not warranted for accuracy or other purposes.
 Data obtained from the State of Indiana GIO Library.
 Orthophotography obtained from Indiana Map Framework Data.



USGS Topographic Map



Location: SR 3
 Township: Henry
 County: Henry

Overall Aerial Map
 Road Project
 SR 3, City of New Castle
 Henry County, Indiana
 Des. Number 1593230



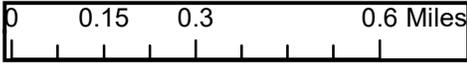
..... Construction Limits

State of Indiana



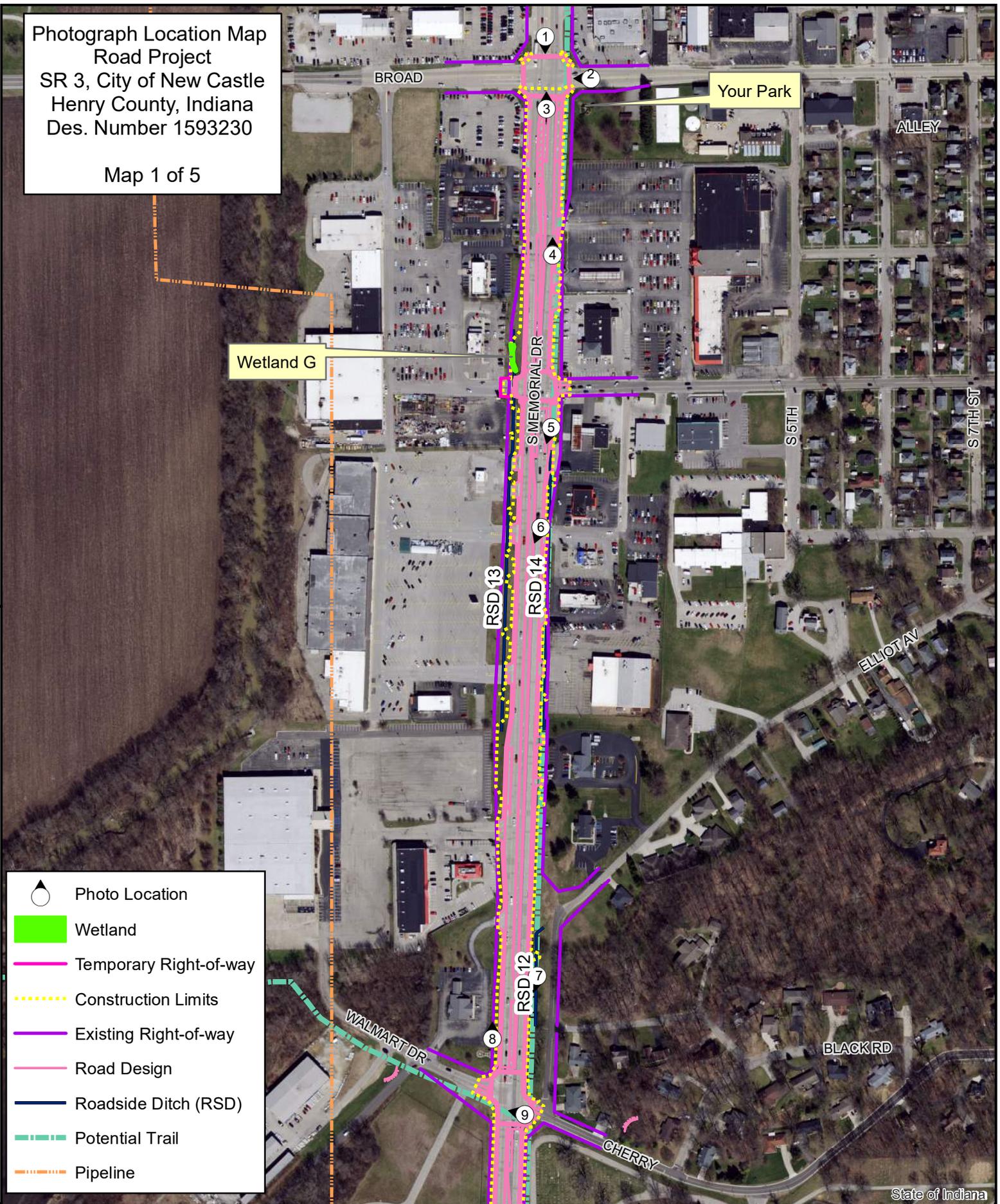
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83
 Map Projection: UTM Zone 16 North



Photograph Location Map
 Road Project
 SR 3, City of New Castle
 Henry County, Indiana
 Des. Number 1593230

Map 1 of 5

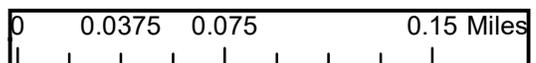


- Photo Location
- Wetland
- Temporary Right-of-way
- Construction Limits
- Existing Right-of-way
- Road Design
- Roadside Ditch (RSD)
- Potential Trail
- Pipeline

State of Indiana

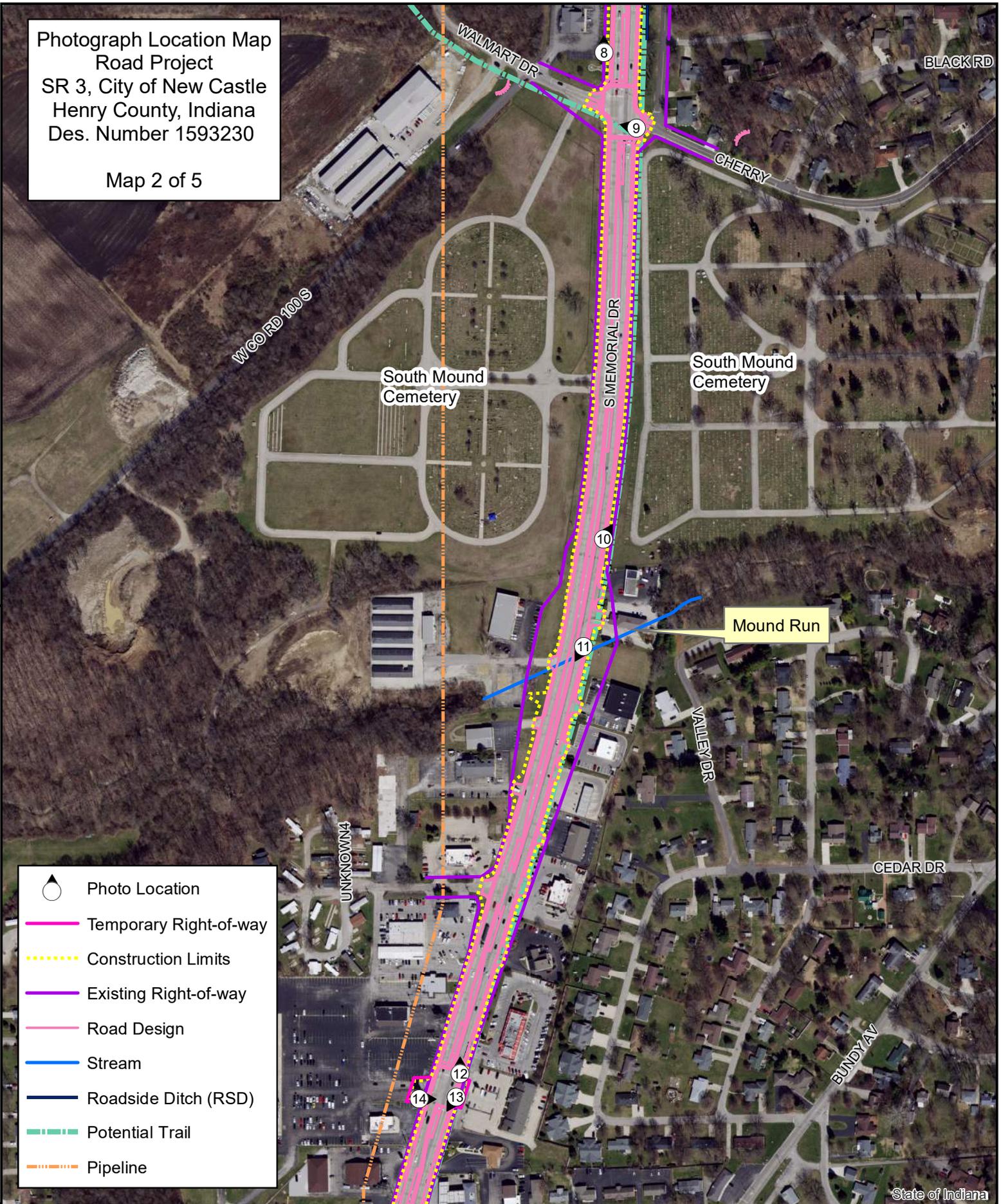
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83
 Map Projection: UTM Zone 16 North



Photograph Location Map
 Road Project
 SR 3, City of New Castle
 Henry County, Indiana
 Des. Number 1593230

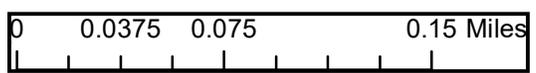
Map 2 of 5



- Photo Location
- Temporary Right-of-way
- Construction Limits
- Existing Right-of-way
- Road Design
- Stream
- Roadside Ditch (RSD)
- Potential Trail
- Pipeline

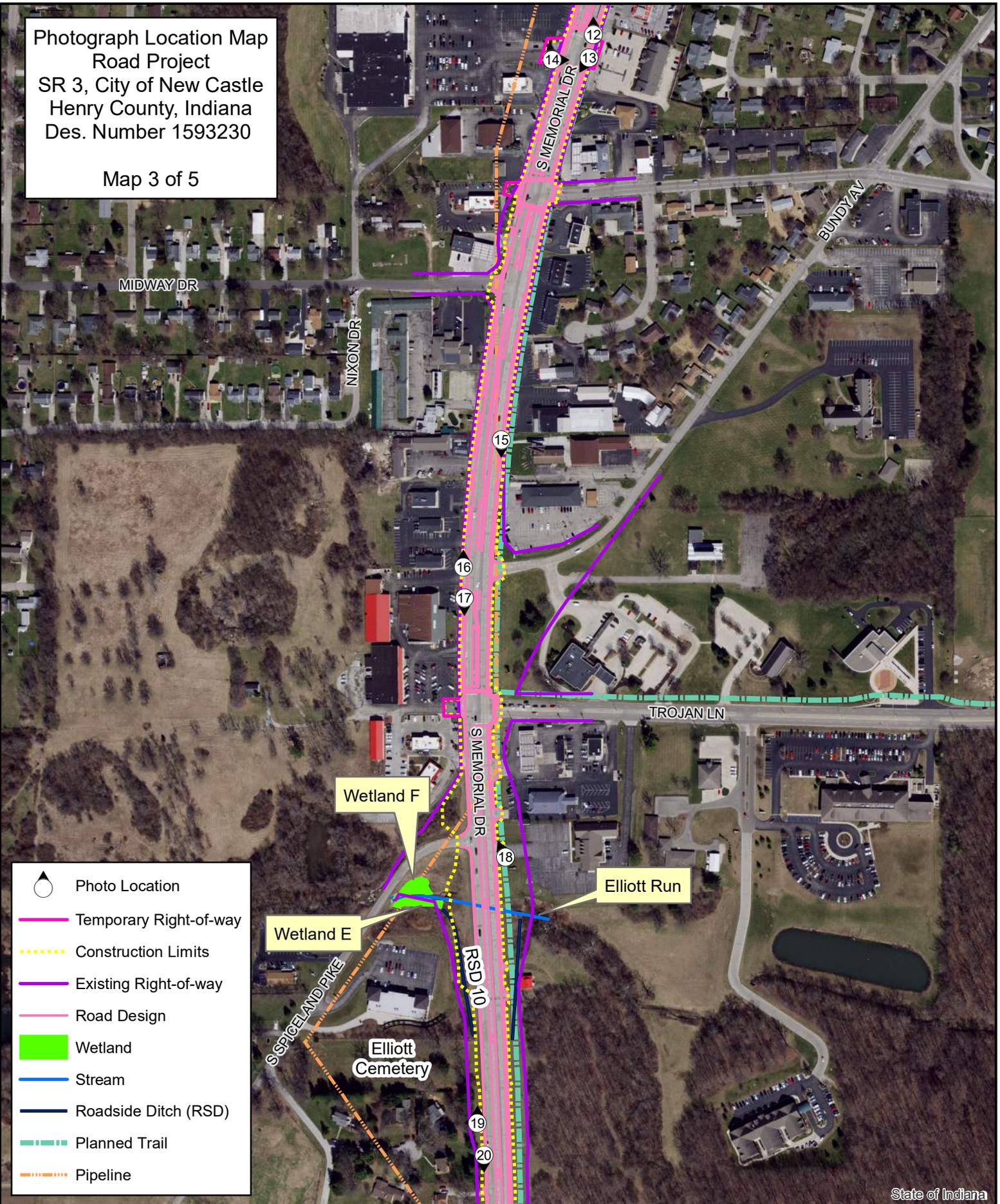
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83
 Map Projection: UTM Zone 16 North



Photograph Location Map
 Road Project
 SR 3, City of New Castle
 Henry County, Indiana
 Des. Number 1593230

Map 3 of 5



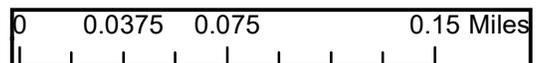
-  Photo Location
-  Temporary Right-of-way
-  Construction Limits
-  Existing Right-of-way
-  Road Design
-  Wetland
-  Stream
-  Roadside Ditch (RSD)
-  Planned Trail
-  Pipeline

State of Indiana



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83
 Map Projection: UTM Zone 16 North



Photograph Location Map
 Road Project
 SR 3, City of New Castle
 Henry County, Indiana
 Des. Number 1593230

Map 4 of 5

Wetland F

Elliott Run

Wetland E

RSD 10

19
20

MEADOWBROOK DR

UNKNOWN

Wetland D

21

RSD 9

S SPICELAND PIKE

22

WRILEY RD

RILEY RD

RSD 8

23
24

ALLEY

SMEMORIAL DR

Cemetery Run

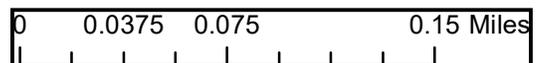
Indiana Spatial Data Portal, UITS, ESRI

	Photo Location
	Wetland
	Construction Limits
	Permanent Right-of-way
	Existing Right-of-way
	Road Design
	Replacement Structure
	Stream
	Roadside Ditch (RSD)
	Potential Trail
	Pipeline



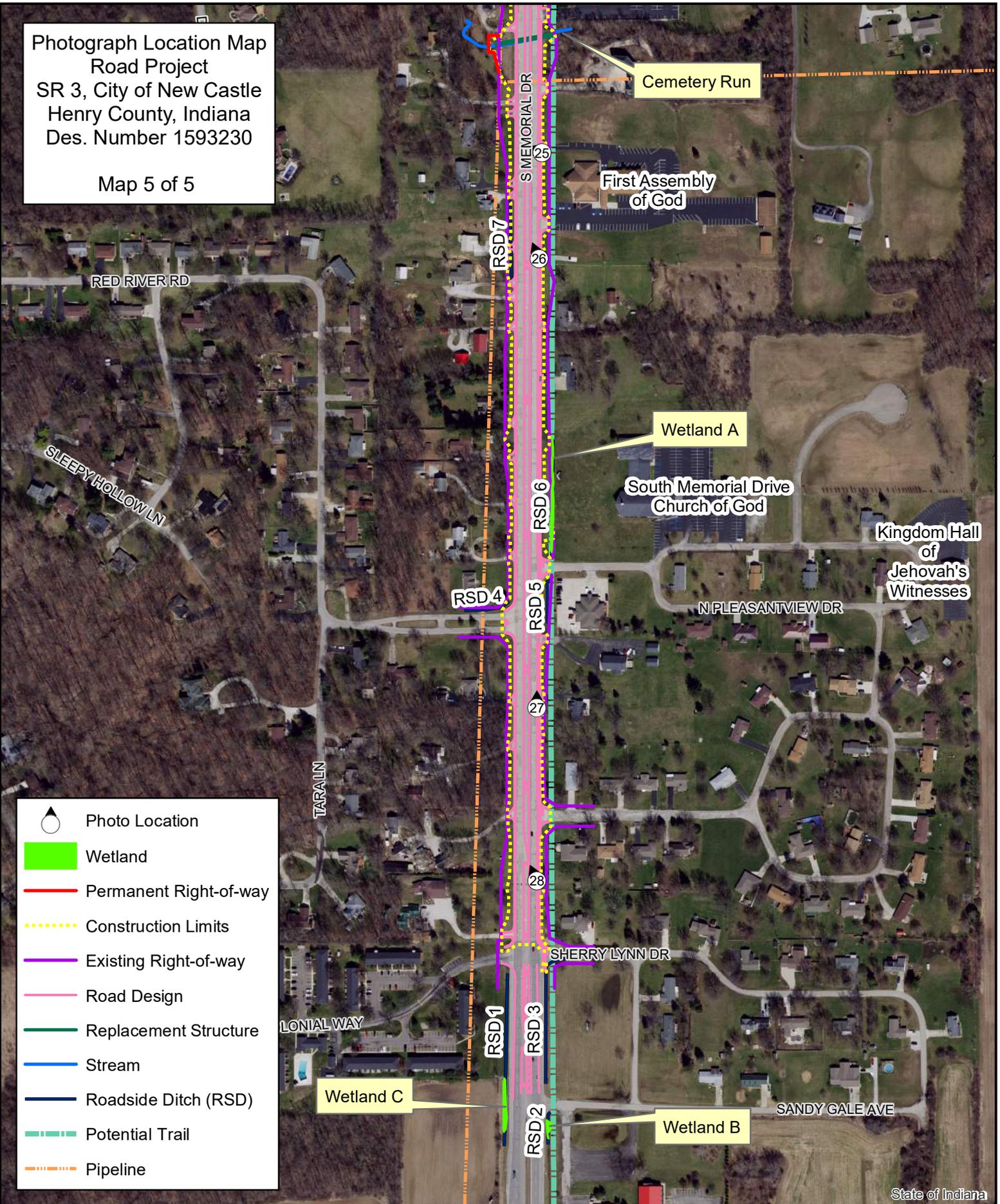
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83
 Map Projection: UTM Zone 16 North



Photograph Location Map
 Road Project
 SR 3, City of New Castle
 Henry County, Indiana
 Des. Number 1593230

Map 5 of 5



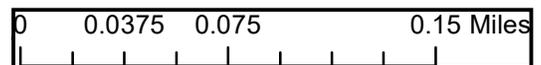
- Photo Location
- Wetland
- Permanent Right-of-way
- Construction Limits
- Existing Right-of-way
- Road Design
- Replacement Structure
- Stream
- Roadside Ditch (RSD)
- Potential Trail
- Pipeline

State of Indiana



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Map Datum: NAD 83
 Map Projection: UTM Zone 16 North





1. Looking south from north of the intersection of SR 38 and SR 3.



2. Looking west from west of the intersection of SR 38 and SR 3.



3. Looking north from south of the intersection of SR 38 and SR 3.



4. Looking north along SR 3 from east of SR 3.



5. Looking southwest along SR 3 from east of SR 3.



6. Looking southwest along SR 3 from east of SR 3.



7. Looking southwest along SR 3 from east of SR 3.



8. Looking north along SR 3 from west of SR 3.



9. Looking northwest from east of the intersection of Walmart Drive/Cherry Street and SR 3.



10. Looking northeast at South Mound Cemetery along SR 3, from east of SR 3.



11. Looking southwest along SR 3 from east of SR 3.



12. Looking north along SR 3 from east of SR 3.



13. Looking southwest along SR 3 from west of SR 3.



14. Looking east from west of SR 3.



15. Looking south along SR 3 from east of SR 3.



16. Looking north along SR 3 from west of SR 3.



17. Looking south along SR 3 from west of SR 3.



18. Looking northwest along SR 3 from east of SR 3.



19. Looking north along SR 3 from west of SR 3.



20. Looking south along SR 3 from west of SR 3.



21. Looking south along SR 3 from east of SR 3.



22. Looking northeast along SR 3 from west of SR 3.



23. Looking east across SR 3 from west of SR 3.



24. Looking east across SR 3 from west of SR 3.



25. Looking northwest across SR 3 from east of SR 3.



26. Looking northwest across SR 3 from east of SR 3.



27. Looking north along SR 3 from east of SR 3.



28. Looking northwest along SR 3 from east of SR 3.

PROJECT	DESIGNATION
1593230	1593230
	2003091
CONTRACT	R-39270

ASSET ID TABLE		
STATION	ASSET ID	DES. NUMBER
391+35	CLV 003-033-112.35	
403+19	CV 003-033-112.57	
510+96	CLV 46722	

INDIANA DEPARTMENT OF TRANSPORTATION



ROAD PLANS

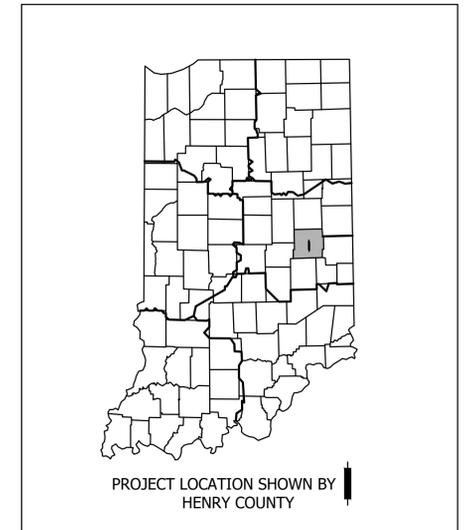
Roadway Rehabilitation

ROUTE: S.R. 3 FROM: RP 112.05 TO: RP 114.83

PROJECT NO. 1593230 PE
1593230 RW
1593230 CN

Roadway Rehabilitation on S.R. 3
Located 3.14 miles north of I-70
Sections of 15, 21, 22, 27 and 28, T-17-N, R-10-E, Henry Township, Henry County,
Indiana.

TRAFFIC DATA	
S.R. 3	
A.A.D.T. (2023)	20,849 V.P.D.
A.A.D.T. (2043)	21,960 V.P.D.
D.H.V. (2043)	2,036 V.P.H.
DIRECTIONAL DISTRIBUTION	52% NB - 48% SB
TRUCKS	5% A.A.D.T. 5% D.H.V.
DESIGN DATA	
DESIGN SPEED	45 M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	ARTERIAL
RURAL/URBAN	URBAN (INTERMEDIATE)
TERRAIN	LEVEL
ACCESS CONTROL	NONE

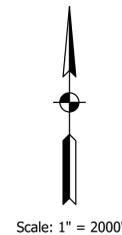
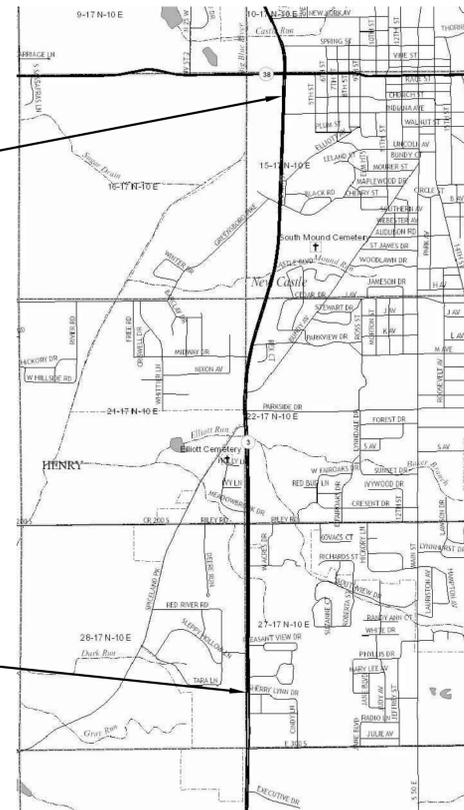


LATITUDE: 39°55'29.5" N LONGITUDE: 85°22'54.8" W

GROSS LENGTH: (14,191 FT.) 2.69 MI.
NET LENGTH: (14,191 FT.) 2.69 MI.
MAX. GRADE: 5.8 %

END PROJECT NO. 1593230
STA. 518+25.74 LINE "G"

BEGIN PROJECT NO. 1593230
STA. 376+35.00 LINE "G"



These Plans Prepared By:



5925 Lakeside Blvd.
Indianapolis, Indiana 46278
(317) 290-9549

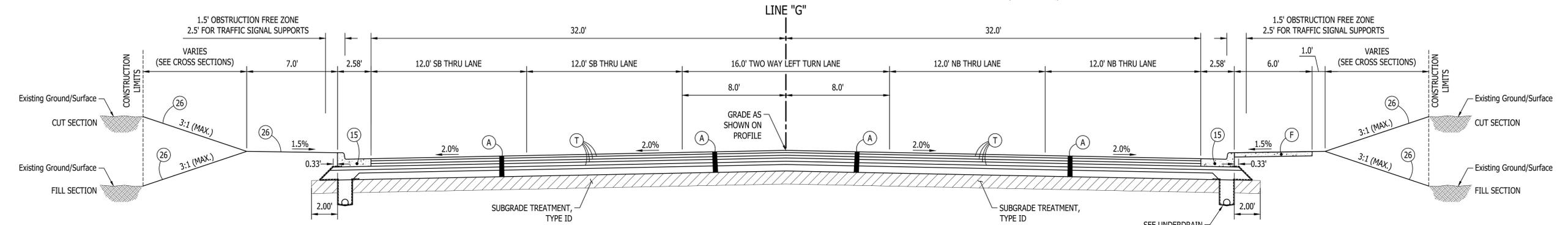
City of New Castle

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2024 TO
BE USED WITH THESE PLANS

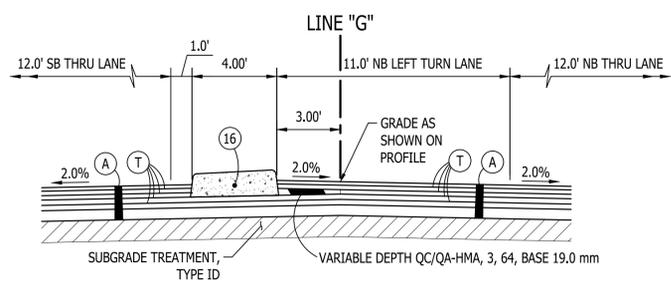
PLANS PREPARED BY: FIRST GROUP ENGINEERING, INC. (317) 290-9549 PHONE NUMBER
CERTIFIED BY: _____ DATE _____
APPROVED FOR LETTING: _____ DATE _____
INDIANA DEPARTMENT OF TRANSPORTATION

DESIGNATION 1593230
SHEETS 1 of 227
PROJECT 1593230
CONTRACT R-39270

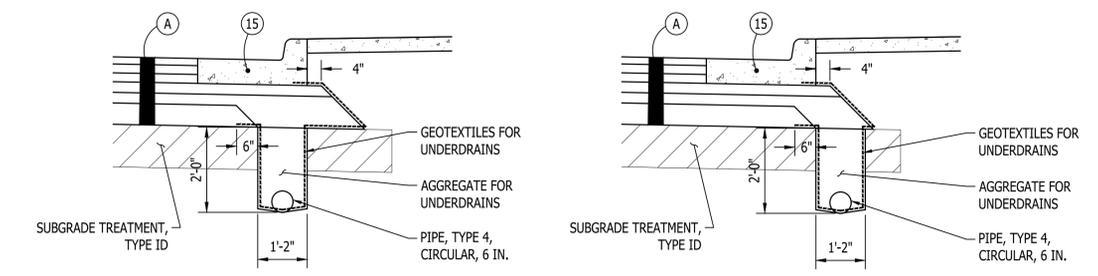
TYPICAL SECTIONS DO NOT APPLY THROUGH INTERSECTIONS OR DEPICT ALL AUXILIARY LANES. SEE PLAN AND PROFILES AND PAVEMENT MARKING DETAILS FOR ADDITIONAL LANE CONFIGURATIONS, DIMENSIONS, TAPER LENGTHS AND LOCATIONS.



TYPICAL SECTION - BEGIN PROJECT TO TROJAN LN.
STA. 376+58 LINE "G" TO STA. 442+34 LINE "G"
SCALE: 1" = 4'



TYPICAL SECTION AT LEFT TURN LANES
BEGIN PROJECT TO TROJAN LN. (REVERSE FOR SOUTHBOUND LANES)
SCALE: 1" = 4'

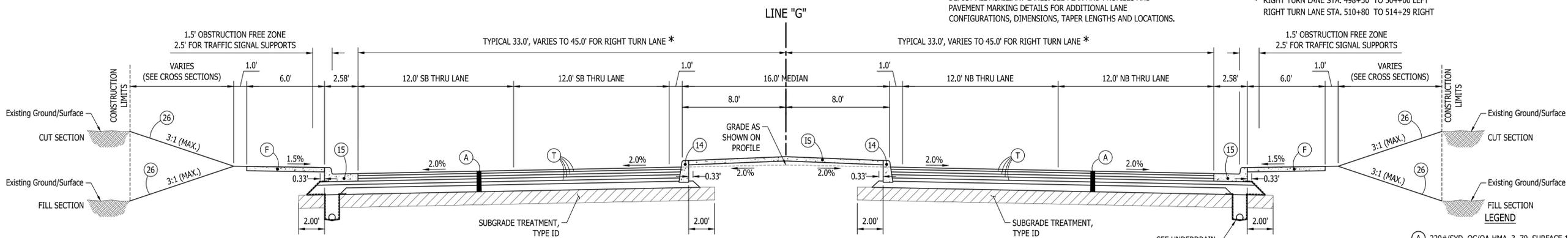


UNDERDRAIN DETAIL
SCALE: 1" = 2'

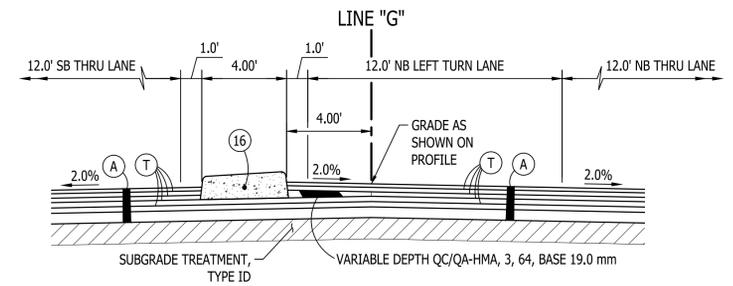
ALTERNATE UNDERDRAIN DETAIL
(USED WHEN PIPE CONNECTING INLET TO INLET RUNS BELOW CURB AND GUTTER)
SCALE: 1" = 2'

TYPICAL SECTIONS DO NOT APPLY THROUGH INTERSECTIONS OR DEPICT ALL AUXILIARY LANES. SEE PLAN AND PROFILES AND PAVEMENT MARKING DETAILS FOR ADDITIONAL LANE CONFIGURATIONS, DIMENSIONS, TAPER LENGTHS AND LOCATIONS.

* RIGHT TURN LANE STA. 498+50 TO 504+00 LEFT
RIGHT TURN LANE STA. 510+80 TO 514+29 RIGHT



TYPICAL SECTION - TROJAN LN. TO NORTH OF INDIANA AVE.
STA. 443+60 LINE "G" TO STA. 515+70 LINE "G"
SCALE: 1" = 4'



TYPICAL SECTION AT LEFT TURN LANES
TROJAN LN. TO NORTH OF INDIANA AVE. (REVERSE FOR SOUTHBOUND LANES)
SCALE: 1" = 4'

- (A) 220#/SYD. QC/QA-HMA, 3, 70, SURFACE 12.5 mm ON
275#/SYD. QC/QA-HMA, 3, 70, INTERMEDIATE 19.0 mm ON
275#/SYD. QC/QA-HMA, 3, 64, BASE 19.0 mm ON
330#/SYD. QC/QA-HMA, 3, 64, BASE 19.0 mm ON
300#/SYD. QC/QA-HMA, 4, 76, INTERMEDIATE, OG, 19.0 mm ON
6 IN. COMPACTED AGGREGATE NO. 53 ON
SUBGRADE TREATMENT, TYPE ID
- (F) SIDEWALK, CONCRETE, 4 IN.
- (IS) ISLAND, CONCRETE, CURB, 4 IN.
- (PC) PCC PATCHING, FULL DEPTH
- (T) ASPHALT FOR TACK COAT
- (14) CONCRETE CURB
- (15) COMBINED CONCRETE CURB AND GUTTER
- (16) CONCRETE CENTER CURB TYPE D
- (26) SODDING
- (27) MULCHED SEEDING, U

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: J.P.H.	DRAWN: J.P.H.	
CHECKED: J.L.B.	CHECKED: J.L.B.	

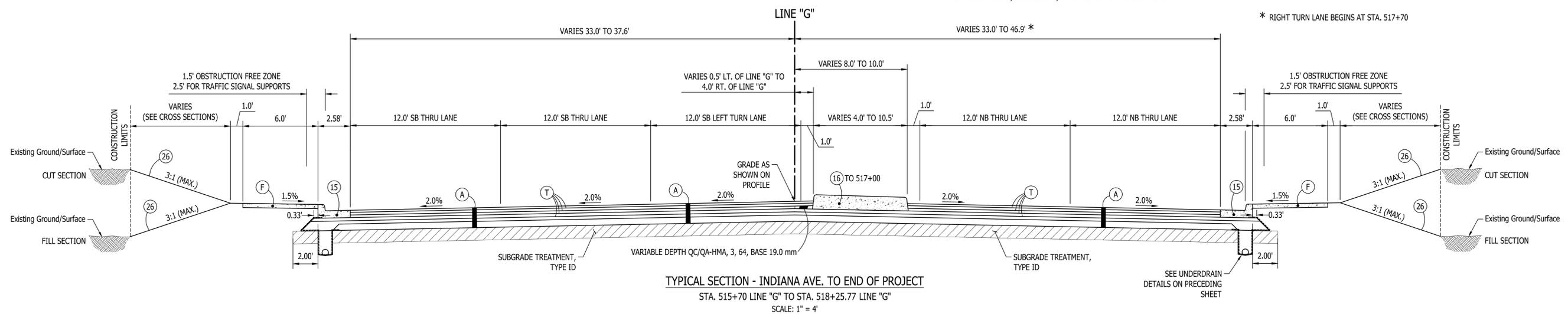
INDIANA
DEPARTMENT OF TRANSPORTATION

S.R. 3 TYPICAL SECTIONS

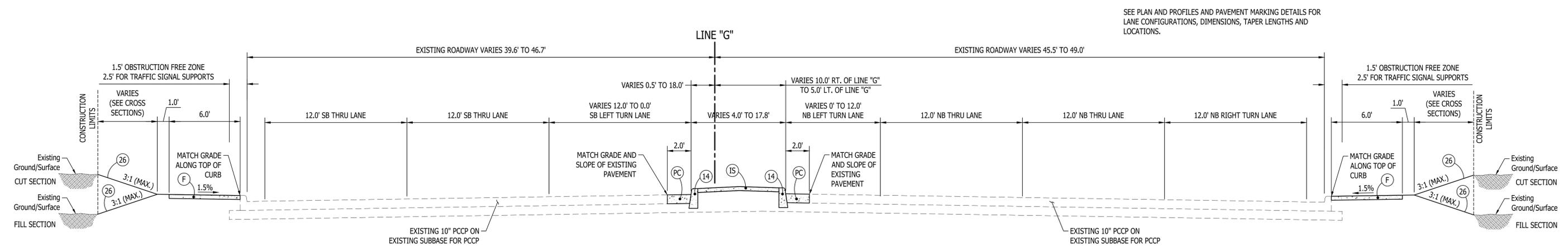
HORIZONTAL SCALE	BRIDGE FILE
AS SHOWN	N/A
VERTICAL SCALE	DESIGNATION
N/A	1593230
SURVEY BOOK	SHEETS
N/A	3 of 227
CONTRACT	PROJECT
R-39270	1593230

TYPICAL SECTIONS DO NOT APPLY THROUGH INTERSECTIONS OR DEPICT ALL AUXILIARY LANES. SEE PLAN AND PROFILES AND PAVEMENT MARKING DETAILS FOR ADDITIONAL LANE CONFIGURATIONS, DIMENSIONS, TAPER LENGTHS AND LOCATIONS.

* RIGHT TURN LANE BEGINS AT STA. 517+70



TYPICAL SECTION - INDIANA AVE. TO END OF PROJECT
STA. 515+70 LINE "G" TO STA. 518+25.77 LINE "G"
SCALE: 1" = 4'



INCIDENTAL CONSTRUCTION SECTION - END OF PROJECT TO S.R. 38
STA. 518+25.77 LINE "G" TO STA. 522+25 LINE "G"
SCALE: 1" = 4'

LEGEND

- (A) 220#/SYD. QC/QA-HMA, 3, 70, SURFACE 12.5 mm ON 275#/SYD. QC/QA-HMA, 3, 70, INTERMEDIATE 19.0 mm ON 275#/SYD. QC/QA-HMA, 3, 64, BASE 19.0 mm ON 330#/SYD. QC/QA-HMA, 3, 64, BASE 19.0 mm ON 300#/SYD. QC/QA-HMA, 4, 76, INTERMEDIATE, OG, 19.0 mm ON 6 IN. COMPACTED AGGREGATE NO. 53 ON SUBGRADE TREATMENT, TYPE ID
- (F) SIDEWALK, CONCRETE, 4 IN.
- (IS) ISLAND, CONCRETE, CURB, 4 IN.
- (PC) PCCP PATCHING, FULL DEPTH
- (T) ASPHALT FOR TACK COAT
- (14) CONCRETE CURB
- (15) COMBINED CONCRETE CURB AND GUTTER
- (16) CONCRETE CENTER CURB TYPE D
- (26) SODDING
- (27) MULCHED SEEDING, U

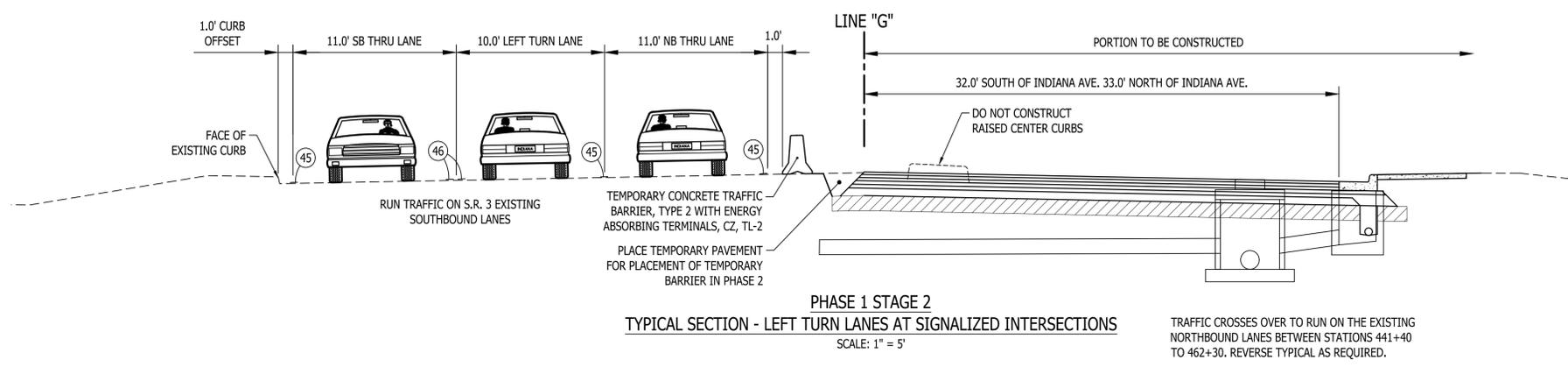
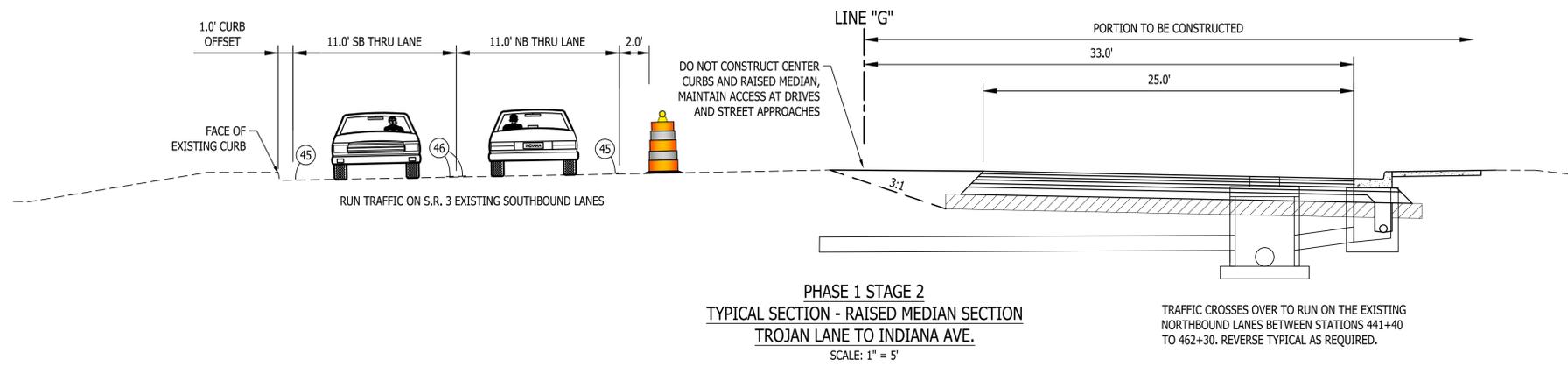
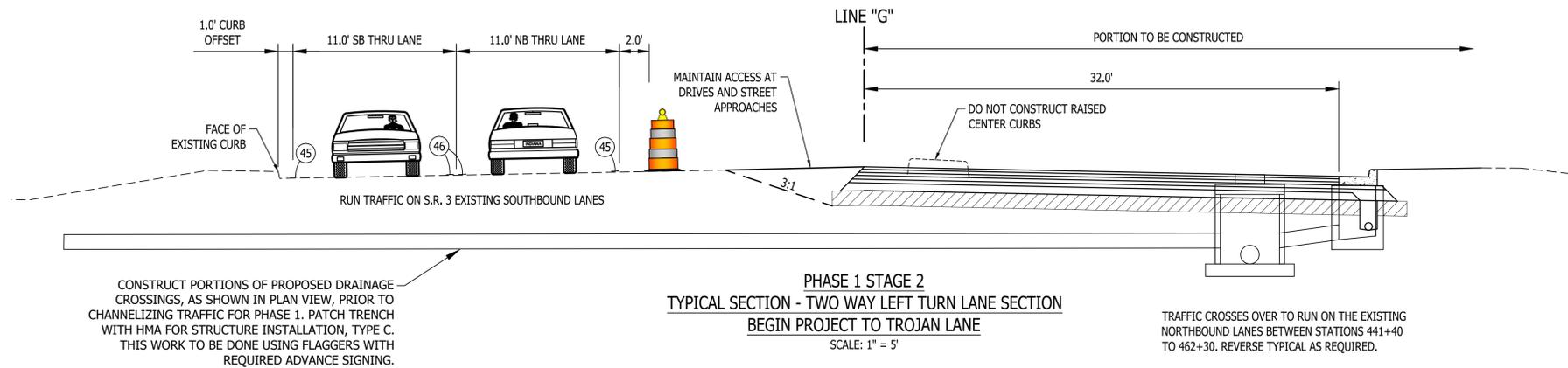
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: J.P.H.	DRAWN: J.P.H.	
CHECKED: J.L.B.	CHECKED: J.L.B.	

INDIANA DEPARTMENT OF TRANSPORTATION

S.R. 3 TYPICAL SECTIONS

HORIZONTAL SCALE	BRIDGE FILE
AS SHOWN	N/A
VERTICAL SCALE	DESIGNATION
N/A	1593230
SURVEY BOOK	SHEETS
N/A	4 of 227
CONTRACT	PROJECT
R-39270	1593230

PHASE 1
CONSTRUCT DOWNSTREAM PORTION OF DRAINAGE STRUCTURES (TYPICAL SECTION NOT SHOWN)
 SEE PLAN VIEW DETAILS FOR TRAFFIC CHANNELLIZATION AND AREA OF CONSTRUCTION



- LEGEND**
- ④⑤ TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
 - ④⑥ TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"

M.O.T. GENERAL NOTES

DESIGN SPEED = 35 M.P.H.

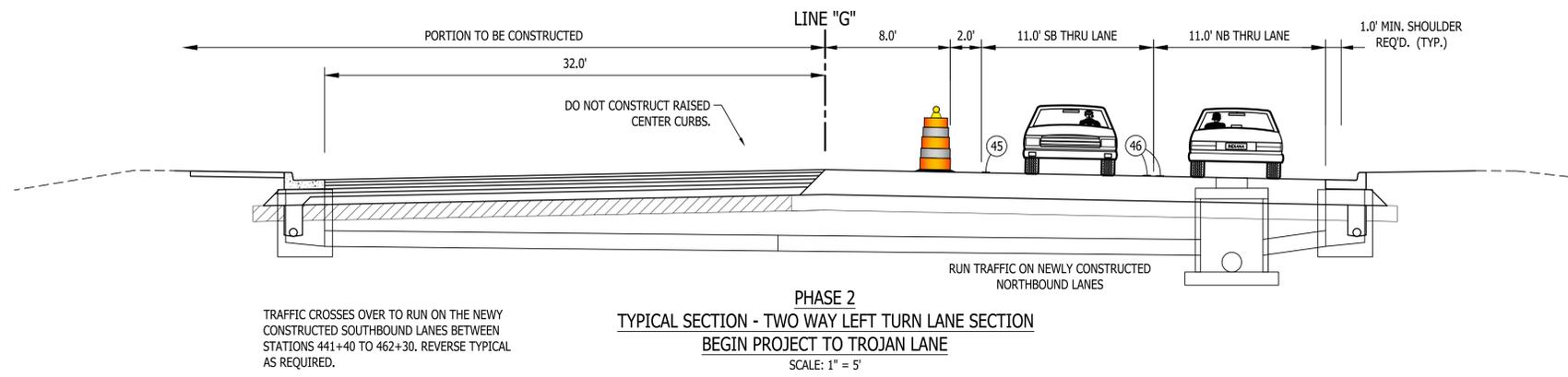
CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELLIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

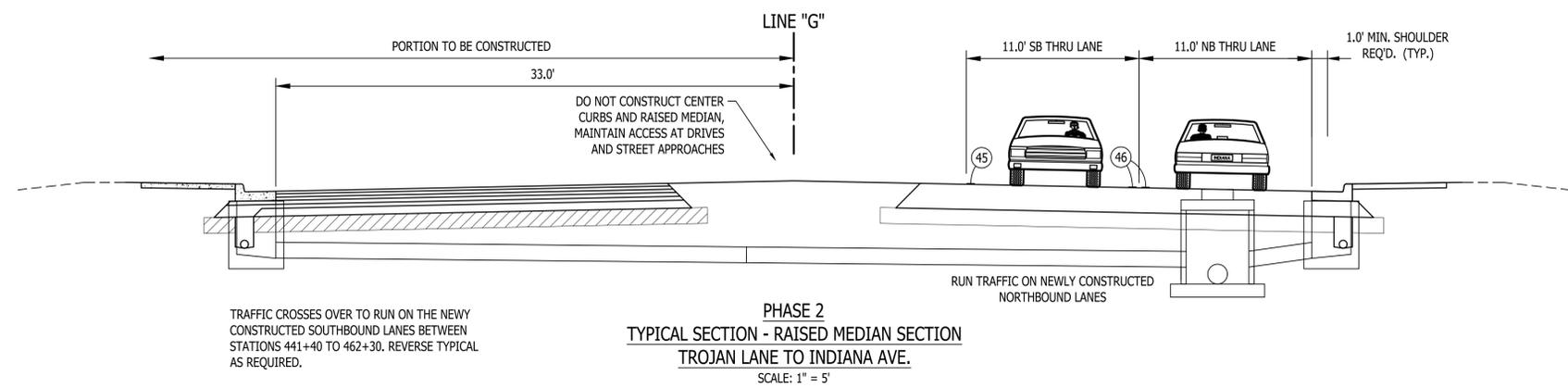
ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

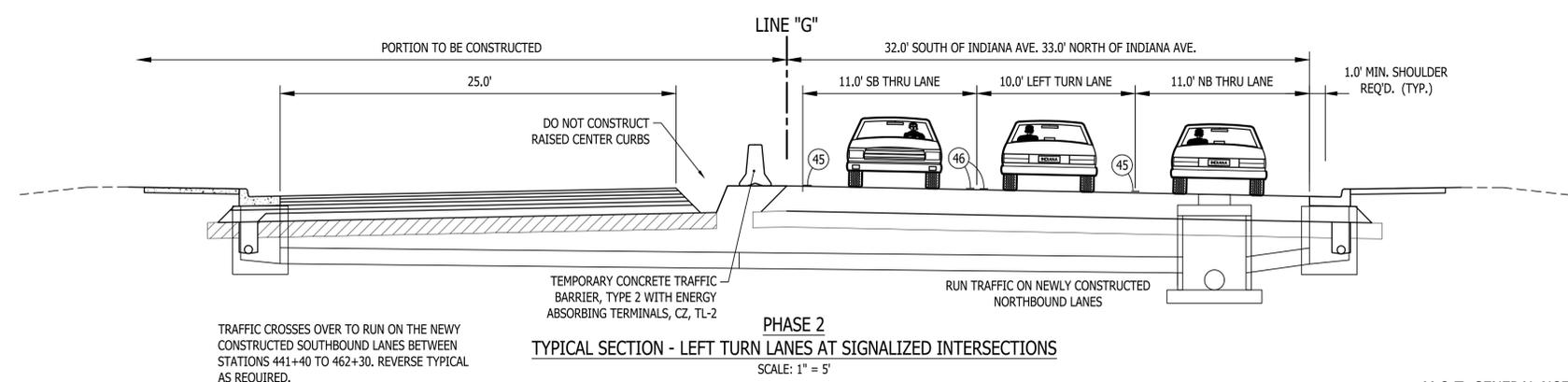
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE
				AS SHOWN	N/A
DESIGNED: J.P.H.	DRAWN: J.P.H.		MOT PHASE 1 TYPICALS	VERTICAL SCALE	DESIGNATION
CHECKED: J.L.B.	CHECKED: J.L.B.			N/A	1593230
				SURVEY BOOK	SHEETS
				N/A	12 of 227
				CONTRACT	PROJECT
				R-39270	1593230



PHASE 2
TYPICAL SECTION - TWO WAY LEFT TURN LANE SECTION
 BEGIN PROJECT TO TROJAN LANE
 SCALE: 1" = 5'



PHASE 2
TYPICAL SECTION - RAISED MEDIAN SECTION
 TROJAN LANE TO INDIANA AVE.
 SCALE: 1" = 5'



PHASE 2
TYPICAL SECTION - LEFT TURN LANES AT SIGNALIZED INTERSECTIONS
 SCALE: 1" = 5'

- LEGEND**
- (45) TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
 - (46) TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"

PHASE 2 STAGE 2
CONSTRUCT SOUTHBOUND LANE OF CROSSOVER AREAS (TYPICAL SECTION NOT SHOWN)
 SEE PLAN VIEW DETAILS FOR TRAFFIC CHANNELIZATION AND AREA OF CONSTRUCTION

PHASE 2 STAGE 3
CONSTRUCT NORTHBOUND LANE OF CROSSOVER AREAS (TYPICAL SECTION NOT SHOWN)
 SEE PLAN VIEW DETAILS FOR TRAFFIC CHANNELIZATION AND AREA OF CONSTRUCTION

PHASE 2 STAGE 4
CONSTRUCT CENTER CURBS AND MEDIAN (TYPICAL AND PLAN VIEW NOT SHOWN)
 CHANNEL TRAFFIC TO RUN IN A SINGLE SOUTHBOUND LANE ON THE OUTER MOST LANE OF THE NEWLY CONSTRUCTED SOUTHBOUND LANES, AND IN A SINGLE NORTHBOUND LANE ON THE OUTER MOST LANE OF THE NEWLY CONSTRUCTED NORTHBOUND LANES. AT THE SIGNALIZE INTERSECTIONS PROVIDE A 10 FT. WIDE LEFT TURN LANE USING THE INSIDE THROUGH LANE. CONSTRUCT MEDIANS AND CENTER CURBS USING FLAGGERS AS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

M.O.T. GENERAL NOTES

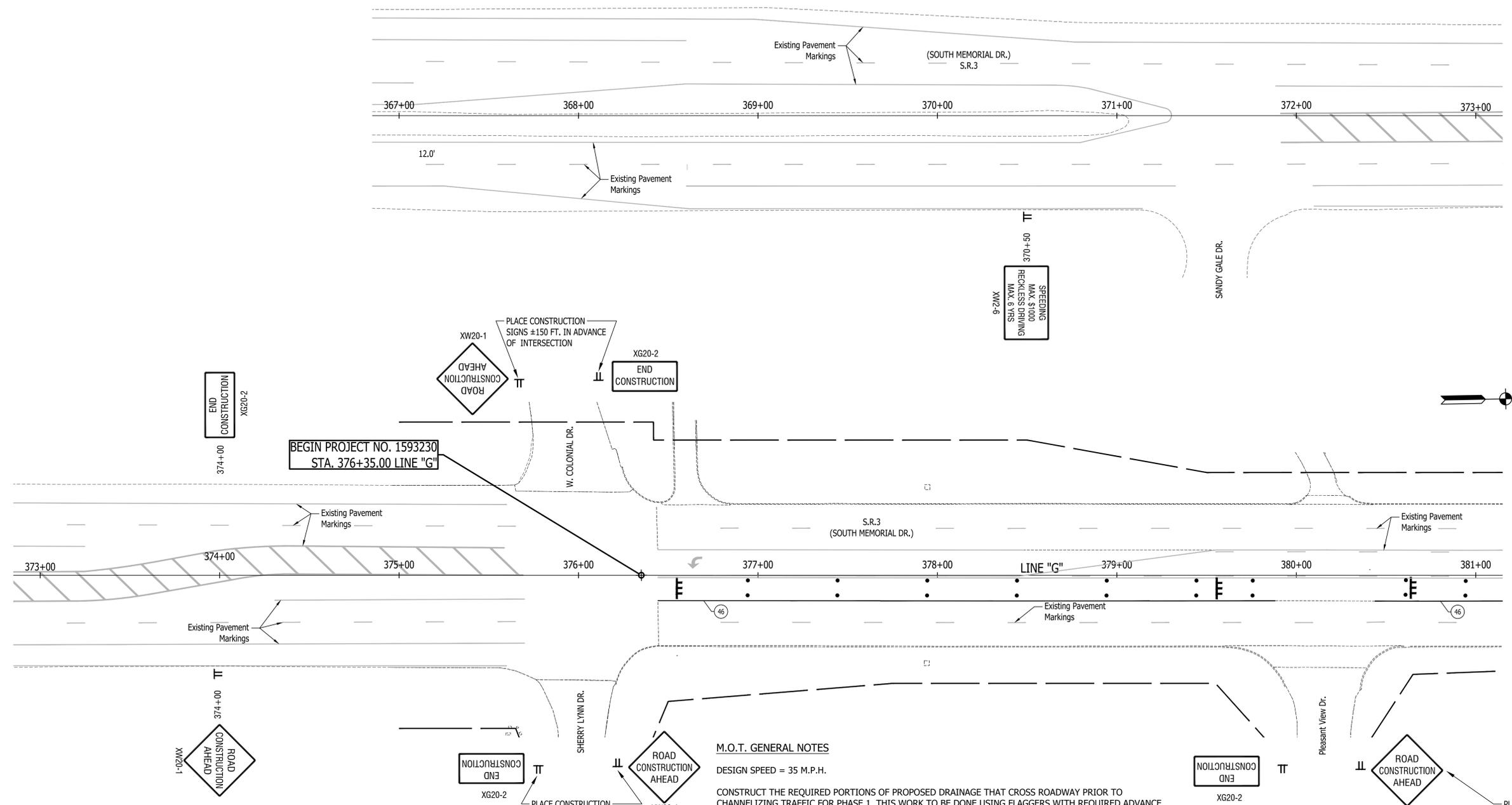
- DESIGN SPEED = 35 M.P.H.
- CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.
- EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.
- ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.
- FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: J.P.H.	DRAWN: J.P.H.	
CHECKED: J.L.B.	CHECKED: J.L.B.	

INDIANA
DEPARTMENT OF TRANSPORTATION

MOT PHASE 2 TYPICALS

HORIZONTAL SCALE	BRIDGE FILE
AS SHOWN	N/A
VERTICAL SCALE	DESIGNATION
N/A	1593230
SURVEY BOOK	SHEETS
N/A	13 of 227
CONTRACT	PROJECT
R-39270	1593230



- FLAGGER (SEE M.O.T. GENERAL NOTES)
- STANDARD DRUM
- BARRICADE, TYPE III-B
- TRAFFIC FLOW ARROW
- CONSTRUCTION SIGNS, TYPE A, B OR C
- FLASHING ARROW SIGN

- LEGEND**
- (45) TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
 - (46) TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"
 - (47) TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 24"
 - (48) TEMPORARY PAVEMENT MARKING, DOTTED, YELLOW, 4" (2' LINE, 4' SPACE)
 - (49) TEMPORARY PAVEMENT MARKING, DOTTED, WHITE, 4" (2' LINE, 4' SPACE)
 - TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW

AREA OF CONSTRUCTION (NOT EXACT CONSTRUCTION LIMITS)

M.O.T. GENERAL NOTES

DESIGN SPEED = 35 M.P.H.

CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

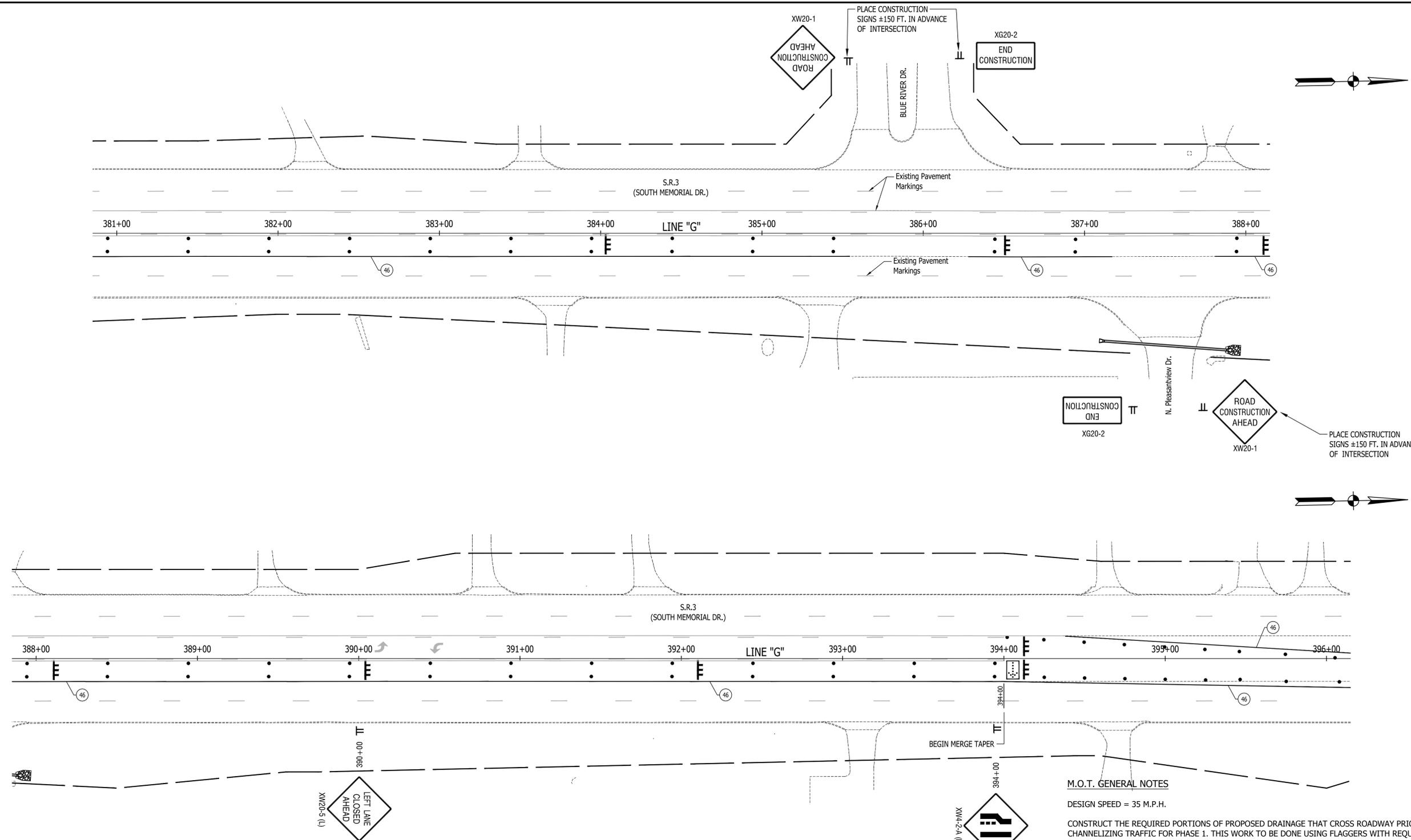
FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____ DATE _____	
DESIGNED: J.P.H.	DRAWN: J.P.H.		
CHECKED: J.L.B.	CHECKED: J.L.B.		

INDIANA DEPARTMENT OF TRANSPORTATION

MOT PHASE 1 PLAN SHEETS

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1593230
SURVEY BOOK	SHEETS
N/A	14 of 227
CONTRACT	PROJECT
R-39270	1593230



M.O.T. GENERAL NOTES

DESIGN SPEED = 35 M.P.H.

CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

AREA OF CONSTRUCTION
(NOT EXACT CONSTRUCTION LIMITS)

- LEGEND**
- FLAGGER (SEE M.O.T. GENERAL NOTES)
 - STANDARD DRUM
 - BARRICADE, TYPE III-B
 - TRAFFIC FLOW ARROW
 - CONSTRUCTION SIGNS, TYPE A, B OR C
 - FLASHING ARROW SIGN

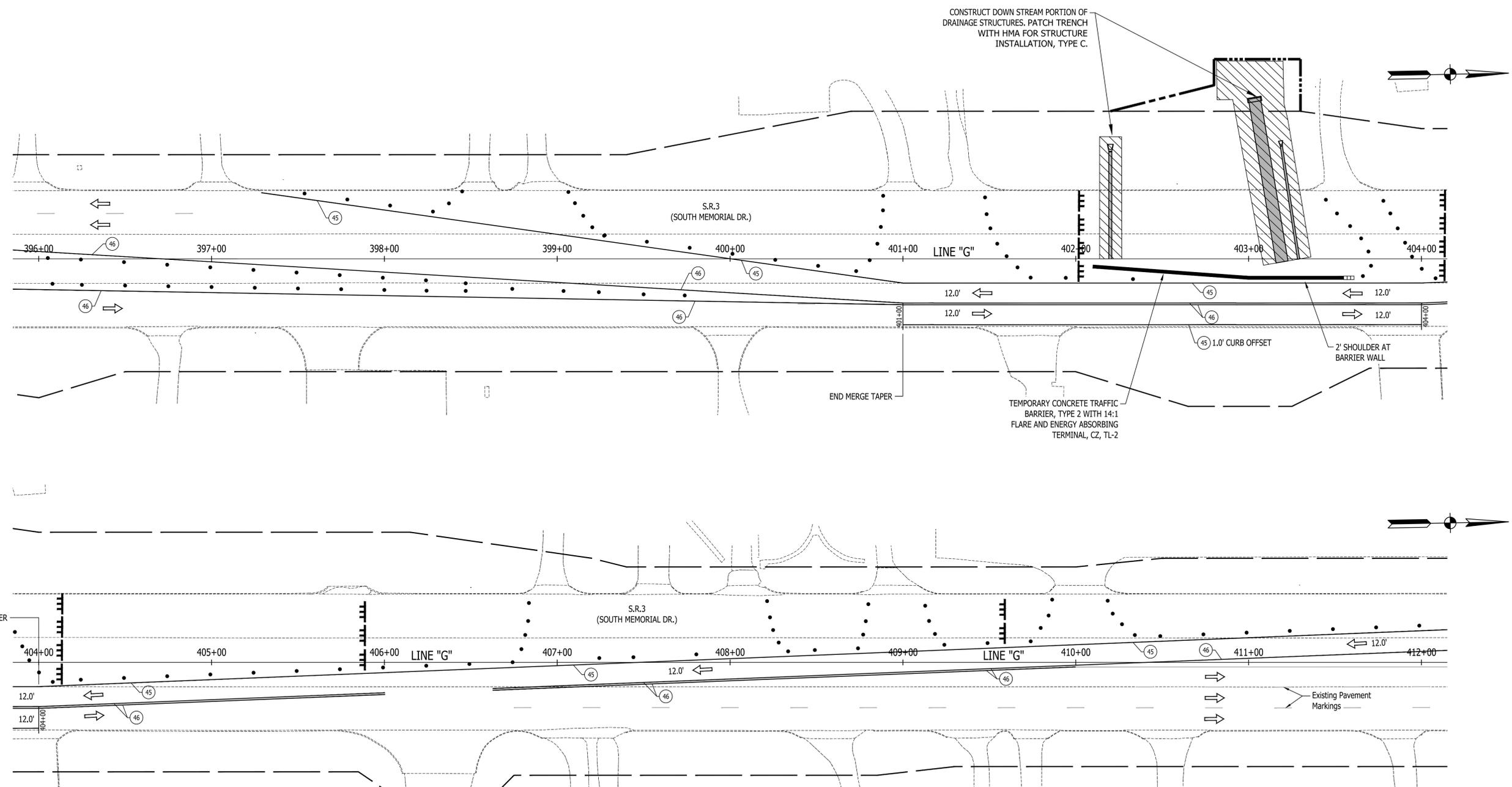
- 45 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
- 46 TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"
- 47 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 24"
- 48 TEMPORARY PAVEMENT MARKING, DOTTED, YELLOW, 4" (2' LINE, 4' SPACE)
- 49 TEMPORARY PAVEMENT MARKING, DOTTED, WHITE, 4" (2' LINE, 4' SPACE)
- TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: J.P.H.	DRAWN: J.P.H.
CHECKED: J.L.B.	CHECKED: J.L.B.

**INDIANA
DEPARTMENT OF TRANSPORTATION**

MOT PHASE 1 PLAN SHEETS

HORIZONTAL SCALE 1" = 30'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1593230
SURVEY BOOK N/A	SHEETS 15 of 227
CONTRACT R-39270	PROJECT 1593230



M.O.T. GENERAL NOTES

DESIGN SPEED = 35 M.P.H.

CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

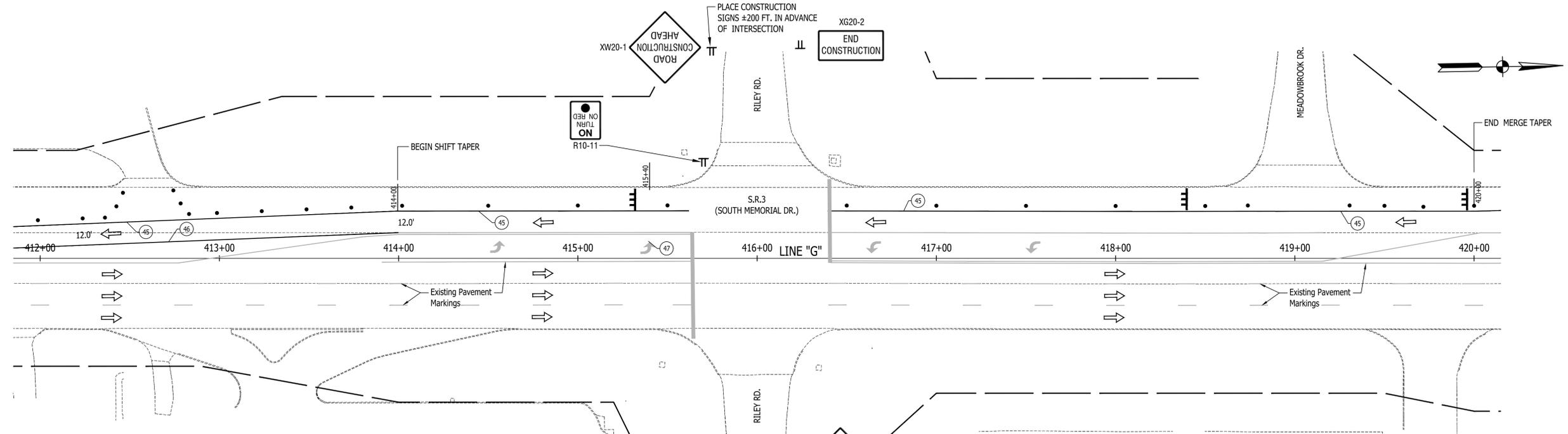
ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

LEGEND

FLAGGER (SEE M.O.T. GENERAL NOTES)	FLASHING ARROW SIGN
STANDARD DRUM	45 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
BARRICADE, TYPE III-B	46 TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"
TRAFFIC FLOW ARROW	47 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 24"
CONSTRUCTION SIGNS, TYPE A, B OR C	TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE
				1" = 30'	N/A
DESIGNED: J.P.H.	DRAWN: J.P.H.		MOT PHASE 1 PLAN SHEETS	VERTICAL SCALE	DESIGNATION
CHECKED: J.L.B.	CHECKED: J.L.B.			N/A	1593230
				SURVEY BOOK	SHEETS
			N/A	16 of 227	
			CONTRACT	PROJECT	
			R-39270	1593230	



M.O.T. GENERAL NOTES

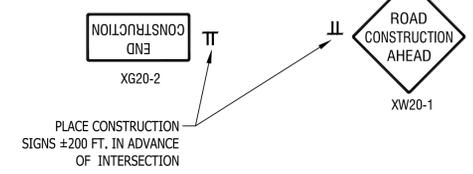
DESIGN SPEED = 35 M.P.H.

CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

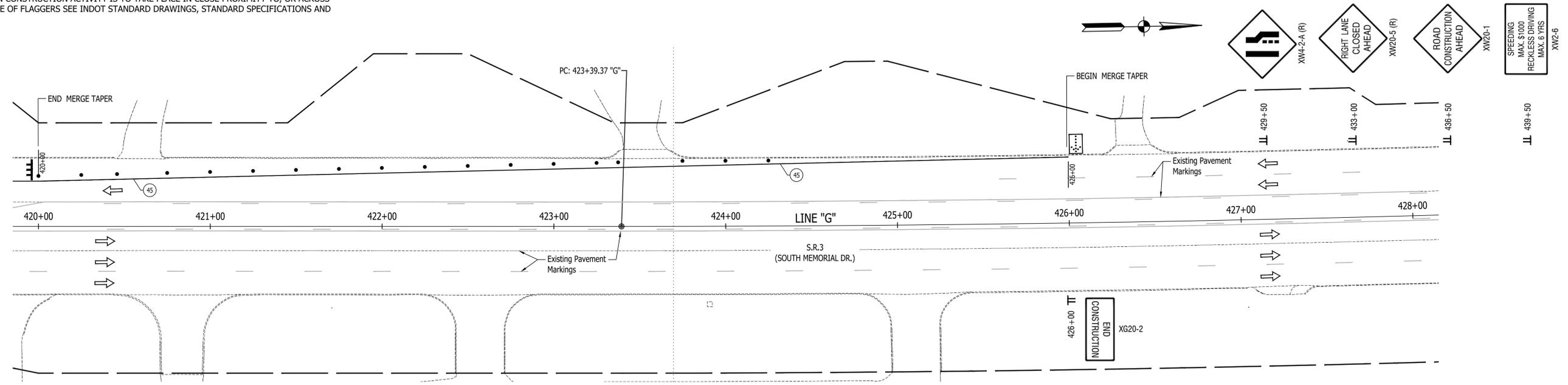
EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

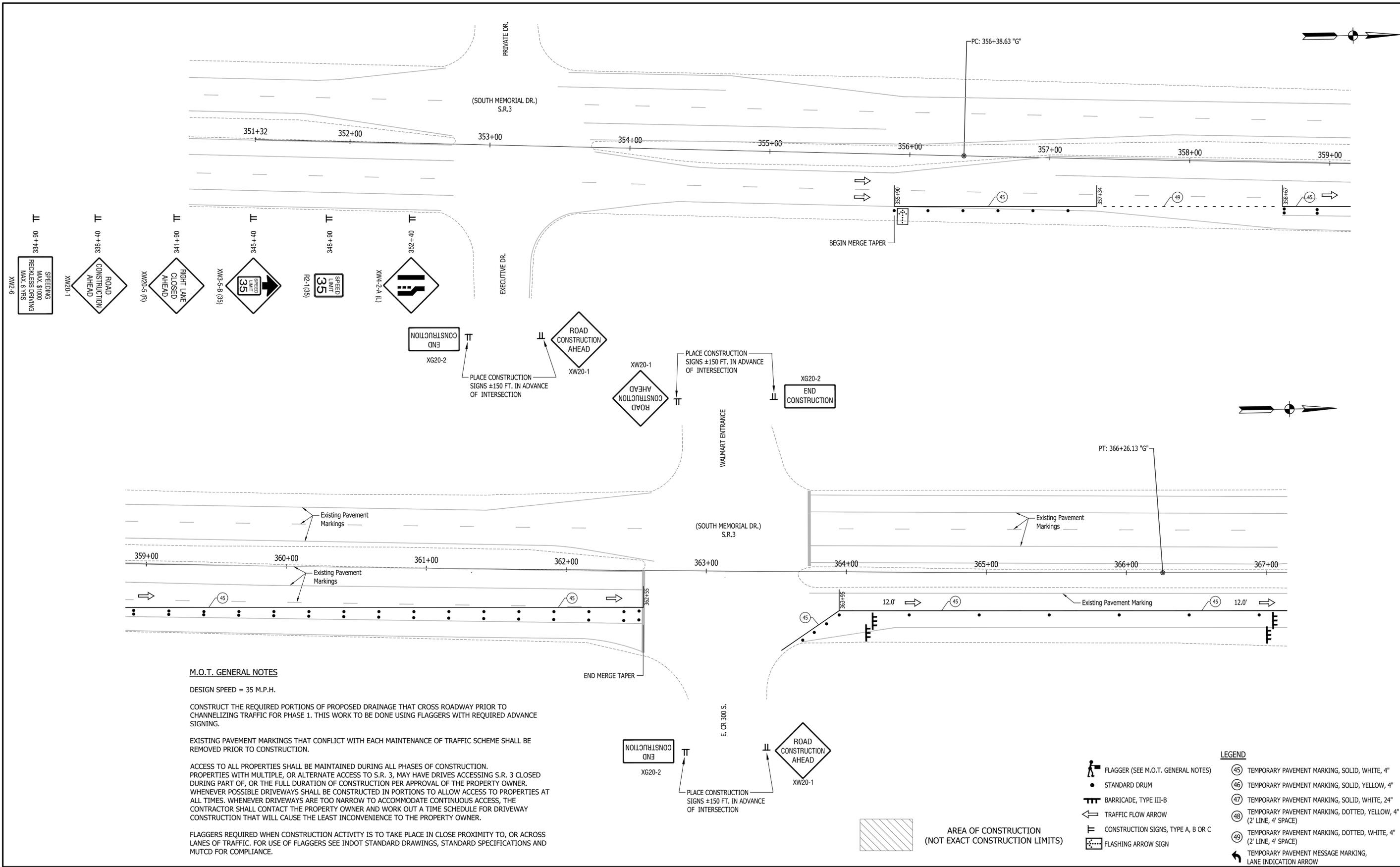
FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.



- LEGEND**
- FLAGGER (SEE M.O.T. GENERAL NOTES)
 - STANDARD DRUM
 - BARRICADE, TYPE III-B
 - TRAFFIC FLOW ARROW
 - CONSTRUCTION SIGNS, TYPE A, B OR C
 - FLASHING ARROW SIGN
 - 45 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
 - 46 TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"
 - 47 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 24"
 - 48 TEMPORARY PAVEMENT MARKING, DOTTED, YELLOW, 4" (2' LINE, 4' SPACE)
 - 49 TEMPORARY PAVEMENT MARKING, DOTTED, WHITE, 4" (2' LINE, 4' SPACE)
 - TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW



RECOMMENDED FOR APPROVAL	DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE
	DESIGNED: J.P.H.	DRAWN: J.P.H.		1" = 30'	N/A
CHECKED: J.L.B.	CHECKED: J.L.B.	MOT PHASE 1 PLAN SHEETS		VERTICAL SCALE	DESIGNATION
				N/A	1593230
				SURVEY BOOK	SHEETS
				N/A	17 of 227
				CONTRACT	PROJECT
				R-39270	1593230



M.O.T. GENERAL NOTES

DESIGN SPEED = 35 M.P.H.

CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

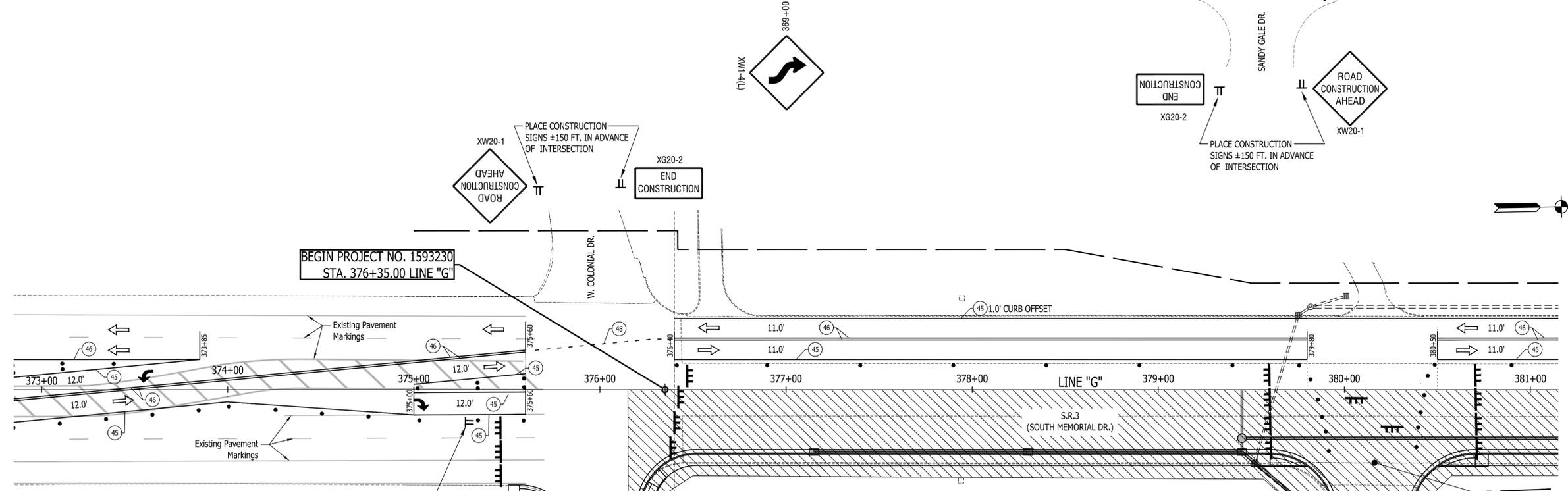
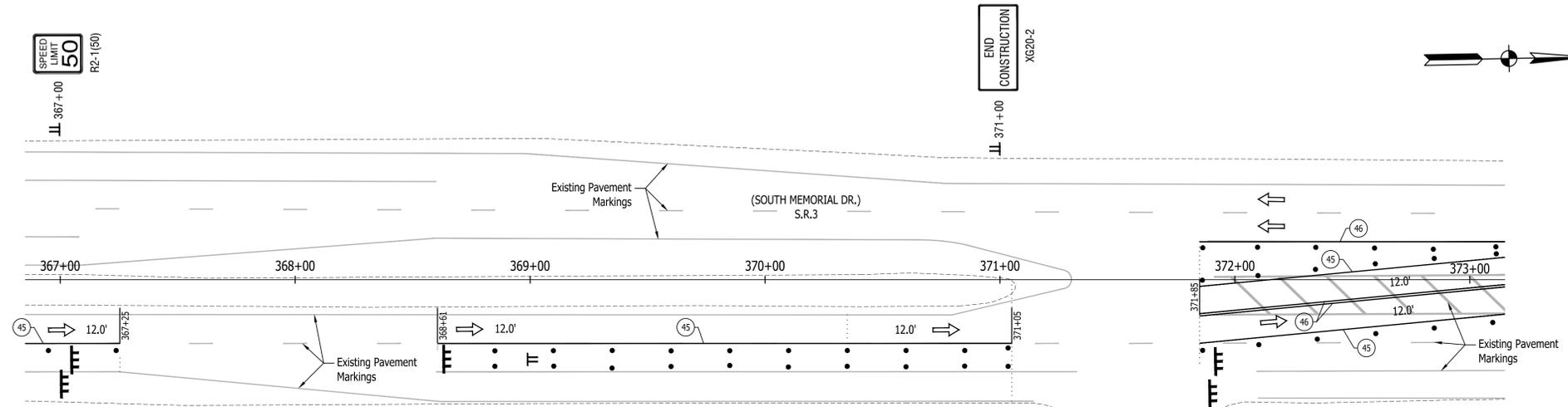
EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

LEGEND	
	FLAGGER (SEE M.O.T. GENERAL NOTES)
	STANDARD DRUM
	BARRICADE, TYPE III-B
	TRAFFIC FLOW ARROW
	CONSTRUCTION SIGNS, TYPE A, B OR C
	FLASHING ARROW SIGN
	TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
	TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"
	TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 24"
	TEMPORARY PAVEMENT MARKING, DOTTED, YELLOW, 4" (2' LINE, 4' SPACE)
	TEMPORARY PAVEMENT MARKING, DOTTED, WHITE, 4" (2' LINE, 4' SPACE)
	TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE
	DESIGNED: J.P.H. DRAWN: J.P.H.			1" = 30'	N/A
CHECKED: J.L.B.	CHECKED: J.L.B.		MOT PHASE 1 STAGE 2 PLAN SHEETS	VERTICAL SCALE	DESIGNATION
				N/A	1593230
				SURVEY BOOK	SHEETS
				N/A	18 of 227
				CONTRACT	PROJECT
				R-39270	1593230



- LEGEND**
- FLAGGER (SEE M.O.T. GENERAL NOTES)
 - STANDARD DRUM
 - BARRICADE, TYPE III-B
 - TRAFFIC FLOW ARROW
 - CONSTRUCTION SIGNS, TYPE A, B OR C
 - FLASHING ARROW SIGN
 - (45) TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
 - (46) TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"
 - (47) TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 24"
 - (48) TEMPORARY PAVEMENT MARKING, DOTTED, YELLOW, 4" (2' LINE, 4' SPACE)
 - (49) TEMPORARY PAVEMENT MARKING, DOTTED, WHITE, 4" (2' LINE, 4' SPACE)
 - TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW

AREA OF CONSTRUCTION (NOT EXACT CONSTRUCTION LIMITS)

M.O.T. GENERAL NOTES

DESIGN SPEED = 35 M.P.H.

CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

SHORT TERM ROAD CLOSURE. CONSTRUCT APPROACH IN PORTIONS WHILE MAINTAINING ACCESS TO LOCAL TRAFFIC. PLACE BARRICADES WITH R11-3 (ROAD CLOSED - LOCAL TRAFFIC ONLY) SIGNS.

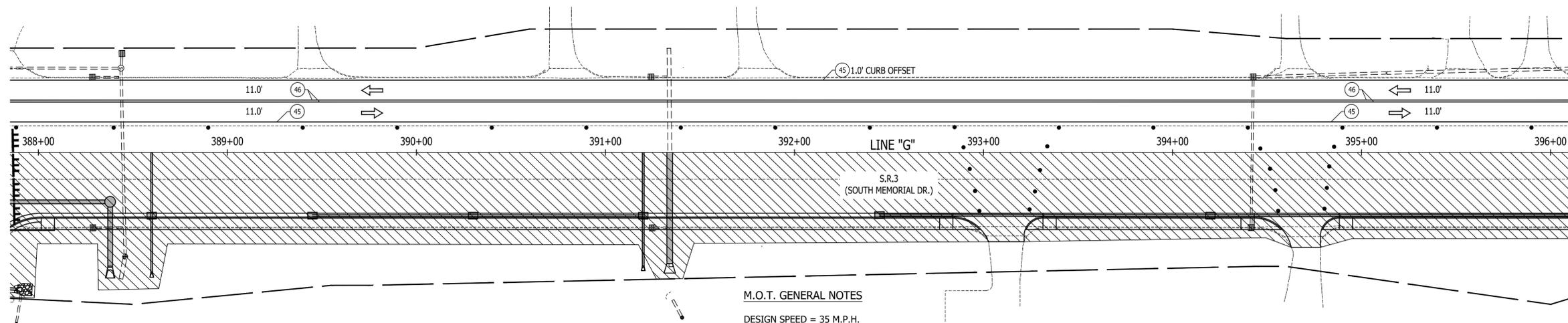
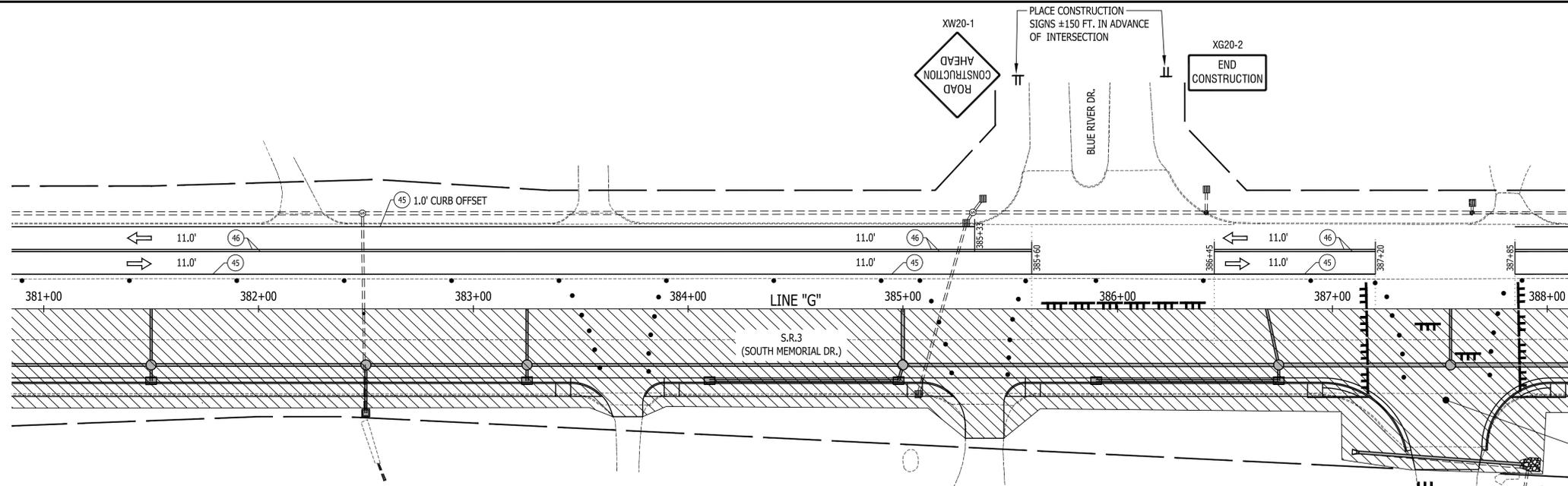
PLACE CONSTRUCTION SIGNS ±150 FT. IN ADVANCE OF INTERSECTION

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: J.P.H.	DRAWN: J.P.H.	
CHECKED: J.L.B.	CHECKED: J.L.B.	

**INDIANA
DEPARTMENT OF TRANSPORTATION**

MOT PHASE 1 STAGE 2 PLAN SHEETS

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1593230
SURVEY BOOK	SHEETS
N/A	19 of 227
CONTRACT	PROJECT
R-39270	1593230



M.O.T. GENERAL NOTES

DESIGN SPEED = 35 M.P.H.

CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

AREA OF CONSTRUCTION
(NOT EXACT CONSTRUCTION LIMITS)

- LEGEND**
- FLAGGER (SEE M.O.T. GENERAL NOTES)
 - STANDARD DRUM
 - BARRICADE, TYPE III-B
 - TRAFFIC FLOW ARROW
 - CONSTRUCTION SIGNS, TYPE A, B OR C
 - FLASHING ARROW SIGN

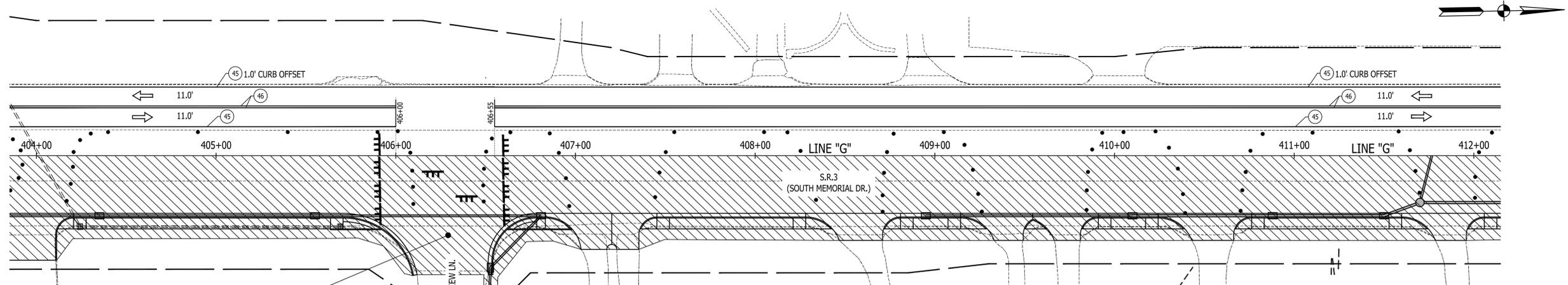
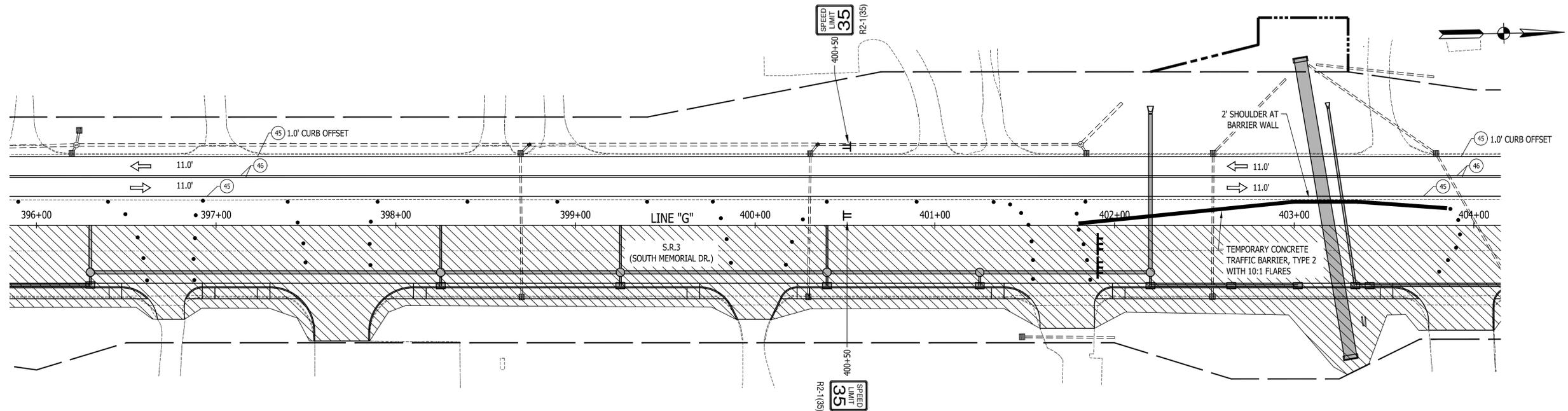
- 45 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
- 46 TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"
- 47 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 24"
- 48 TEMPORARY PAVEMENT MARKING, DOTTED, YELLOW, 4" (2' LINE, 4' SPACE)
- 49 TEMPORARY PAVEMENT MARKING, DOTTED, WHITE, 4" (2' LINE, 4' SPACE)
- TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: J.P.H.	DRAWN: J.P.H.	
CHECKED: J.L.B.	CHECKED: J.L.B.	

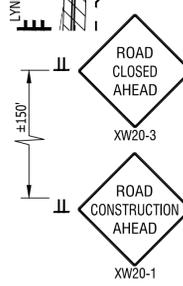
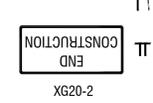
INDIANA DEPARTMENT OF TRANSPORTATION

MOT PHASE 1 STAGE 2 PLAN SHEETS

HORIZONTAL SCALE	BRIDGE FILE
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1593230
SURVEY BOOK	SHEETS
N/A	20 of 227
CONTRACT	PROJECT
R-39270	1593230



SHORT TERM ROAD CLOSURE. CONSTRUCT APPROACH IN PORTIONS WHILE MAINTAINING ACCESS TO LOCAL TRAFFIC. PLACE BARRICADES WITH R11-3 (ROAD CLOSED - LOCAL TRAFFIC ONLY) SIGNS.



PLACE CONSTRUCTION SIGNS ±150 FT. IN ADVANCE OF INTERSECTION

M.O.T. GENERAL NOTES

DESIGN SPEED = 35 M.P.H.

CONSTRUCT THE REQUIRED PORTIONS OF PROPOSED DRAINAGE THAT CROSS ROADWAY PRIOR TO CHANNELIZING TRAFFIC FOR PHASE 1. THIS WORK TO BE DONE USING FLAGGERS WITH REQUIRED ADVANCE SIGNING.

EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH EACH MAINTENANCE OF TRAFFIC SCHEME SHALL BE REMOVED PRIOR TO CONSTRUCTION.

ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION. PROPERTIES WITH MULTIPLE, OR ALTERNATE ACCESS TO S.R. 3, MAY HAVE DRIVES ACCESSING S.R. 3 CLOSED DURING PART OF, OR THE FULL DURATION OF CONSTRUCTION PER APPROVAL OF THE PROPERTY OWNER. WHENEVER POSSIBLE DRIVEWAYS SHALL BE CONSTRUCTED IN PORTIONS TO ALLOW ACCESS TO PROPERTIES AT ALL TIMES. WHENEVER DRIVEWAYS ARE TOO NARROW TO ACCOMMODATE CONTINUOUS ACCESS, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER AND WORK OUT A TIME SCHEDULE FOR DRIVEWAY CONSTRUCTION THAT WILL CAUSE THE LEAST INCONVENIENCE TO THE PROPERTY OWNER.

FLAGGERS REQUIRED WHEN CONSTRUCTION ACTIVITY IS TO TAKE PLACE IN CLOSE PROXIMITY TO, OR ACROSS LANES OF TRAFFIC. FOR USE OF FLAGGERS SEE INDOT STANDARD DRAWINGS, STANDARD SPECIFICATIONS AND MUTCD FOR COMPLIANCE.

AREA OF CONSTRUCTION (NOT EXACT CONSTRUCTION LIMITS)

LEGEND

FLAGGER (SEE M.O.T. GENERAL NOTES)	FLASHING ARROW SIGN
STANDARD DRUM	45 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 4"
BARRICADE, TYPE III-B	46 TEMPORARY PAVEMENT MARKING, SOLID, YELLOW, 4"
TRAFFIC FLOW ARROW	47 TEMPORARY PAVEMENT MARKING, SOLID, WHITE, 24"
CONSTRUCTION SIGNS, TYPE A, B OR C	TEMPORARY PAVEMENT MESSAGE MARKING, LANE INDICATION ARROW

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
			1" = 30'	N/A
DESIGNED: J.P.H. DRAWN: J.P.H.	MOT PHASE 1 STAGE 2 PLAN SHEETS		VERTICAL SCALE	DESIGNATION
CHECKED: J.L.B. CHECKED: J.L.B.			N/A	1593230
		SURVEY BOOK		SHEETS
		N/A		21 of 227
		CONTRACT		PROJECT
		R-39270		1593230